

Agenda

Strathfield Local Planning Panel Meeting

Notice is hereby given that a Strathfield Local Planning Panel Meeting will be held at Town Hall (Supper Room), 65 Homebush Road, Strathfield on:

Thursday, 6 February 2020

Commencing at 10:00am for the purpose of considering items included on the Agenda

Persons in the gallery are advised that the proceedings of the meeting are being recorded for the purpose of ensuring the accuracy of the Minutes. However, under the Local Government Act 1993, no other tape recording is permitted without the authority of the Council or Committee. Tape recording includes a video camera and any electronic device capable of recording speech.

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TO: Strathfield Local Planning Panel Meeting - 6 February 2020
REPORT: SLPP – Report No. 1
SUBJECT: DA2019/181 - 26 KINGSLAND ROAD, STRATHFIELD - LOT 30 DP 211
DA NO. DA2019/181

SUMMARY

Proposal: Demolition of existing buildings and construction of a two (2) storey dwelling with basement level

Applicant: Linda Sukkar

Owner: Linda Sukkar

Date of lodgement: 31 October 2019

Notification period: 7 November to 21 November 2019

Submissions received: Nil

Assessment officer: MR

Estimated cost of works: \$1,150,000

Zoning: R2 – Low Density Residential – SLEP 2012

Heritage: No

Flood affected: No

Is a Clause 4.6 variation proposed? No

RECOMMENDATION OF OFFICER: APPROVAL

EXECUTIVE SUMMARY

The application seeks Council approval for the demolition of existing structures and construction of a two (2) storey dwelling with basement level. The application also involves retention of the existing in-ground swimming pool in the rear yard.

The application was notified in accordance with Part L of the Strathfield Consolidated Development Control Plan 2005 (SCDCP 2005), from 7 November to 21 November 2019. No submissions were received during this time.

The proposed development is considered acceptable and generally supportable, complying with most of the relevant development standards and requirements under the Strathfield Local Environmental Plan 2012 (SLEP 2012) and SCDCP 2005. The final design of the proposed development has an appropriate response to the suburban, low density residential character of the streetscape and surrounding locality and the unique environmental constraints and neighbouring properties associated with the subject land.

BACKGROUND

22 November 2018

A Pre-Development Application meeting was held regarding this development. As a result of this, a formal letter of response was issued by Council dated 22 November 2018. The main points covered in this letter included:

- Floor space ratio,
- Building height,

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- Streetscape,
- Landscaping,
- Side setbacks,
- Visual privacy,
- Proposed driveway crossing, and
- Rear upper level balcony.

31 October 2019	The subject application was lodged.
7 November 2019	The application was placed on exhibition, with the last date for submissions being 21 November 2019.
6 December 2019	A deferral letter was issued to the applicant, raising a number of issues including floor space ratio, basement level, landscaped area, visual amenity and privacy.
9 December 2019	Council officers met with the applicant to discuss the issues raised in the deferral letter.
12 and 18 December 2019	The applicant provided additional information to address the issues raised in the deferral letter.

DESCRIPTION OF THE SITE AND LOCALITY

The subject site is legally described as Lot 30 in DP 211 and is known as No. 26 Kingsland Road, Strathfield. The site is a corner allotment located on the eastern side of Kingsland Road and on the northern side of Highgate Street (refer to Figure 1). The site is within the R2 – Low Density Residential zone, pursuant to the provisions of SLEP 2012. The streetscape and surrounding locality have a typical suburban character featuring tree-lined streets and single detached dwellings of mixed design and scale. Housing designs are generally dominated by a combination of palatial and postmodern contemporary two (2) storey residences and more traditional, single storey, post-war Californian bungalows. Common architectural features and traits along Kingsland Road and Highgate Street include attached double garaging, black and red tiled, pitched or gabled roofs, front balconies, and exposed red/maroon/brown brick or rendered white/grey masonry finishes. Many residences have front fencing, with the prevailing fence designs featuring brick piers and metal infill, light-coloured, rendered masonry walls, or hedging/plantings adjacent to low rise, exposed brick walls.

The site comprises a rectangular shape and a west-east orientation. It has a wide frontage of 17.655m, a maximum depth of 36.555m and a total area of 644.87m². The site has a slight fall of 0.83m from Kingsland Road to the rear (eastern) boundary. The subject site is mostly modified and is devoid of any significant native vegetation. The only vegetation occurring consists of open grassed turf areas, some shrubs and hedges, and a Queen Palm (*Syagrus romanzoffiana*) in the front setback. The site is occupied by a two (2) storey mid-century, rendered and bricked house featuring Tudor-style built elements such as a four-sided mansard/French roof and dormer windows (refer to Figures 2 and 3). Current vehicular access is via a driveway off Kingsland Road. The site also contains a kidney-shaped in-ground swimming pool in the south-eastern corner, adjacent to the secondary street boundary (refer to Figure 4).

The site is surrounded by the following properties:

- Adjoining the northern boundary: multi-storey, modern, palatial dwellings on two (2) separate lots at No. 16 and 18 Wakeford Road. It is noted that the residence at No. 16 Wakeford Road comprises a basement level;

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- Adjoining the rear (eastern) boundary: two (20) storey, red brick dwelling with an outbuilding and pool at No. 15 Highgate Street (refer to Figure 5). It is noted that this residence has a wrap-around balcony that has views over the rear yard of the subject land;
- South of the site and across the road: two (2) storey dwellings on two (2) separate lots at No. 18 and 20 Highgate Street (Figures 6 and 7); and
- West of the site and across the road: a single storey, red brick dwelling at No. 27A Kingsland Road (Figure 8).

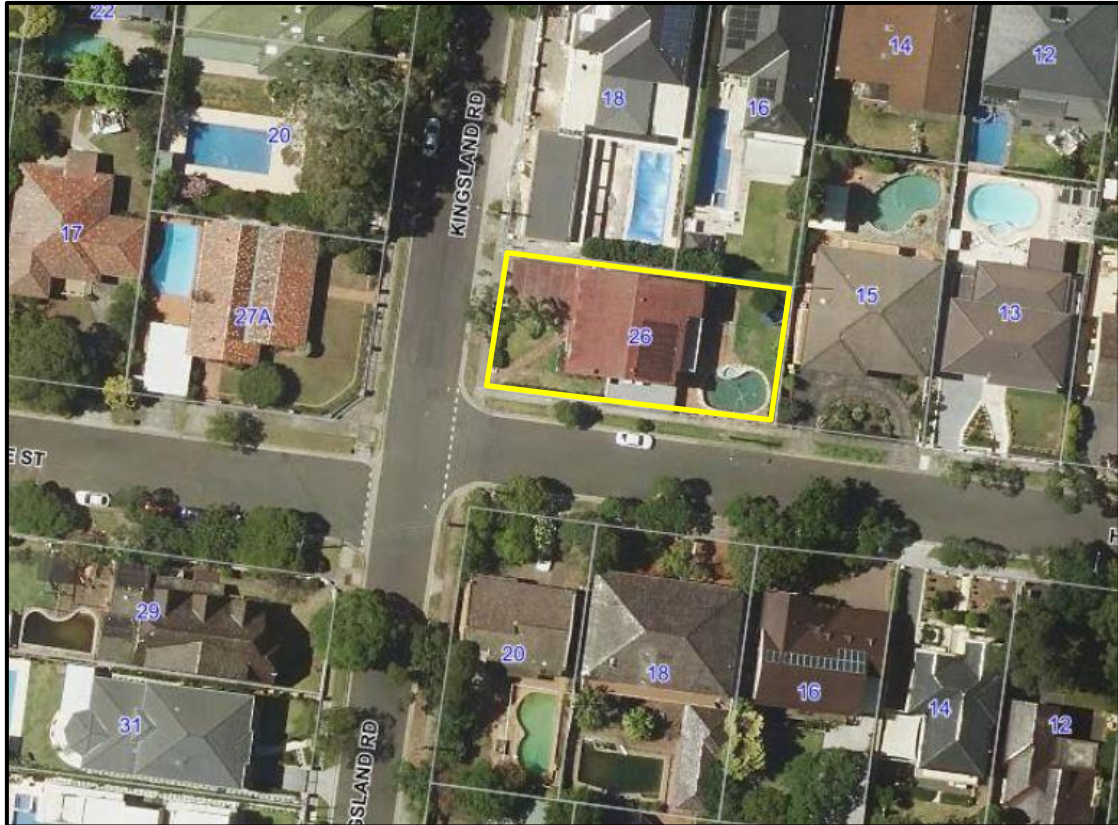


Figure 1: The subject site (as highlighted in yellow) and the surrounding context.

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Figure 2: Front façade of existing dwelling featuring mansard roof and dormer windows.



Figure 3: Rear (eastern) elevation of existing dwelling. Note the first floor balcony.

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Figure 4: Existing pool in south-eastern corner of the rear yard.



Figure 5: Two (2) storey residence at No. 15 Highgate Street.

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Figure 6: Two (2) storey residence at No. 20 Highgate Street.



Figure 7: Two (2) storey residence at No. 18 Highgate Street.

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Figure 8: Single storey residence at No. 27A Kingsland Road.

PROPERTY BURDENS AND CONSTRAINTS

There are no easements or burdens on the land, which could affect, or be affected by, the proposed development.

DESCRIPTION OF THE PROPOSED DEVELOPMENT

The application seeks Council approval for the demolition of an existing dwelling and construction of a two (2) storey dwelling with a basement level.

The specific elements of the proposed development are:

- Basement level containing:
 - A garage with two (2) vehicular spaces,
 - Stairwell and lift facilities,
 - Plant room, and
 - Rainwater tank and pool equipment room (refer to Figure 9);
- Ground floor comprising:
 - Living and family rooms,
 - Study room,
 - Dining room,
 - Kitchen,
 - Alfresco and BBQ facilities,
 - Laundry,
 - Water closet, and
 - Lift and stairwell vertical transport facilities (refer to Figure 10);
- First floor comprising:
 - Master bedroom with ensuite,
 - One (1) bedroom with walk in robe,

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- Three (3) bedrooms,
- Two (2) bathrooms,
- One (1) balcony on the western side, one balcony on the south-eastern side, and one (1) balcony on the southern side, and
- Spaces for a lift, stairwell and laundry chute (refer to Figure 11); and
- **External works** involving the removal of the existing Queen Palm (*Syagrus romanzoffiana*) in the front setback, retention of the existing pool and boundary fencing along the northern and eastern sides, the retention of the high masonry wall fence along Highgate Street, and the creation of deep soil landscaped areas (garden beds and grassed turf areas) and decorative gravel areas. New front fences are also proposed along Kingsland Road and Highgate Street.

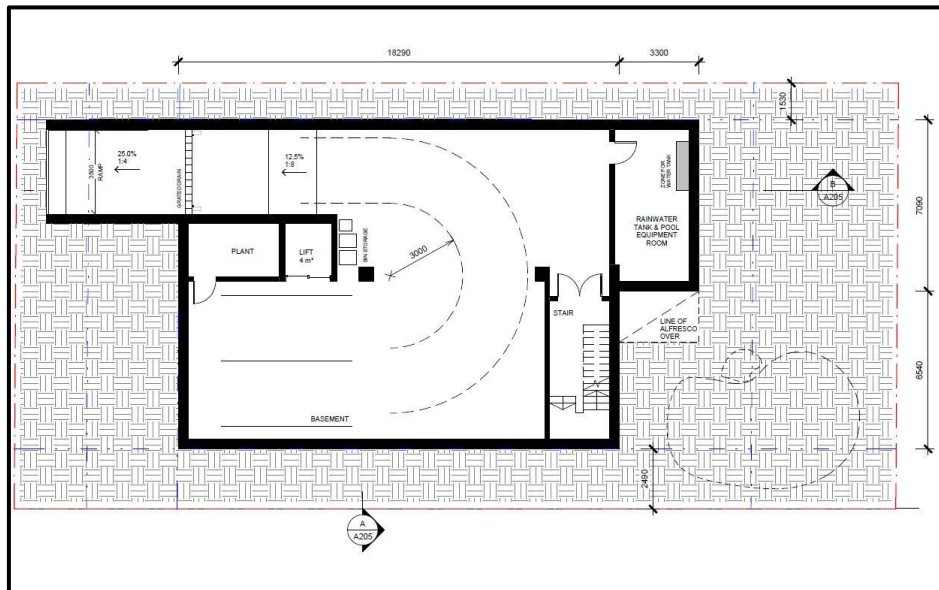


Figure 9: Basement level of new dwelling.

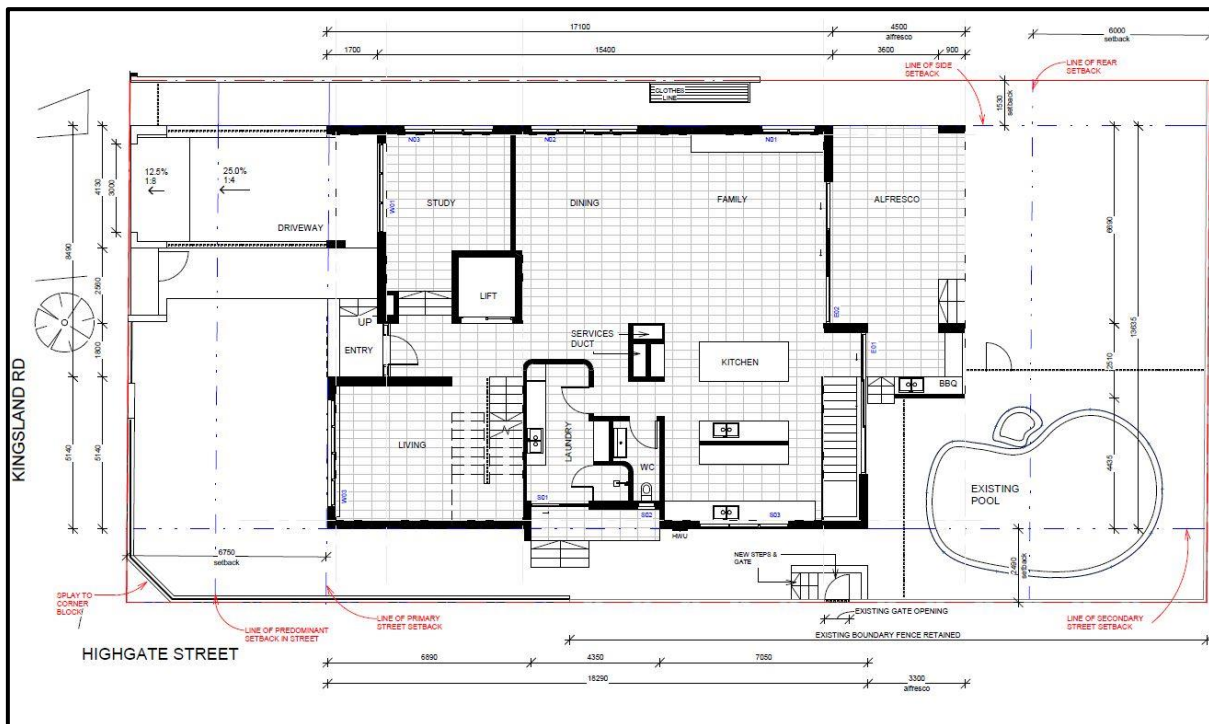


Figure 10: Ground floor of new dwelling.

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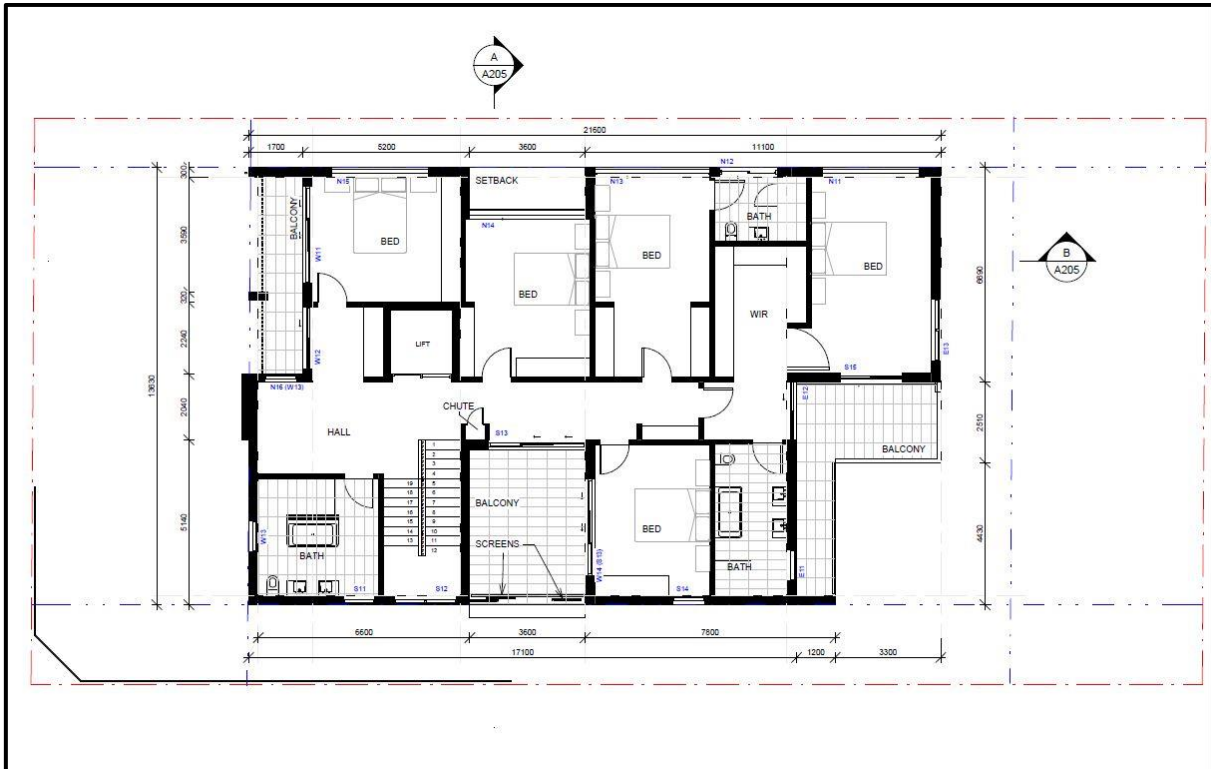


Figure 11: First floor of new dwelling.

Elevations of the western and southern extents of the new dwelling are shown in Figures 12 and 13 below.

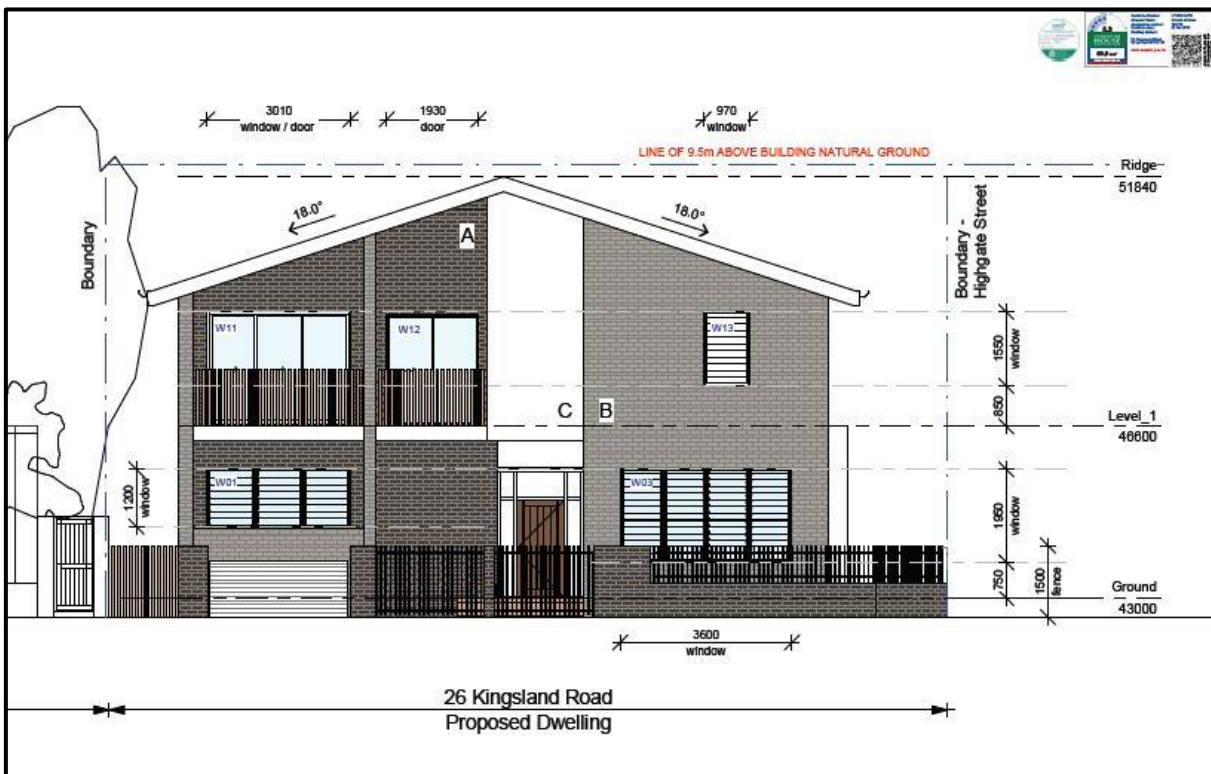


Figure 12: Eastern elevation of new dwelling.

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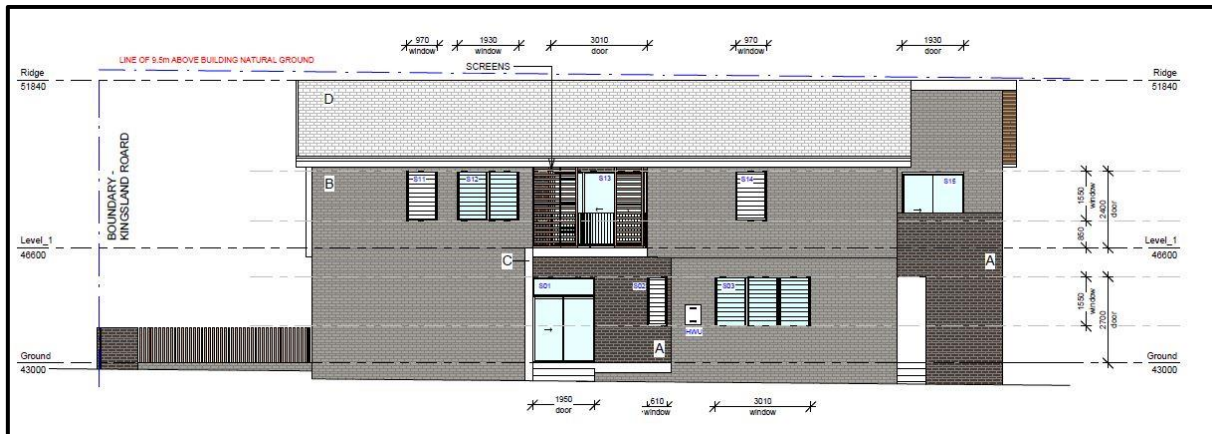


Figure 13: Southern elevation of new dwelling.

REFERRALS

INTERNAL REFERRALS

Stormwater Engineer Comments

The application was referred to Council's Stormwater Engineer who undertook an assessment of the proposal in regards to stormwater management and advised that there are no objections to the proposal, subject to the imposition of recommended conditions of consent.

SECTION 4.15 CONSIDERATIONS – EP&A Act, 1979

In determining a development application, the consent authority is to take into consideration the following matters within Section 4.15 of the *Environmental Planning and Assessment Act 1979* as relevant to the application:

4.15(1)(a)(i) The provisions of any environmental planning instrument

STATE ENVIRONMENTAL PLANNING POLICY (SEPP) – BASIX 2004

In accordance with the BASIX SEPP all new housing in NSW is required to meet a designated target for energy and water reduction.

A BASIX Certificate was submitted as part of the application, which indicates that the proposal meets the required reduction targets. An appropriate condition of consent will be imposed to ensure future compliance with these targets.

STATE ENVIRONMENTAL PLANNING POLICY NO. 55 – REMEDIATION OF LAND (SEPP 55)

SEPP 55 applies to the land and pursuant to Section 4.15 is a relevant consideration. A review of the available history for the site gives no indication that the land associated with this development is contaminated. There are no historic uses that would trigger further site investigations. It is noted that there is currently a dwelling house within the site and this use has been in existence since at least 1943.

The objectives outlined within SEPP55 are considered to be satisfied.

STATE ENVIRONMENTAL PLANNING POLICY (VEGETATION IN NON-RURAL AREAS) 2017

State Environmental Planning Policy (Vegetation in Non-Rural Areas) 2017 replaces the repealed provisions of clause 5.9 of SLEP 2012 relating to the preservation of trees and vegetation.

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The intent of this SEPP is consistent with the objectives of the repealed Standard where the primary aims/objectives are related to the protection of the biodiversity values of trees and other vegetation on the site.

Given the nature of the proposal involving no removal of any significant canopy trees, a referral to Council's Tree Coordinator was not required. The proposed removal of the existing Queen Palm (*Syagrus romanzoffiana*) is supported and acceptable; particularly given that the proposal provides opportunity for additional and high quality deep soil landscaping treatments. This includes additional native trees and plantings that will make a positive contribution to the streetscape and local setting. Conditions will be imposed to ensure the removal of the tree will be compensated by appropriate replacement tree planting.

STRATHFIELD LOCAL ENVIRONMENTAL PLAN 2012 (SLEP 2012)

An assessment of the proposal against the aims of SLEP 2012 is as follows:

Cl. 1.2(2) Aims	Complies
(a) <i>To achieve high quality urban form by ensuring that new development exhibits design excellence and reflects the existing or desired future character of particular localities and neighbourhoods in Strathfield</i>	Yes
(b) <i>To promote the efficient and spatially appropriate use of land, the sustainable revitalisation of centres, the improved integration of transport and land use, and an appropriate mix of uses by regulating land use and development</i>	Yes
(c) <i>To promote land uses that provide a wide range of employment, recreation, retail, cultural, service, educational and other facilities for the local community</i>	Yes
(d) <i>To provide opportunities for economic growth that will enhance the local community</i>	Yes
(e) <i>To promote future development that integrated land use and transport planning, encourages public transport use, and reduced the traffic and environmental impacts of private vehicle use</i>	Yes
(f) <i>To identify and protect environmental and cultural heritage</i>	Yes
(g) <i>To promote opportunities for social, cultural and community activities</i>	Yes
(h) <i>To minimise risk to the community by identifying land subject to flooding and restricting incompatible development</i>	N/A

Permissibility

The subject site is zoned R2 – Low Density Residential under the Strathfield Local Environmental Plan 2012 (SLEP 2012).

Dwelling houses and ancillary structures are permissible within the R2 – Low Density Residential zone with consent and is defined under SLEP 2012 as follows:

“Dwelling house means a building containing only one dwelling.”

The proposed development for the purpose of a dwelling and ancillary structures is consistent with the definition above and is therefore permissible with consent.

Zone Objectives

An assessment of the proposal against the objectives of the R2 – Low Density Residential zone is as follows:

Objectives	Complies
➤ To provide for the housing needs of the community within a low density residential environment.	Yes

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➤ To enable other land uses that provide facilities or services to meet the day to day needs of residents.	Yes
➤ To ensure that development of housing does not adversely impact the heritage significance of adjacent heritage items and conservation areas.	N/A

Comments: The proposal involves the demolition and replacement of an existing dwelling with a two (2) storey dwelling comprising a more contemporary design and finishes. This ensures that the housing needs of the community are met and preserves the low density, residential character of the immediate locality.

Part 4: Principal development standards

An assessment of the proposal against the relevant provisions of this part is as follows.

Height of building

Cl.	Standard	Controls	Proposed	Complies
4.3	Height of building	9.5m	9.5m	Yes
Objectives				Complies
(a)	To ensure that development is of a height that is generally compatible with or which improves the appearance of the existing area			Yes
(b)	To encourage a consolidation pattern that leads to the optimum sustainable capacity height for the area			Yes
(c)	To achieve a diversity of small and large development options.			Yes

Comments: The proposed development complies with the maximum building height development standard.

Floor space ratio (FSR)

Cl.	Standard	Controls	Proposed	Complies
4.4	Floor space ratio (FSR)	0.6:1 (386.92m ²)	0.6:1 (386.92m ²)	Yes
Objectives				Complies
(a)	To ensure that dwellings are in keeping with the built form character of the local area			Yes
(b)	To provide consistency in the bulk and scale of new dwellings in residential areas			Yes
(c)	To minimise the impact of new development on the amenity of adjoining properties			Yes
(d)	To minimise the impact of development on heritage conservation areas and heritage items			N/A
(e)	In relation to Strathfield Town Centre: i. to encourage consolidation and a sustainable integrated land use and transport development around key public transport infrastructure, and ii. to provide space for the strategic implementation of economic, social and cultural goals that create an active, lively and people-oriented development			N/A
(f)	In relation to Parramatta Road Corridor – to encourage a sustainable consolidation pattern that optimises floor space capacity in the Corridor			N/A

Comments: The proposed development complies with the maximum FSR development standard.

Part 5: Miscellaneous Provisions

Part 5 does not apply to the application.

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Part 6: Local Provisions

An assessment of the proposal against the relevant provisions of this part is as follows.

6.1 Acid sulfate soils

The subject site is identified as within Acid Sulfate Soils (ASS) – Class 5 land and the provisions of Clause 6.1 are applicable. The objectives of this clause are to ensure that development does not disturb, expose or drain acid sulfate soils and cause environmental damage. Within Class 5, the trigger under SLEP 2012 is works within 500m of adjacent Class 1,2,3 or 4 land that is below 5m AHD and by which the watertable is likely to lower the water table below 1m AHD on adjacent Class 1,2,3 or 4 ASS land. Given the subject land is not located within 500m of Class 1, 2, 3, and 4 land, the proposed development is not required to be accompanied by an Acid Sulfate Soils Management Plan, and therefore, satisfies the requirements of Clause 6.1.

6.2 Earthworks

The only element that requires more than 1m of excavation involves works associated with the proposed basement level and these works will be confined to the extent of the basement footprint. A condition of consent will be imposed to ensure an appropriate management of soils is undertaken during earthworks.

6.4 Essential services

Clause 6.4 of the SLEP 2012 requires consideration to be given to the adequacy of essential services available to the subject site. The subject site is located within a well serviced area and features existing water and electricity connection and access to Council's stormwater drainage system. As such, the subject site is considered to be adequately serviced for the purposes of the proposed development.

4.15(1)(a)(ii) The provisions of any draft environmental planning instruments

There are no applicable draft planning instruments that are or have been placed on public exhibition, to consider as part of this assessment.

4.15(1)(a)(iii) The provisions of any development control plan

STRATHFIELD CONSOLIDATED DEVELOPMENT CONTROL PLAN 2005 (SCDCP 2005)

An assessment of the proposal against the relevant provisions of this development control plan is as follows.

PART A – DWELLING HOUSES AND ANCILLARY STRUCTURES

2: Architectural Design & Streetscape Presentation

2.1	Objectives	Satisfactory
A.	To ensure that development respects the predominant height, scale, character, type, form, colour, materials and architectural qualities of the existing dwelling house (in the case of alterations and additions) and surrounding neighbourhood especially any adjoining or nearby heritage item or heritage conservation area.	Yes
B.	To achieve quality architecture in new development through the appropriate composition and articulation of building elements.	Yes
C.	To ensure that the dominant building rhythm of the streetscape is reflected in the building design in terms of the spacing and proportion of the built elements.	Yes

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D.	To ensure that new dwellings have facades, which define, address and enhance the public domain.	Yes
E.	To encourage contemporary architecture that is innovative, uses high quality detailing, and incorporates elements characteristic of Strathfield.	Yes
F.	To promote the continuance of pyramidal roof forms within Strathfield where they are already prevalent.	Yes
G.	To retain a feeling of openness and space between built elements by maintaining landscaped setbacks and preserve the appearance of dwellings set in the tree lined streets and park-like environment.	Yes
H.	To reduce the use of highly reflective colours and materials that create visual prominence.	Yes
I.	To ensure fencing is sympathetic to the design of the dwelling and the street and enhances the character of both the individual house and street whilst maintaining casual surveillance of the neighbourhood.	Yes
J.	To protect and retain the amenity of adjoining properties.	Yes

2.2 Development Controls		Complies	
<i>Streetscape Presentation</i>			
.1.	1	New dwellings address street frontage with clear entry.	Yes
	2	Consistently occurring building features integrated within dwelling design.	Yes
	3	Consideration of streetscape elements	Yes
	4	Integrated security grilles/screens, ventilation louvres and garage doors	Yes
<i>Scale, Massing & Rhythm of Street</i>			
.2.	1	Scale, massing, bulk and layout complement the existing streetscape and the dominant building rhythm	Yes
	2	Building height and mass maintains amenity to adjacent properties open space or the public domain	Yes
<i>Building Forms</i>			
.3.	1	Building form articulated.	Yes
	2	Dwellings on corner sites address both street frontages and articulated	Yes
<i>Roof Forms</i>			
	1	Roof form complements predominant form in the locality	Yes
	2	Roof form minimises bulk and scale of building and remains an important architectural element in the street.	Yes
	4	Roof structures are not visible from the public domain	Yes
<i>Materials</i>			
.4.	5	Materials compatible with the existing dwelling house, adjoining dwelling houses and the streetscape (type, form and colour)	Yes
	6	Monotone face brick walls and terracotta tiles for roofs where common in the streetscape	Yes
	7	New buildings and facades do not result in glare (Reflectivity Report may be required)	Yes
<i>Colours</i>			
	8	New development incorporates traditional colour schemes	Yes

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9	The external colours integrate harmoniously with the external design of the building	Yes
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Comments: The proposed development involves a new dwelling that features a pitched roof with dark grey roof tiles, a mixture of dark and light coloured, exposed brick finishes, some timber cladding, open balconies with metal balustrades and numerous glazing (window) treatments. Due to the subject site being a corner lot, the proposed development will be readily visible from Kingsland Road and Highgate Street. This design, combined with the proposed landscaping treatments and external works, is consistent and compatible with other dwelling houses occurring along Kingsland Road, Highgate Street and surrounds and is an appropriate response to the residential character of the surrounding area. The overall scheme has been sufficiently balanced through the provision of appropriate and fully compliant setbacks from all property boundaries, and deep soil landscaping treatments integrated in throughout the site.

The front setback has a number of new plantings to be established including two (2) trees that will assist in softening the built form. The road reserve along both Kingsland Road and Highgate Street has been mostly preserved and all street trees within these areas remain and will be unaffected. Additionally, the proposed development complies with all of the development standards and controls under the SLEP 2012 and SCDCP 2005 that relate to design, scale and mass. As the proposal maintains the existing land use as a single detached dwelling house it does not result in a significant change to the land use and built forms that typically occur within the R2 – Low Density Residential zone and along Kingsland Road and Highgate Street. Considering the above, the semi-modern design and scale of the new dwelling is appropriate and anticipated within the corner site featuring dual frontages and generous lot width.

Driveway access for the basement level will require some excavation and the construction of retaining walls that will be visible from the street. The landscaping treatments in the front setback will assist in countering these hardscape elements. The use of the existing driveway on the northernmost section of the front ensures that the driveway leading to the basement is hidden away to one side and will not have a dominant aesthetic viewed from the street.

In light of the above considerations, the proposed development, with the implementation of conditions, is consistent and compatible with the existing and future desired streetscape character of the immediate locality and has been sensitively designed with consideration of the site's context within a low density residential area. Therefore, the proposed development is considered acceptable and supportable.

4: Building Envelope

4.1	Objectives	Satisfactory
A.	To ensure that dwellings are compatible with the built form of the local area and that overall bulk and scale, size and height of dwellings relative to natural ground level responds to the adjoining dwellings, topography and desired future character.	Yes
B.	To minimise impact on the amenity of adjoining properties.	Yes
C.	To establish and maintain the desired setbacks from the street and define the street edge.	Yes
D.	To create a perception or reinforce a sense of openness in the locality.	Yes
E.	To maintain view corridors between dwellings	Yes
F.	To assist in achieving passive surveillance whilst protecting visual privacy.	Yes
G.	To provide a transitional area between public and private space.	Yes

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4.2 Development Controls		Complies	
<i>Floor Space Ratio</i>			
.1.	1	Floor Space Ratio permissible pursuant to SLEP 2012	Yes
	2	Development compatible with the lot size	Yes
<i>Building Height</i>			
.2.	1	Height of building permissible pursuant to SLEP 2012	Yes
	3	Dwelling houses and any ancillary structures 2-storeys (max)	Yes
	4	Building height responds to the gradient of the site to minimise cut and fill	Yes
<i>Street Setbacks</i>			
.3.1.	1	<p>Setbacks consistent with minimum requirements of Table A.1 Subclause 1 states a minimum street setback of 9m.</p> <p>Despite Subclause 1, a primary street setback of less than 9m may be considered where:</p> <ul style="list-style-type: none"> The predominant front setback in the street is less than 9m; The proposed setback is not less than the setback of the existing dwelling; or The proposed setback would not conflict with the character of the existing streetscape. <p>Minimum secondary street setback of 3m.</p>	<p>Yes</p> <p>6.75m (primary street)</p> <p>2.49m (secondary street setback)</p> <p>(refer to comments below)</p>
	<i>Side and Rear Setbacks</i>		
.3.2.	1	<p>A combined side setback of 20% of the width of the block (incorporating a 1.2m min side setback on each side).</p> <p>20% of 17.655m = 3.531m</p>	<p>Yes</p> <p>3.99m = 22.6%</p> <p>1.5m (northern side)</p> <p>2.49m (secondary street frontage)</p>
	2	A rear setback of 6m (min)	<p>Yes</p> <p>>6m</p>
	4	Garages and carports setbacks consistent with Table A.2	Yes

Comments: The predominant front setback of the immediate vicinity surrounding the site and along Kingsland Road and Highgate Street is less than 9m, as indicated by the front building lines of the following neighbouring residences:

- No. 27A Kingsland Road (across the road): 8m;
- No. 29 Kingsland Road (diagonally across the road): 5.9m;
- No. 31 Kingsland Road: 7.8m;
- No. 15 Highgate Street (adjoining): 7.6m;
- No. 18 Highgate Street (across the road): 5.6m; and
- No. 20 Highgate Street (across the road): 8.2m.

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Further to this, the existing dwelling within the subject site features the following front setbacks:

- Double garage: 1.3m;
- Enclosed porch area: 6.5m; and
- First floor wall: 9.7m.

The proposed dwelling will have a minimum front setback of 6.75m and will have considerable articulation and recessing of both ground and first floors incorporated into the front façade (western elevation). The setback and façade design are consistent and compatible with the front building line and streetscape presentation of existing residences within the immediate locality, and particularly dwelling houses along Kingsland Road and Highgate Street that have similar dual street frontages. The minimum front setback of the proposed dwelling is also not less than the existing dwelling's front setbacks.

Given the above considerations, the reduced setback is considered acceptable and supportable and will have negligible impacts on the existing and future desired streetscape character of the surrounding area and within Kingsland Road.

It is noted that the proposed dwelling will also have a reduced secondary street setback of 2.49m, which is a variation to the minimum requirement of 3m. Under Table A.1 of Part A of the SCDP 2005, this requirement applies if the main entrance to the dwelling is from the secondary frontage. Given that the main entrance of the proposed dwelling is from the primary street frontage (i.e. Kingsland Road), the reduced setback is considered supportable. It is noted that despite the 2.49m setback, the resultant combined side setback of the proposal is more than the required 20%. In addition, a similarly orientated allotment (No. 29 Kingsland Road) contains a dwelling house with a similar and reduced secondary street setback (2.5m) – ensuring that the proposed dwelling's secondary street setback is generally consistent with the streetscape and surrounding area. Therefore, the reduced setback from Highgate Street is considered appropriate and acceptable in this instance.

5: Landscaping

5.1	Objectives	Satisfactory
A.	To encourage landscaping that is appropriate to the style and scale of the dwelling and adjoining development, and to the streetscape.	Yes
B.	To enhance the existing streetscape and promote a scale and density of planting that softens the visual impact of buildings, structures, vehicle circulation and ancillary areas.	Yes
C.	To preserve existing landscape elements on site (such as existing trees) and encourage their integration into the design of proposals.	Yes
D.	To ensure adequate deep soil planting is retained on each allotment.	Yes
E.	To ensure developments make an equitable contribution to the landscape setting of the locality.	Yes
F.	To ensure both existing and new landscaping provides suitable shade and facilitates convective cooling breeze paths in summer.	Yes
G.	To encourage the use of native flora such as open woodland canopy trees, to provide a habitat for native fauna.	Yes
H.	To ensure that landscaped areas are designed to minimise water use.	Yes
I.	To provide functional private open spaces for active or passive use by residents.	Yes
J.	To provide private open areas with provision for clothes drying facilities screened from the street and lane or a public place.	Yes

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K.	<i>To ensure the protection of trees during construction</i>	Yes
L.	<i>To ensure suitable fencing is provided to reduce acoustic impacts and enhance visual privacy between neighbouring residents whilst enabling front fences passive surveillance of the street.</i>	Yes
M.	<i>To maximise the amenity of existing and proposed developments, including solar access, privacy and open space.</i>	Yes

5.2 Development Controls

Complies

<i>Landscaped area</i>			
.1.	1	Landscaped area in accordance with Table A.3 Minimum required: 41.5% (267.62m ²)	Yes – conditioned to comply 42% (270.7m²) (refer to comments below)
	2	At least 50% of the minimum landscaped area located behind the building line to the rear boundary	Yes
	3	At least 50% of the front yard maintained as deep soil soft landscaping	Yes
	4	Minimise hard surface area (concrete/brick/stone paving and bitumen). Run-off directed to permeable surfaces.	Yes
	5	Planting areas soften the built form	Yes
	6	Front gardens respond and contribute to the garden character of Strathfield.	Yes
	7	Retain and reinforce the prevailing streetscape and surrounding locality	Yes
	8	Plant species must be retained, selected and planted to improve amenity	Yes
<i>Tree Protection</i>			
.2.	3	Development provides for the retention and protection of existing significant trees	Yes
	6	Opportunities for planting new canopy trees within the front setback	Yes
	7	At least one (1) canopy tree provided in the rear yard.	Yes
	8	Trunk of a proposed canopy tree planted 4m (min) from built structures, or 3m (min) from pier beam footings	Yes
	9	Planting on side boundaries a 0.6m (min) deep root deflection barrier provided 1.5m (min) either side of the tree center	Yes
	10	Driveway construction does not result in the removal, lopping or root damage to any street tree	Yes
	11	25% (min) of canopy trees and other vegetation shall be locally sourced indigenous species	Yes
<i>Private Open Space</i>			
.3.	1	Provided in a single parcel rather than a fragmented space, directly accessible from internal living areas	Yes
	2	Includes a deep soil area compliant with the minimum landscaped area.	Yes
	3	Terraces and decks (at least 10m ²) with one length or width 3m (min) and directly accessible from an internal living area. Decks cannot be located more than 500mm above NGL	Yes

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4	Unless 3m (min) width, areas within setbacks are not to be included as private open space	Yes
5	Private open space located at the rear of the property.	Yes
<i>Fencing</i>		
1	Fencing designed to be compatible and sympathetic to the style of the dwelling, adjoining properties and the streetscape	Yes
2	Landscaping used when the streetscape is characterised by the absence of front fences	Yes
3	Fencing forward of FBL shall not exceed 1.5m. Solid components shall not exceed 0.7m above NGL with the exception of brick piers	Yes
4	Solid fencing 1.8m (max) is permitted along a secondary street frontage to enclose a private open space	Yes
5	Side and rear fences limited to 1.8m (max)	Yes
6	Side fences forward of the FBL taper down to the front fence.	Yes
7	Front fences visually permeable	Yes
8	Front fences on busy roads designed to provide acoustic attenuation	Yes
9	Listed undesirable materials and finishes not used forward FBL	Yes
10	Corner allotments incorporate a 1.5m x 1.5m (min) splay adjacent to the road intersection	Yes
11	Solid fences adjoining driveways are provided with 1m x 1m (min) splay	Yes
12	A splay adjacent to a road intersection or driveway entrance must be landscaped	Yes
13	Significant trees maintained	Yes
14	Stormwater flows through or under fencing on sloping sites	Yes

Comments: The proposed landscaping treatments involve garden beds and grassed turf areas. New plantings to be established within garden beds include native groundcovers and shrubs such as Native Ginger (*Alpinia caerulea*), Pigface (*Carpobrotus* species), Dwarf Lomandra (*Lomandra* 'Tanika') and Lillypilly (*Syzygium australe* 'Resilience'), two (2) trees in the front setback – Native Frangipani (*Hymenosporum flavum*) and a tree in the rear yard – Water Gum (*Tristaniaopsis laurina*).

The proposed decorated gravel areas indicated on the Landscape Plan are not acceptable as these contribute to the deficiency in deep soil landscaped areas. Conditions are recommended to delete the decorated gravel areas and replacing these with deep soil landscaped areas such as grassed turf areas and garden beds. The conditions recommended will also ensure that the proposed development complies with the minimum landscaped area requirement. In general, the deep soil landscaping treatments involved in the proposal are acceptable and supportable as these feature numerous native plantings, including at least two (2) trees in the front setback and one (1) tree in the rear private open space as well as screen planting along the eastern boundary. This planting will provide adequate privacy for both the rear yard/pool area and for the adjoining neighbour (No. 15 Highgate Street).

The proposed fencing along the northern and eastern sides will have a maximum height of 1.5m (solid component being 0.7m high), comprise of a vegetated splay at the corner of Kingsland Road and Highgate Street, and will be constructed of dark coloured, exposed brick and open, vertical metal palisade inserts. It is noted that the new fence along Highgate Street connects with the retained masonry wall so that the privacy and security of the rear yard is maintained. The proposed fencing has a design that matches the new dwelling, provides appropriate streetscape

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presentation, privacy and security whilst defining pedestrian and vehicular access and complying with the relevant controls under the SDCDP 2005. Therefore, the new fencing is considered acceptable.

6: Solar Access

6.1 Objectives		Satisfactory
A.	To ensure the design of new dwelling houses and alterations and additions maximises solar access to living areas and open space areas.	Yes
B.	To minimise overshadowing of adjoining properties.	Yes
6.2 Development Controls		Complies
Sunlight Access		
.1.	1 New dwellings - 3 hours solar access between 9.00am and 3.00pm on June 21 to the windows of habitable rooms and 50% of private open space	Yes
	3 50% of the principal private open space of any adjoining premises receives 3 hours solar access between 9.00am and 3.00pm on June 21	Yes
	4 The proposed development does not further reduce the amount of solar access	Yes

Comments: The proposed development will have minimal overshadowing impacts. The location and maximum height of the proposed dwelling combined with the subject site being a corner allotment with a west-east orientation ensure that the private open space and living room windows of adjoining properties will receive at least 3 hours of solar access during mid-winter. Thus, the proposed development meets the relevant solar access requirements under the SDCDP 2005.

7: Privacy

7.1 Objectives		Satisfactory
A.	Development that is designed to provide reasonable privacy to adjacent properties	Yes
B.	To maintain reasonable sharing of views from public places and living areas	Yes
C.	To ensure that public views and vistas are protected, maintained and where possible, enhanced	Yes
D.	To ensure that canopy trees take priority over views	Yes
E.	To ensure that the siting and design of buildings minimises noise impacts from abutting roads, rail corridors and other noise-generating land uses	Yes
7.2 Development Controls		Complies
Visual Privacy		
.1.	1 Protect POS, bedrooms, balconies and living rooms of proposed and any existing adjoining dwellings from direct overlooking	Yes
	2 Provide adequate separation of buildings	Yes
	3 Ensure elevation of finished floor levels above NGL is not excessive	Yes
	4 Improve privacy to adjacent properties with screen planting	Yes
.2.	Windows	

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1	Windows do not directly face the windows, balconies and courtyards of adjoining dwelling	Yes
2	A window within 9m of another window in a habitable room of an adjoining dwelling is offset by 0.5m (min) or a sill height of 1.7m (min) above the FFL	Yes
3	Windows directly facing balconies or courtyards are narrow, incorporate obscure glazing and/or a sill height of 1.7m (min) above FFL	Yes
<i>Elevated Decks Verandahs and Balconies</i>		
1	Elevated decks, verandahs and upper storey balconies not permitted on side boundaries (exceptions apply)	Yes
		No
2	Elevated decks, verandahs and balconies incorporate privacy screens	(refer to comments below)
.3.		
3	Rear balconies (no more than 1m (depth) x 2m (length) permitted if the balcony does not unreasonably impact on adjoining premises	No (refer to comments below)
4	Balconies extending the full width of the front façade are not permitted	Yes

Comments: The proposed development will not generate significant privacy and amenity impacts given that most of first floor windows of the new dwellings are from low utility rooms such as bedrooms, bathrooms, hallways and stairwells. It is noted that the stairwell windows face Highgate Street. The proposed dwelling features three (3) balconies on the first floor. The following matters are considered:

- A front facing balcony is proposed on the western side of the dwelling and will be accessed off a bedroom and hallway. This balcony is less than 1.7m wide, has a solid wall on the northern side and faces Kingsland Road. This balcony does not face private open spaces and will provide street surveillance along this road.
- A south facing balcony is proposed to be accessed from a bedroom and hallway. This balcony is recessed centrally within the southern façade and comprises privacy screens, and faces Highgate Street. This balcony does not face private open spaces and will provide street surveillance along this road.
- A rear facing balcony is proposed on the eastern side of the dwelling and will be accessed off a bedroom and hallway. This balcony is concentrated to the south-eastern portion of the first floor, is L-shaped, has a varying width of 1.2m to 2.51m and has a minimum setback of more than 6m from the rear (eastern) boundary. This balcony does face the side of the east-adjointing neighbour (No. 15 Highgate Street). In order to minimise potential privacy and acoustic impacts associated with this space, a condition is recommended to reduce the width to be a maximum of 1.2m. The design of this balcony, as conditioned, will not directly overlook this neighbour's private open space and will provide additional street surveillance upon Highgate Street. In context of the site and its surrounds, the rear facing balcony (subject to conditions) is acceptable.

In considering the above matters, the proposed first floor balconies in the new dwelling will have minimal privacy impacts and provide sufficient visual relief and break-up of the built form, and are therefore, considered acceptable and supportable.

Due to the slight fall of the site towards the rear, the alfresco area is slightly elevated from natural ground level (0.55m). However, the majority of the finished floor levels of the ground floor are predominantly on or close to the natural ground level. Further, the generous setbacks of the dwelling from property boundaries ensures that any overlooking from the ground floor will be minimal.

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The proposed development involves no changes to the natural ground levels within the rear yards or any areas outside the building footprint of the new dwelling and the existing pool. By retaining these levels and by providing planting within the garden beds along the property boundaries, the proposal is able to achieve adequate privacy for private open spaces whilst maintaining privacy and amenity for the neighbouring residences. Given the above, the proposal will not generate significant privacy and amenity impacts and is acceptable and supportable in this regard.

8: Vehicle Access and Parking

8.1	Objectives	Satisfactory
A.	<i>To provide adequate and convenient on-site car parking.</i>	Yes
B.	<i>To ensure that the location and design of driveways, parking spaces and other areas used from the movement of motor vehicles are efficient, safe and convenient.</i>	Yes
C.	<i>To ensure garages, carports, basements and hard standing areas for cars do not visually dominate the street façade of the dwelling.</i>	Yes
D.	<i>To ensure that construction materials used for driveways respect the architectural qualities of the dwelling.</i>	Yes
E.	<i>To minimise the area of access driveways to minimise impermeable surfaces and maximise landscaped areas.</i>	Yes
F.	<i>To ensure basements have discreet entries, safe access and a high degree of natural cross-ventilation.</i>	Yes
G.	<i>To minimise excavation to reduce disturbance to natural ground level particularly adjacent to site boundaries.</i>	Yes
H.	<i>To ensure that any proposed basement minimises disturbance to natural drainage systems and that flooding, drainage or ventilation impacts would not be created for the site, or for adjoining or nearby properties.</i>	Yes
8.2	Development Controls	Complies
.1.	<i>Driveway and Grades</i>	
	1 Existing driveways must be used (exceptions apply)	Yes
	2 The width of driveways at the property boundary is to be 3m	Yes
	3 The edge of driveway crossings located 1m (min) clear of any existing stormwater pits or poles and 2m clear of tree trunks	Yes
	4 Parking and access points easily accessible and recognisable, non-disruptive to pedestrian flow and safety and located to minimise traffic hazards and potential for vehicles to queue.	Yes
	5 One (1) vehicular crossing (max) to any public road (exceptions apply)	Yes
	6 Vehicles accessing sites which front main roads shall be capable of entering and exiting in a forward direction	Yes
	7 Vehicular turning areas for garages complies with relevant Australian Standard	Yes
	8 Rear lane / secondary street frontage - parking and access provided from the secondary street/lane	No (refer to comments below)
	9 Driveways avoid long and straight appearance by using variations and landscaping	Yes
	10 Driveway set back 0.5 metres (min) from side boundaries	Yes
11 Driveways incorporate unit paving into the design	Yes	

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	12	Areas of concrete visible from a public road are to be minimal and coloured charcoal, grey or brown	Yes
	13	Coloured concrete is not permitted in the driveway crossing outside the property boundary	Yes
	<i>Garages, Carports and Car Spaces</i>		
.2.	1	Two (2) car parking spaces required behind the FBL of all new dwellings (exceptions apply)	Yes
	2	Garages recessed behind the main front facade and/or non-dominant	Yes
	3	Garages not more than 150mm above NGL at entry unless the slope of the site exceeds 1:8 (12.5%)	Yes
	4	Dimensions of parking spaces and garages comply with the Australian Standards	Yes
	5	Garages are not to be converted or used for any purpose other than that for which they are approved	Yes
	<i>Basements</i>		
.3.	1	The area of a basement shall be limited to and contained within the ground level footprint of the dwelling	Yes
	2	Excavation not permitted within the minimum side setbacks.	Yes
	3	The maximum height of the basement above NGL is limited to 1m measured to the predominant finished floor level of the level above.	Yes
	4	Internal clearance of 2.2m (min)	Yes
	5	Driveways have a maximum 1:4 gradient and comply with Australian Standards	Yes
	6	Basement entries and ramps/driveways not greater than 3.5m wide	Yes
	7	Driveway ramps are perpendicular to the property boundary at the street frontage	Yes
	8	Basements permit vehicles to enter and exit the basement in a forward direction	Yes
	10	Pump-out systems and stormwater prevention in accordance with Council's Stormwater Management Code	Yes
	11	Basements are not to be used for habitable purposes	Yes

Comments: The subject site benefits from two (2) street frontages on Kingsland Road and Highgate Street. Both are low order, local roads with comparable traffic levels. The existing dwelling currently obtains access off Kingsland Road, with its private open space configured along the rear (eastern) and southern portions of the lot. The proposed dwelling mirrors the same vehicular and pedestrian access and has the same configuration for its private open space. This means that the new dwelling contravenes the above control of preferring corner allotments to achieve access from the secondary street. It is noted, however; that the intention of this control is to minimise vehicular access on streets or roads that typically are exposed to greater traffic levels than streets on lower order roads. Given that both Kingsland Road and Highgate Street are similar in terms of being local roads and traffic levels, the provided vehicular access from Kingsland Road is considered acceptable and supportable. Further, it maintains the existing situation and it ensures no additional crossings are required and no street trees will be impacted.

9: Altering Natural Ground Level (Cut and Fill)

9.1	Objectives	Satisfactory
A.	To maintain existing ground levels and minimise cut and fill to reduce site disturbance.	Yes

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B.	<i>To ensure existing trees and shrubs are undisturbed, ground water tables are maintained and impacts on overland flow/drainage are minimised.</i>		Yes
9.2	Development Controls		Complies
1	Fill limited to 1m (max) above NGL		Yes
2	Clean fill used only		Yes
3	Cut and fill batters stabilised consistent with the soil properties		Yes
4	Vegetation or structural measures are implemented when the site is disturbed.		Yes
5	Areas of excavation setback from property boundaries in line with building setback controls. No excavation permitted within the minimum required setbacks.		Yes
6	The work does not affect or undermine the soil stability or structural stability of buildings and Council assets on adjoining properties.		Yes
7	A dilapidation report for all buildings which adjoin proposed excavation areas as required		Yes
8	Avoid excessive fill that creates the potential for overlooking of adjoining properties		Yes

Comments: Apart from the excavation required for the basement level, the proposed development involves minimal changes to the existing natural ground levels of the site. Therefore, the proposal meets the relevant requirements under the SCDCP 2005 associated with cut and fill.

10: Water and Soil Management

10.1	Objectives		Satisfactory
A.	<i>To encourage the incorporation of Water Sensitive Urban Design (WSUD) and Botany Improvement Plan principles in the development.</i>		Yes
B.	<i>To ensure compliance with Council's Stormwater Management Code</i>		Yes
C.	<i>To ensure compliance with the NSW State Government's Flood Prone Lands Policy.</i>		Yes
D.	<i>To ensure that appropriate soil erosion and sediment control measures are implemented on all sites that involve soil disturbances during construction.</i>		Yes
E.	<i>To ensure new building work does not detrimentally affect the existing drainage system of any area of the Municipality.</i>		Yes
F.	<i>To ensure that new development in areas that may be affected by acid sulphate soils do not adversely impact the underlying ground conditions, soil acidity and water quality.</i>		Yes
G.	<i>To appropriately manage stormwater and overland flow to minimise damage to occupants and property</i>		Yes
10.2	Development Controls		Complies
.1.	<i>Stormwater Management and Flood Prone areas</i>		
	2	Compliance with Council's Stormwater Management Code	Yes
.2.	<i>Acid Sulfate Soils</i>		
	1	Site managed consistent with the provisions contained in Clause 6.1 SLEP 2012	Yes
.3.	<i>Soil Erosion and Sediment Control</i>		
	1	Soil erosion and sediment control measures detailed and implemented prior to the commencement of work.	Yes

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	2	Sediment control measures applied	Yes
	3	Plans provided detailing stormwater quality treatment	Yes

Comments: Council's Stormwater Engineer confirmed that the proposal complies with the relevant stormwater and water and soil management controls under the SCDCP 2005 and Council's Stormwater Management Code.

11: Access, Safety and Security

11.1	Objectives	Satisfactory
A.	To encourage the incorporation of crime prevention principles in the design of the proposed developments.	Yes
B.	To increase the safety and perception of safety in public and semi-public spaces.	Yes
C.	To provide passive surveillance of the public domain to promote a safe pedestrian environment whilst maintaining the privacy of residents	Yes
D.	To ensure the safety of pedestrians by separating pedestrian access from vehicular access.	Yes
11.2	Development Controls	Complies
	<i>Address and Entry Sightlines</i>	
.1.	1 Occupants able to overlook public places to maximise passive surveillance	Yes
	2 Landscaping design around dwellings and ancillary structures to accommodate plant maturation	Yes
	3 External lighting enhance safe access and security and light spill does not adversely impact on adjoining properties.	Yes
	4 Crime Prevention through Environmental Design (CPTED) principles incorporated.	Yes
	<i>Pedestrian Entries</i>	
.2.	1 Pedestrian entries and vehicular entries suitably separated	Yes
	2 Dwelling entrances easily identifiable	Yes
	3 House numbers are to be clearly visible from the street	Yes

Comments: The proposed dwelling will have clearly defined and identifiable pedestrian and vehicular access and entries. The front entrance of the new dwelling faces Kingsland Road and will be visible from this street. The overall scheme has been adequately designed to formalise access and ensures that the above access, safety and security requirements are met.

12: Ancillary Development

12.1	Objectives	Satisfactory
A.	To ensure that ancillary development is compatible with the design of the principal dwelling, streetscape and adjoining dwellings in form, materials and colours	Yes
B.	To limit the size, bulk and scale of ancillary structures and minimise their visibility from the public domain.	Yes
C.	To ensure that the provision of ancillary structures improves the amenity of residents whilst ensuring that the amenity of surrounding dwellings and neighbouring lots is maintained.	Yes
D.	To ensure that the provision of ancillary structures, such as air conditioning units, are considered at the design stage of a proposed development.	Yes

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12.2	Development Controls	Complies	
	<i>Air-conditioning units</i>		
.1.	1	Located away from the habitable rooms of adjoining properties and/or screened by acoustic treatments	Yes
	2	Any building work must not reduce the structural integrity of existing buildings	Yes
	3	Installation of residential grade air conditioners only	Yes
	4	Installation to comply with the Protection of the Environment Operations Act 1997 and Protection of the Environment Operations (Noise Control) Regulation 2008	Yes
	<i>Swimming Pools</i>		
.6.	1	Side and rear setbacks from the outside edge of the pool concourse are 1m (min) wide and comprise deep soil soft landscape area	Yes
	2	If greater than 1m above ground, the space between the bond beam/concourse and the ground is finished to Council's satisfaction	Yes
	3	The pool filter and pump equipment designed and located to not emit a noise level that exceeds 5dBA above the ambient background noise level measured at any property boundary. The pool equipment shall be located within an enclosed structure so as to not be readily visible	Yes
	4	Lighting positioned to prevent light spillage and minimise any nuisance to adjoining premises	Yes
	5	Enclosures shall comply with the Swimming Pools Act and relevant Australian Standards as amended.	Yes

Comments: The proposed development meets the above requirements.

13: Ecologically Sustainable Development

13.1	Objectives	Satisfactory	
A.	To encourage passive and active strategies in the design of dwellings and ancillary structures that promotes the achievement of ecologically sustainable practices and BASIX requirements.	Yes	
B.	To ensure dwellings are designed to allow sufficient natural ventilation and lighting whilst minimising heat gain during summer and maximising solar access during winter, thereby reducing the need for artificial cooling and heating.	Yes	
C.	To minimise the over use of Sydney's limited high quality domestic water supply by ensuring new dwellings incorporate water storage tanks for use in toilet flushing, landscape irrigation and to encourage new dwellings, additions and alterations to incorporate water saving devices and water conservation strategies.	Yes	
D.	To encourage the use of new technology that reduces energy consumption, minimises greenhouse emissions and results in cost savings	Yes	
E.	To encourage the re-use of building materials, thereby reducing waste to landfill, transportation costs, conserving raw materials and reducing energy expenditure. (Refer to Part H Waste Minimisation and Management of SCDCP 2005).	Yes	
13.2	Development Controls	Complies	
	<i>Natural Lighting and Heating</i>		
.1.	1	Living areas facing north, sleeping areas facing east/south, and utility areas orientated west/south to maximise winter solar access.	Yes
	2	Trees planted to the north of the dwelling must be deciduous to allow solar access during the winter	Yes
	3	Materials used of high thermal mass	Yes
	<i>Natural Cooling and Ventilation</i>		
.2.	1	Windows and walls on northern facades shaded by shading devices, trees, eaves	Yes

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	and louvres sympathetic to the dwelling		
	2	Windows positioned to capture breezes and allow for cross-ventilation	Yes
<i>Water Tanks</i>			
	1	Located behind the dwelling or behind the front building line and screened from view from the public domain	Yes
	2	Associated support structures and plumbing are a colour that complements the dwelling.	Yes
	3	Above ground water tanks located 450mm (min) from any property boundary	Yes
	4	Above ground water tanks do not exceed 3m in height above NGL	Yes
.3.	5	Overflow piped directly to the approved stormwater drainage system except in cases where stormwater is required to be directed to on-site stormwater detention (OSD) storage	Yes
	6	No part of the water tank or support stand may rest on a wall footing	Yes
	7	Installation does not involve the filling of more than 1m above existing ground level	Yes
	8	The tank not located over or adjacent to a water main or sewer main or installed over any associated structure or fittings	Yes
	9	Support structure installed to the requirements of a qualified practicing structural engineer or to the maker's specifications.	Yes
<i>Hot Water Heater Units</i>			
.4.	1	Located behind the dwelling or wholly behind the dwelling	Yes
	2	Not located on balconies unless screened from public view	Yes
	3	Placed within a short distance of the most frequent point of use	Yes

Comments: The proposed development complies with the relevant requirements under the SCDPC 2005 in relation to ecologically sustainable development and the design and siting of rainwater tanks and hot water heater units.

PART H - WASTE MANAGEMENT (SCDCP 2005)

A Waste Management Plan was submitted as part of the development application and has demonstrated compliance with the relevant controls.

4.15(1)(a)(iia) The provisions of any planning agreement or draft planning agreement

No planning agreement has been entered into under section 7.4 of the *Environmental Planning and Assessment Act 1979*.

4.15(1)(a)(iv) The provisions of the regulations

Clause 92 of the *Environmental Planning and Assessment (EP&A) Regulation 2000* requires Council to take into consideration the provisions of the Australian Standard *AS2601-1991: The Demolition of Structures*, in the determination of a development application.

Having regard to these prescribed matters, the proposed development involves the demolition of a building for the purposes of *AS 2601 – 1991: The Demolition of Structures*.

Should this application be approved, appropriate conditions of consent are included within the recommended to ensure compliance with any relevant regulations.

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4.15(1)(b) *The likely impacts of the development, including environmental impacts on both the natural and built environments, and social and economic impacts in the locality*

The proposed development minimises adverse impacts on adjoining properties and achieves an appropriate level of streetscape compatibility. The proposed development is not considered to result in any significant environmental and social impacts.

4.15(1)(c) *The suitability of the site for the development*

The proposed development is considered to be suitable to the site in that it preserves the low density residential use of the site and the character of immediate locality. The subject site does not yield any significant environmental constraints that would prevent development of a single dwelling. Therefore, the subject site is suitable for the proposed development.

4.15(1)(d) *Any submissions made in accordance with this Act or the regulations*

The application was publicly notified in accordance with the provisions of Part L of SCDCP 2005. No submissions were received as a result.

4.15(1)(e) *The public interest*

The public interest is served through the detailed assessment of this development application under the relevant local planning controls and legislation and consideration of any submissions received relating to it by Council. The proposed development is not considered to be contrary to the public interest.

LOCAL INFRASTRUCTURE CONTRIBUTIONS

Section 7.13 of the *Environmental Planning and Assessment Act 1979* relates to the collection of monetary contributions from applicants for use in developing key local infrastructure. This section prescribes in part as follows:

A consent authority may impose a condition under section 7.11 or 7.12 only if it is of a kind allowed by, and is determined in accordance with, a contributions plan (subject to any direction of the Minister under this Division).

STRATHFIELD 7.12 INDIRECT CONTRIBUTIONS PLAN

Section 7.12 Indirect Contributions are applicable to the proposed development in accordance with the Strathfield Indirect Development Contributions Plan as follows:

Local Amenity Improvement Levy: **\$11,500.00**

CONCLUSION

Having regard to the relevant matters for consideration under Section 4.15 of the *Environmental Planning and Assessment Act 1979* the proposal is considered to be acceptable on its merits and is recommended for approval subject to conditions of consent.

PEER REVIEW

The content and recommendation of the development assessment report has undergone peer review and is satisfactory for consideration by the Panel.

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RECOMMENDATION

That Development Application No. DA2019/181 for demolition of existing buildings and construction of a two (2) storey dwelling with basement level at 26 Kingsland Road, Strathfield be **APPROVED**, subject to the following conditions:

SPECIAL CONDITIONS (SC)**1. MODIFICATION TO LANDSCAPE PLAN – DELETION OF DECORATIVE GRAVEL (SC)**

All areas annotated on the approved Landscape Plan as decorative gravel must be deleted and converted into deep soil landscaped areas (such as grassed turf areas or garden beds).

This modification must be included and reflected in the revised set of Architectural Plans, Landscape Plans and Stormwater Plans, and any other documentation that are to be submitted and approved by the Principal Certifying Authority prior to the issue of any Construction Certificate.

(Reason: To ensure compliance with the minimum landscaped area requirement under SDCDP 2005.)

2. REDUCE BALCONY SIZE (SC)

The rear (east) facing balcony on the first floor that is accessed from a bedroom and hallway must be reduced in width to have a maximum width of 1.2m.

This modification must be included and reflected in the revised set of Architectural Plans, Landscape Plans and Stormwater Plans, and any other documentation that are to be submitted and approved by the Principal Certifying Authority prior to the issue of any Construction Certificate.

(Reason: To minimise privacy and acoustic impacts associated with this balcony.)

GENERAL CONDITIONS (GC)**3. APPROVED PLANS AND REFERENCE DOCUMENTATION (GC)**

The development, except where modified by a condition(s) of consent, is to be carried out in accordance with the following plans and reference documentation:

Plans affixed with Council's 'Development Consent' stamp relating to Development Consent No. DA2019/181:

Drawing No.	Title/Description	Prepared by	Issue / Revision & Date	Date received by Council
A100	Site Plan & Site Analysis	LSA	Revision No. 1 Oct 2019	31 Oct 2019
A101	Plan – Ground Floor	LSA	Revision No. 2 Dec 2019	12 Dec 2019
A102	Plan – First Floor	LSA	Revision No. 2 Dec 2019	12 Dec 2019
A103	Plan – Basement	LSA	Revision No. 2 Dec 2019	12 Dec 2019
A104	Plan – Roof	LSA	Revision No. 1 Oct 2019	31 Oct 2019

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A201	Elevation (West) – Front	LSA	Revision No. 1 Oct 2019	31 Oct 2019
A202	Elevation – South (Second Frontage)	LSA	Revision No. 1 Oct 2019	31 Oct 2019
A203	Elevation – East (Rear)	LSA	Revision No. 1 Oct 2019	31 Oct 2019
A204	Elevation – North (Side)	LSA	Revision No. 1 Oct 2019	31 Oct 2019
A205	Section A & B	LSA	Revision No. 1 Oct 2019	31 Oct 2019
A206	Ramp Detail	LSA	Revision No. 1 Oct 2019	31 Oct 2019
A300	Colours & Materials	LSA	Revision No. 1 Oct 2019	31 Oct 2019
A302	Demolition Plan	LSA	Revision No. 1 Oct 2019	31 Oct 2019
A303	Sediment and Erosion control plan	LSA	Revision No. 1 Oct 2019	31 Oct 2019
L/01	Landscape Plan	Bluegum Design Services	Revision B 1 Oct 2019	31 Oct 2019
L/02	Landscape Details	Bluegum Design Services	1 Oct 2019	31 Oct 2019
2036 – S1/3	Stormwater Drainage/Sediment Control Details	John Romanous & Associates	Revision B 22 Oct 2019	31 Oct 2019
2036 – S2/3	Stormwater Drainage/Sediment Control Details	John Romanous & Associates	Revision B 22 Oct 2019	31 Oct 2019
2036 – S3/3	Stormwater Drainage/Sediment Control Details	John Romanous & Associates	Revision B 22 Oct 2019	31 Oct 2019

Reference Documentation affixed with Council's 'Development Consent' stamp relating to Development Consent No.DA2019/181:

Title / Description	Prepared by	Issue/Revision & Date	Date received by Council
Waste Management Plan	Unknown author	Oct 2019	31 Oct 2019
BASIX Certificate Certificate No. 876195S	Linda Sukkar	30 Oct 2019	31 Oct 2019
Nationwide House Energy Rating Scheme Certificate Certificate No. LYUO1XJVE1	Pranab Chakma	29 Oct 2019	31 Oct 2019

In the event of any inconsistency, the conditions of this consent shall prevail.

(Reason: To ensure that the form of the development undertaken is in accordance with the determination of Council.)

4. **BUILDING HEIGHT - MAXIMUM RL TO BE COMPLIED WITH (GC)**

The height of the building measured from Australian Height Datum (AHD) must not exceed

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Relative Level (RL) 51.84 AHD to the ridge of the building.

(Reason: To ensure the approved building height is complied with.)

5. CONSTRUCTION HOURS (GC)

No construction or any other work related activities shall be carried out on the site outside the hours of 7.00 am to 5.00 pm Mondays to Fridays and 8am to 1pm Saturdays.

No building activities are to be carried out at any time on a Sunday or public holiday.

Where the development involves the use of jackhammers/rock breakers and the like or other heavy machinery, such equipment may only be used between the hours of 7.00 am - 5.00 pm Monday to Friday only.

(Reason: To maintain amenity to adjoining land owners.)

6. CONSTRUCTION WITHIN BOUNDARIES (GC)

The development including but not limited to footings, walls, roof barges and guttering must be constructed wholly within the boundary of the premises. No portion of the proposed structure shall encroach onto the adjoining properties. Gates must be installed so they do not open onto any footpath or adjoining land.

(Reason: Approved works are to be contained wholly within the subject site.)

7. DEMOLITION – GENERALLY (GC)

Alteration and demolition of the existing building is limited to that documented on the approved plans (by way of notation). No approval is given or implied for removal and/or rebuilding of any portion of the existing building which is not shown to be altered or demolished.

(Reason: To ensure compliance with the approved development.)

8. DEMOLITION - SITE SAFETY FENCING (GC)

Site fencing is to be erected to a minimum height of 1.8m complying with WorkCover Guidelines, to exclude public access to the site, prior to any demolition works occurring and maintained for the duration of the demolition works.

If applicable, a separate Hoarding Application for the erection of an A class (fence type) or B class (overhead type) hoarding/fencing along the street frontage(s) complying with WorkCover requirements must be obtained including:

- payment to Council of a footpath occupancy fee based on the area of footpath to be occupied and Council's Schedule of Fees and Charges before the commencement of work; and
- provision of a Public Risk Insurance Policy with a minimum cover of \$10 million in relation to the occupation of and works within Council's road reserve, for the full duration of the proposed works, must be obtained with a copy provided to Council.

(Reasons: Statutory requirement and health and safety.)

9. LANDSCAPING - TREES PERMITTED TO BE REMOVED (GC)

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The following listed trees are permitted to be removed to accommodate the proposed development:

- Queen Palm (*Syagrus romanzoffiana*) in the front setback.

All trees permitted to be removed by this consent shall be replaced (2 trees for each tree removed) by species selected from Council's Recommended Tree List and must have a minimum mature height of 10 metres.

Replacement trees shall be a minimum 50 litre container size. Trees are to conform to the NATSPEC guide and Guide for assessing the quality of and purchasing of landscape trees by Ross Clarke, 2003. Trees are to be true to type, healthy and vigorous at time of delivery and planting, shall be pest and disease free, free from injury and wounds and self-supporting; and shall be maintained until maturity.

All trees are to be staked and tied with a minimum of three (3) hardwood stakes. Ties are to be hessian and fixed firmly to the stakes, one tie at half the height of the main stem, others as necessary to stabilise the plant.

Root deflection barriers having a minimum depth of 600mm are to be installed adjacent to all footpaths and driveways.

Soil conditioner/fertilizer/moisture retention additive/s are to be applied in accordance with manufacturer's recommendations, and mixed into the backfilling soil after planting tree/s.

A minimum 75mm depth of organic mulch shall be placed within an area 0.5m from the base of the tree.

Details demonstrating compliance shall be demonstrated to the Principal Certifying Authority, prior to the issue of any Occupation Certificate.

(Reason: To ensure appropriate planting back onto the site.)

10. ENVIRONMENTAL PROTECTION – TREE (GC)

All street trees and trees on private property that are protected under Council's controls, shall be retained except where Council's prior written consent has been obtained. The felling, lopping, topping, ringbarking, wilful destruction or removal of any tree/s unless in conformity with this approval or subsequent approval is prohibited.

All healthy trees and shrubs identified for retention on the approved drawings are to be suitably protected in accordance with Section 4, Tree protection measures, and Appendix C Tree Protection Zone Sign, of Australian Standard AS4970 - 2009, Protection of Trees on Development Sites, prior to the commencement of any works (including any demolition and excavation) and maintained for the duration of the works.

In this regard the street tree and all protected trees on or adjacent to the site will be protected as described below;

- i. A Tree Protection (Management) Plan prepared by a AQF Level 5 Arborist is to be available on site at all times prior to the issue of the Constriction Certificate, for all demolition and site works. This Tree Protection (Management) Plan must be shown on the demand of Authorised Council staff.
- ii. Protective perimeter fencing must be consistent with AS 4687, using metal

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- fence posts greater than 20mm diameter and solid or chain wire mesh 2 metres in height supported on concrete pads/feet at 2-2.5 metre spacing. Fencing placement should not be closer than 2.5 metres from the trunk of each protected tree, or at a distance as specified by the approved Tree Protection (Management) Plan and approved by Council's Tree Management Officer. Shade cloth is to be used to prevent entry of dust and contaminated liquids into the root protection area.
- iii. Signs must be placed on the Fencing. These signs must comply with AS1319. The signs must state the purpose of the fencing and the name and contact details of the builder and the appointed AQF Level 5 consultant arborist.
 - iv. Root protection is to be provided in the form of mulching to a depth of 50-100mm, inside of the fenced tree protection zone.
 - v. Ground (Root) Trunk and Branch Protection must comply with the diagram of Figure 4 of AS4970 – 2009. Ground protection within the tree protection zone and drip line should be in the form of heavy steel plates or timber rumble boards over mulch or aggregate. Trunk and branch protection should be to a minimum height of 2 metres and must include measures to protect the tree/s against damage from the movement of trucks, plant equipment and materials at greater heights above ground level.
 - vi. Temporary signs, cables, wires must not be attached to, or suspended, on any street tree or protected tree.
 - vii. Above ground utilities must not be placed within the canopy of an existing protected tree, or create the future need for clearance pruning.
 - viii. Below ground utilities/services must not be located inside the fenced tree protection zone.
 - ix. Scaffolding must not be located in the fenced tree protection zone unless authorised by the approved Tree Protection (Management) Plan and Council's Tree Management Officer
 - x. Council must be notified in writing within 24 hours, for all temporary removal of the tree protection fencing ground (root) trunk and branch protection within the root protection area.
 - xi. Pruning of a street tree is prohibited without the written consent of Council.

(Reason: To ensure the protection of trees to be retained on the site.)

11. LANDSCAPING - CANOPY TREES IN FRONT YARD (GC)

New development shall provide at least two (2) canopy trees within the front setback adjacent the front boundary. New canopy trees shall be planted from 200 litre containers at the time of planting and capable of reaching a potential mature height of >insert< m.

The new trees must not be planted closer than 1.5 metres from a structure or property boundary.

Species shall be selected from Council's Recommended Tree List (Available from Council's website or from the Customer Service Centre). Compliance with this condition is to be demonstrated to the Principal Certifying Authority, prior to the issue of any Occupation Certificate.

Trees are to conform to the NATSPEC guide and Guide for assessing the quality of and purchasing of landscape trees by Ross Clarke, 2003. Trees are to be true to type, healthy and vigorous at time of delivery and planting, shall be pest and disease free, free from injury and wounds and self-supporting; and shall be maintained until maturity.

All trees are to be staked and tied with a minimum of three (3) hardwood stakes. Ties are to be hessian and fixed firmly to the stakes, one tie at half the height of the main stem, others

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as necessary to stabilise the plant.

Root deflection barriers having a minimum depth of 600mm are to be installed adjacent to all footpaths and driveways.

Soil conditioner/fertilizer/moisture retention additive/s are to be applied in accordance with manufacturer's recommendations, and mixed into the backfilling soil after planting tree/s.

A minimum 75mm depth of organic mulch shall be placed within an area 0.5m from the base of the tree.

Details demonstrating compliance shall be demonstrated to the Principal Certifying Authority, prior to the issue of any Occupation Certificate.

(Reason: To ensure the landscape character of the locality is maintained.)

12. MATERIALS – SCHEDULE OF EXTERNAL FINISHES TO BE SUBMITTED (GC)

A schedule detailing all external materials, colours and finishes including windows, doors and roofing materials shall be submitted to Council for approval.

All external materials, colours and finishes including windows and doors and roofing materials must be of high quality and compatible with those of the surrounding streetscape and locality. All external materials and colours shall be of low glare and reflectivity. Details demonstrating compliance with this condition shall be provided to the Principal Certifying Authority, prior to the issue of any Construction Certificate.

(Reason: To ensure a positive contribution to the streetscape and to minimise excessive glare and reflectivity.)

13. PRINCIPAL CERTIFYING AUTHORITY (PCA) IDENTIFICATION SIGN (GC)

Prior to commencement of any work, signage must be erected in a prominent position on the work site identifying:

- i) The Principal Certifying Authority (PCA) by showing the name, address and telephone number of the PCA;
- ii) The Principal Contractor by showing the Principal Contractor's name, address and telephone number (outside of work hours) for that person; and
- iii) The sign must state that unauthorised entry to the work site is prohibited.

Any such sign is to be maintained while the work is being carried out, but must be removed when the work has been completed.

This clause does not apply to building work, subdivision work or demolition work that is carried out inside an existing building that does not affect the external walls of the building.

(Reason: Statutory requirement.)

14. PRIVACY - SCREEN PLANTING TO SIDE AND REAR BOUNDARIES (GC)

Evergreen shrubs with a minimum 5 litre container size achieving a minimum mature height of 3m shall be planted along the side and rear boundaries to provide suitable privacy screening. Screen planting shall be maintained at all times. Details of the selected plants shall be submitted to and approved by the Principal Certifying Authority. Compliance with this condition shall be certified by the Principal Certifying Authority, prior to occupation of the premises.

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(Reason: To maintain privacy amenity to neighbouring residences.)

15. **SITE MANAGEMENT (DURING DEMOLITION AND CONSTRUCTION WORKS)**

All of the following are to be satisfied/complied with during demolition, construction and any other site works:

- i) All demolition is to be carried out in accordance with Australian Standard AS 2601-2001.
- ii) Demolition must be carried out by a registered demolition contractor.
- iii) A single entrance is permitted to service the site for demolition and construction. Protection pads are to be installed to the kerb, gutter and nature strip where trucks and vehicles enter the site.
- iv) No blasting is to be carried out at any time during construction of the building.
- v) Care must be taken during demolition/ excavation/ building/ construction to prevent any damage to adjoining buildings.
- vi) Adjoining owner property rights and the need for owner's permission must be observed at all times, including the entering onto land for the purpose of undertaking works.
- vii) Any demolition and excess construction materials are to be recycled wherever practicable.
- viii) The disposal of construction and demolition waste must be in accordance with the requirements of the Protection of the Environment Operations Act 1997.
- ix) All waste on the site is to be stored, handled and disposed of in such a manner as to not create air pollution (including odour), offensive noise or pollution of land and/or water as defined by the Protection of the Environment Operations Act 1997. All excavated material should be removed from the site in an approved manner and be disposed of lawfully to a tip or other authorised disposal area.
- x) All waste must be contained entirely within the site.
- xi) A site plan including temporary waste storage locations, construction material storage areas and vehicular access to and from the site must be included with application.
- xii) Section 143 of the Protection of the Environment Operations Act 1997 requires waste to be transported to a place which can lawfully accept it. All non-recyclable demolition materials are to be disposed of at an approved waste disposal depot in accordance with legislation.
- xiii) Details as to the method and location of disposal of demolition materials (weight dockets, receipts, etc.) should be kept on site as evidence of approved methods of disposal or recycling and must be presented to Council and/or Principal Certifying Authority upon request.
- xiv) All materials on site or being delivered to the site are to generally be contained within the site. The requirements of the Protection of the Environment Operations Act 1997 must be complied with when placing/stockpiling loose material, disposing of concrete waste, or other activities likely to pollute drains or water courses.
- xv) Any materials stored on site must be stored out of view or in such a manner so as not to cause unsightliness when viewed from nearby lands or roadways.
- xvi) Public footways, include nature strips and roadways adjacent to the site must be maintained and cleared of obstructions during construction. No building materials, waste containers or skips may be stored on the road reserve or footpath without prior separate approval from Council, including payment of relevant fees.
- xvii) Building operations such as brick-cutting, washing tools or paint brushes, and mixing mortar not be performed on the roadway or public footway or any other locations which could lead to the discharge of materials into the stormwater drainage system.
- xviii) All site waters during excavation and construction must be contained on site in an approved manner to avoid pollutants entering into waterways or Council's stormwater drainage system.

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- xix) Any work must not prohibit or divert any natural overland flow of water.
- xx) Toilet facilities for employees must be provided in accordance with WorkCover NSW.

(Reason: To ensure that demolition, building and any other site works are undertaken in accordance with relevant legislation and policy and in a manner which will be non-disruptive to the local area.)

16. STORMWATER MANAGEMENT PLAN - CERTIFICATION REQUIREMENT (GC)

A detailed Stormwater Management Plan (SWMP) in line with Council's Stormwater Management Code is to be prepared and certified by an Accredited Civil/Hydraulic Engineer and submitted to the Principal Certifying Authority and Council, prior to the issue of a Construction Certificate. The SWMP is to be based on the approved development as modified by any conditions of consent.

(Reason: To ensure appropriate provision is made for the disposal and management of stormwater generated by the development, and to ensure public infrastructure in Council's care and control is not overloaded.)

17. SYDNEY WATER - TAP IN™ (GC)

The approved plans must be submitted to a Sydney Water Tap in™ to determine whether the development application will affect Sydney Water's sewer and water mains, stormwater drains and/or easements, and if further requirements need to be met. The approved plans will be appropriately endorsed. For details please refer to 'Plumbing, building and developing' section of Sydney Water's web site at www.sydneywater.com.au then see 'Building', or telephone 13000 TAP IN (1300 082 746). The Certifying Authority must ensure that a Tap in™ agent has appropriately stamped the plans prior to the issue of the Construction Certificate.

(Reason: Compliance with Sydney Water requirements.)

18. UTILITIES AND SERVICES - PROTECTION OF (GC)

Any footings or excavation to be located or undertaken adjacent to Council's stormwater must be designed to address the following requirements:

- i) All footings and excavation must be located wholly within the site and clear of any easement boundaries;
- ii) All footings and excavation must be located a minimum of 1000mm from the centreline of the pipeline or 500mm from the outside of the pipeline, whichever is the greater distance from the centreline; and
- iii) Footings must extend to at least the depth of the invert of the pipeline unless the footings are to be placed on competent bedrock.

If permanent excavation is proposed beneath the overtop of the pipeline, suitable means to protect the excavation and proposed retaining structures from seepage or other water flow from the pipeline and surrounding subsoil, must be provided. The design must be prepared by a qualified practicing Structural/Civil Engineer.

Construction plans must be approved by the appropriate utility's office (e.g. council, Sydney Water Corporation) to demonstrate that the development complies with the utility's requirements.

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The applicant must provide written evidence (e.g. compliance certificate, formal advice) of compliance with the requirements of all relevant utility providers (e.g. Water, Energy, Telecommunications, Gas).

(Reason: To ensure protection of Council assets.)

CONDITIONS TO BE SATISFIED PRIOR TO THE ISSUE OF A CONSTRUCTION CERTIFICATE (PCC)**19. BASIX COMMITMENTS (CC)**

The approved BASIX Certificate shall be submitted to the Principal Certifying Authority with the application for a Construction Certificate.

Where a change or changes are proposed in the BASIX commitments, the applicant must submit a new BASIX Certificate to the Principal Certifying Authority and Council. If any proposed change in the BASIX commitments is inconsistent with the development consent the applicant will be required to submit a modification to the development consent to Council under Section 4.55 of the Environmental Planning and Assessment Act 1979.

All commitments in the BASIX Certificate must be shown on the plans accompanying the Construction Certificate.

(Reason: Statutory compliance.)

20. BUILDING CODE OF AUSTRALIA - COMPLIANCE WITH (CC)

All architectural drawings, specifications and related documentation shall comply with the Building Code of Australia (BCA). All work must be carried out in accordance with the requirements of the Building Code of Australia (BCA).

In the case of residential building work for which the Home Building Act 1989 requires there to be a contract of insurance in force in accordance with Part 6 of that Act, such a contract of insurance is to be in force before any building work authorised to be carried out by the consent commences.

Details demonstrating compliance with this condition are to be submitted to the Principle Certifying Authority, prior to issue of the Construction Certificate.

(Reason: This is a 'prescribed' condition under clause 98(1) of the Environmental Planning and Assessment Regulation 2000.)

21. CAR PARKING - BASEMENT CAR PARKING REQUIREMENTS (CC)

Details demonstrating compliance with the following is to be submitted to the Principal Certifying Authority, prior to issue of the Construction Certificate:

- i) Minimal internal clearance of the basement is 2.2m in accordance with BCA requirements. The maximum floor to ceiling height within the entire basement must be 2.2m to ensure that the basement level is not utilised for habitable purposes.
- ii) Any parking spaces indicated within the basement must be capable of allowing vehicles to enter and exit the basement in a forward direction. Turning circles must be indicated on the relevant documentation to be submitted as part of the Construction Certificate.
- iii) Driveways shall comply with Australian/New Zealand Standard AS/NZS 2890.1:2004 - Parking facilities - Off-street car parking and achieve a maximum gradient of 1:4.

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- iv) Basement entries and ramps/driveways within the property are to be no more than 3.5m wide.
- iv) Provision of pump-out systems and stormwater prevention shall be in accordance with Council's Stormwater Management Guide.

(Reason: To ensure suitable development.)

22. CAR PARKING - VEHICULAR ACCESS RAMPS (CC)

Vehicular access ramps shall comply with the provisions of AS/NZS 2890.1:2004. Plans to be submitted shall contain the following details:

- i) Longitudinal section along the extreme wheel paths of each driveway/access ramp at a scale of 1:25 demonstrating compliance with the scraping provisions of AS/NZS 2890.1:2004. It shall include all levels and grades, including those levels stipulated as boundary levels, both existing and proposed. It shall extend from the centre-line of the roadway through to the parking area.
- ii) Sections showing the clearance to the underside of any overhead structure (minimum headroom requirements 2200mm minimum for standard headroom clearance or 2400mm where disabled access provisions are to be provided) demonstrating compliance with the provisions of AS/NZS 2890.1:2004.
- iii) Longitudinal section along the gutter line showing how it is intended to transition the vehicular crossing into the existing kerb and gutter. Boundary levels shall generally run parallel with the kerb levels.
- iv) Location of verge trees, street furniture and service installations.
- v) Superimposition of vehicle turning circles for access into parking spaces.
- vi) Certification that the design complies with AS/NZS 2890.1:2004 by a Qualified Engineer.

The certification referred to in (vi) above shall be submitted to the Principal Certifying Authority prior to the issue of a Construction Certificate (for the design) and to the Principal Certifying Authority prior to the issue of any Occupation Certificate for the 'as-built works'.

(Reason: To ensure adequate vehicular access can be achieved.)

23. CAR PARKING - COMPLIANCE WITH AS/NZS 2890.1:2004 (CC)

Car parking dimensions must comply with the Australian/New Zealand Standard AS/NZS 2890.1:2004 - Parking facilities - Off-street car parking. Details demonstrating compliance are to be submitted to the Principal Certifying Authority, prior to the issue of a Construction Certificate.

(Reason: To ensure compliance with Australian Standards relating to parking of vehicles.)

24. CAR PARKING - VEHICULAR CIRCULATION, AISLE AND RAMP WIDTHS (CC)

The aisle widths, internal circulation, ramp widths and grades of the car park shall comply with the Roads and Traffic Authority Guidelines and AS/NZS 2890.1:2004 - Off-Street Car Parking Code.

Vehicular manoeuvring paths are to be prepared demonstrating that all vehicles can enter or depart the site in a forward direction without encroaching on required car parking spaces.

(Reason: To ensure compliance with Australian Standards relating to manoeuvring, access and parking of vehicles.)

25. CAR PARKING - VEHICULAR CROSSING WIDTH AT PROPERTY BOUNDARY (CC)

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Vehicular crossings are to have a maximum width of 3m at the property boundary. Details demonstrating compliance are to be submitted to the Principal Certifying Authority, prior to the issue of a Construction Certificate.

The vehicular crossing (and all associated excavation works) must not be constructed closer than 2 metres from a retained protected street tree.

(Reason: Compliance with SCDCP 2005.)

26. COMMENCEMENT OF WORKS (NO WORKS UNTIL A CC IS OBTAINED)

Building work, demolition or excavation must not be carried out until a Construction Certificate has been issued by either Strathfield Council or a Principal Certifying Authority.

Demolition of any part of a building triggers 'commencement of erection of building' pursuant of section 4.19 of the EP&A Act 1979. Accordingly, demolition works must not commence until a Construction Certificate has been issued, a Principal Certifying Authority has been appointed and a Notice of Commencement has been issued.

(Reason: To ensure compliance with statutory provisions.)

27. COUNCIL PERMITS – FOR ALL ACTIVITIES ON COUNCIL LAND (CC)

Works Permit

(as per Section 68 of the Local Government Act 1993 and Section 138 and 139 of the Roads Act 1993)

A Works Permit is required for construction of a vehicular crossing (driveway), new stormwater down pipe connection to kerb and gutter, new footpath and/or stormwater connection. A Works Permit Application Form is available from Council's Customer Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website.

Standing Plant Permit

This permit must be applied for where it is intended to park a concrete pump, crane or other plant on the roadway or footpath. A Standing Plant Permit Application Form is available from Council's Customer Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website. Please note a Road Closure Permit is not required for standing plant.

Skip Bin Permit

This permit must be applied for if you intend to place a skip bin on the roadway or footpath. A Skip Bin Application Form is available from Council's Customer Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website.

Temporary Full or Part Road Closure Permit

This permit must be applied for if you require a full or a part road closure to take place to assist in your construction works. Please use the Works Permit Application Form, which is available from Council's Customer Service Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website. Please note a Road Closure Permit is not required for standing plant.

Hoarding/Fencing Permit

This permit must be applied for if you intend to erect a Class A (fence type) or Class B (overhead type) hoarding/fencing along the street frontage(s). A Hoarding Permit

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Application Form is available from Council's Customer Service Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website.

Work Zone Permit

This permit must be applied for if you require permanent parking along the kerbside at the front of the site during construction works. A Work Zone Permit Application Form is available from Council's Customer Service Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website.

Ground Anchoring Permit

This permit must be applied for, for the installation of ground anchors under Council's footway/road reserve. It does not cover ground anchors under private properties. A separate approval is required to be obtained from Roads and Maritime Services (RMS) if it is proposed to install ground anchors under a State or Classified Regional Road (please refer to the end of this application form for more information).

(Reason: Council requirement.)

28. EROSION AND SEDIMENTATION CONTROL PLAN (CC)

An Erosion and Sediment Control Plan is to be prepared where construction or excavation activity requires the disturbance of the soil surface and existing vegetation. Details including drawings and specifications must provide adequate measures for erosion and sediment control to ensure:

- i) Compliance with the approved Soil and Water Management Plan.
- ii) Removal or disturbance of vegetation and top soil is confined to within 3m of the approved building area (no trees to be removed without approval).
- iii) All uncontaminated run-off is diverted around cleared or disturbed areas.
- iv) Silt fences or other devices are installed to prevent sediment and other debris escaping from the cleared or disturbed areas into drainage systems or waterways.
- v) All erosion and sediment controls are fully maintained for the duration of demolition/development works.
- vi) Controls are put into place to prevent tracking of sediment by vehicles onto adjoining roadways.
- vii) All disturbed areas are rendered erosion-resistant by turfing, mulching, paving or similar.
- viii) All water pumped or otherwise removed from excavations or basement areas is filtered to achieve suspended solids/non filterable residue levels complying with the Australian Water Quality guidelines for Fresh and Marine Waters.
- ix) Pumped or overland flows of water are discharged so as not to cause, permit or allow erosion before the commencement of work (and until issue of the occupation certificate).

Details of the proposed soil erosion and sedimentation controls are to be submitted to the Principal Certifying Authority with the Construction Certificate Application. Under no circumstances are any works to commence, prior to these details being approved by the Principal Certifying Authority and the controls being in place on the site.

(Reason: Environmental protection.)

29. LANDSCAPING - MAINTENANCE STRATEGY (CC)

To ensure the survival of landscaping following works, a landscape maintenance strategy for the owner/occupier to administer over a 12 month establishment period following the issue of the Occupation Certificate shall be prepared and provided to the satisfaction of the

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Principal Certifying Authority with the Construction Certificate application. The strategy is to address maintenance issues such as, but not limited to plant survival, irrigation, soil testing, weeding, staking, fertilizing, remedial pruning and plant replacement.

(Reason: Ensure landscape survival.)

30. NOISE – CONSTRUCTION (CC)

All works carried out on site during construction/demolition/excavation/earthworks shall comply with the NSW Protection of the Environment Operations Act 1997, the Department of Environment and Climate Changes “Interim construction noise guideline” and AS 2436-2010 – “Guide to noise and vibration control on construction, demolition and maintenance sites” for the control of construction noise which specifies that:

- Construction period of 4 weeks and under – The L90 Level measured over a period of not less than 15 minutes when the construction site is operating must not exceed the background level by more than 20 dB(A) at the boundary.
- Construction period greater than 4 weeks but not exceeding 26 weeks – The L90 level measured over a period of not less than 15 minutes when the construction site is operating must not exceed the background level by more than 10 dB(A) at the boundary.
- Construction period exceeding 26 weeks – The L90 level measured over a period of not less than 15 minutes when the construction site is operating must not exceed the background level by more than 5 dB(A) at the boundary.

Should complaints of a noise nuisance be substantiated, Council may require the acoustic treatment of the premises to ensure compliance with the NSW Department of Environment and Climate Changes “Interim construction noise guideline” and AS 2436-2010 – “Guide to noise and vibration control, demolition and maintenance sites” for the control of construction noise.

(Reason: Noise attenuation.)

31. PRIVACY - OBSCURE GLAZING IN WET AREAS (CC)

All bathroom, ensuite and toilet windows shall be installed with obscure glazing. Plans shall be notated accordingly and details demonstrating compliance submitted to the Principal Certifying Authority, prior to the issue of a Construction Certificate.

(Reason: Privacy amenity.)

32. SECTION 7.12 CONTRIBUTION PAYMENT - INDIRECT CONTRIBUTIONS PLAN) (CC)

In accordance with the provisions of Section 7.13 of the Environmental Planning and Assessment Act 1979 and the Strathfield Indirect Development Contributions Plan 2010-2030, a contribution in the form of cash, cheque or credit card (financial transaction fee applies) shall be paid to Council for the following purposes:

Local Amenity Improvement Levy	\$11,500.00
--------------------------------	--------------------

The total amount of the contribution is valid as at the date of determination and is subject to quarterly indexation. The amount of the contribution under this condition shall be indexed in accordance with clause 4.12 of the Strathfield Indirect Development Contributions Plan 2010-2030.

Contributions must be receipted by Council and submitted to the Accredited Certifier, prior

DA2019/181 - 26 Kingsland Road, Strathfield - Lot 30 DP 211 (Cont'd)

to the issue of any Construction Certificate.

A copy of this condition is to be presented to Council's Customer Service Centre when paying the contribution so that it can be recalculated.

Note: A copy of Strathfield Council's Section 7.12 Indirect Development Contributions Plan may be downloaded from Council's website.

(Reason: To enable the provision of public amenities and services required/anticipated as a consequence of increased demand resulting from the development.)

33. STORMWATER - RAINWATER RE-USE (CC)

A rainwater re-use system shall be provided in accordance with either the BASIX minimum requirements, any relevant Council Rainwater Policy and/or specification of the management of stormwater, whichever is applicable. A detailed stormwater plan showing the proposed re-use system shall be submitted to and approved by the Principal Certifying Authority, prior to the issue of a Construction Certificate.

(Reason: Compliance and amenity.)

34. STORMWATER DRAINAGE (CC)

The following stormwater precautions are required to be provided on the site:

- i) Grated drains shall be provided along the driveway/basement entry at the vehicular crossing(s) and are to connect to the internal drainage system.
- ii) Overflow paths shall be provided to allow for flows in excess of the capacity of the pipe/drainage system draining the site.
- iii) Allowance shall be made for surface runoff from adjacent properties, and to retain existing surface flow path systems through the site. Any redirection or treatment of these flows shall not adversely affect any other properties
- iv) Boundary fencing shall be erected in such a manner as not to interfere with the natural flow of ground and surface water to the detriment of any other party.

(Reason: To ensure the site is drained appropriately and does not interfere with the natural flow of flood waters.)

35. SWIMMING POOLS / SPAS (CONSTRUCTION OF)

The approved swimming pool/spa must comply with all relevant legislation including:

- i) Swimming Pools Amendment Act 1992 (as amended);
- ii) Swimming Pools Regulations 2008; and
- iii) Australian Standard AS1926.1-2012 – Safety Barriers for Swimming Pools.

Details demonstrating compliance with this condition shall be demonstrated to the Principal Certifying Authority, prior to the issue of a Construction Certificate.

(Reason: Safety and statutory compliance.)

36. SWIMMING POOLS / SPAS - MECHANICAL PLANT ENCLOSURE (CC)

Any mechanical plant associated with the swimming pool and spa shall be acoustically treated to comply with the Protection of the Environment Operations Act 1997. Details are to be submitted to the Principal Certifying Authority, prior to the issue of a Construction

DA2019/181 - 26 Kingsland Road, Strathfield - Lot 30 DP 211 (Cont'd)

Certificate.

(Reason: To ensure the acoustic amenity of the neighbouring residents.)

37. TREE BONDS (CC)

A tree bond of **\$9000** (calculated in accordance with Council's adopted Fees and Charges) shall be paid to Council, prior to the issue of a Construction Certificate.

The deposit is required as security against any damage to existing trees to be retained on Council's road reserve, during works on the site. The applicant must bear the cost of all restoration works to Council's property damaged during the course of this development.

Payment may be accepted in the form of cash, bank guarantee, cheque or credit card (financial transactions fees apply). Note: Additional fees apply for the lodgement of a bank guarantee in lieu of cash bond applies in accordance with Council's adopted Fees and Charges.

A refund of the Tree Bond must be made in writing.

Tree Bonds may be forfeited if a tree is dead, made dangerous or has been terminally damaged, or will be held until tree/s have fully recovered from the construction damage or were replacement/planted trees have become fully established and are over 6 metres in height.

(Reason: To ensure the protection of trees to be retained on Council's Road Reserve.)

38. VEHICULAR CROSSINGS - WORKS PERMIT FOR CONSTRUCTION OF (CC)

Full-width, heavy-duty concrete vehicular crossing(s) shall be provided across the footpath at the entrance(s) and/or exit(s) to the site and designed in accordance with Council's guidelines and specifications. In this regard, a Works Permit is to be obtained (available from Council's Customer Services Centre or downloaded from Council's website), and the appropriate fees and charges paid, prior to the lodgement of a Construction Certificate.

(Reason: To ensure appropriate access to the site can be achieved.)

CONDITIONS TO BE SATISFIED PRIOR TO THE COMMENCEMENT OF WORKS (PCW)**39. APPOINTMENT OF A PRINCIPAL CERTIFYING AUTHORITY (PCA) (CW)**

No work shall commence in connection with this Development Consent until:

- i) A construction certificate for the building work has been issued by the consent authority or a Principal Certifying Authority.
- ii) The person having the benefit of the development consent has appointed a principal certifying authority for the building work, and notified the principal certifying authority that the person will carry out the building work as an owner/builder, if that is the case.
- iii) The principal certifying authority has, no later than 2 days before the building work commences:
 - notified the Council of his or her appointment, and
 - notified the person having the benefit of the development consent of any critical stage inspections and other inspections that are to be carried out in respect of the building work.
- iv) The person having the benefit of the development consent, if not carrying out the work as an owner-builder, has:

DA2019/181 - 26 Kingsland Road, Strathfield - Lot 30 DP 211 (Cont'd)

- appointed a principal contractor for the building work who must be the holder of a contractor licence if any residential building work is involved;
 - notified the principal certifying authority of such appointment; and
 - unless that person is the principal contractor, notified the principal contractor of any critical stage inspections and other inspections that are to be carried out in respect of the building work.
- v) The person having the benefit of the development consent has given at least 2 days' notice to the Council of the person's intention to commence the erection of the building.

Note: If the principal certifying authority is the Council, the nomination will be subject to the payment of a fee for the service to cover the cost of undertaking all necessary inspections and the issue of the appropriate certificates.

Under the Environment Planning and Assessment (Quality of Construction) Act, 2003, a sign must be erected in a prominent position on the work site showing the name, address and telephone number of the principal certifying authority; the name of the principal contractor (if any) for the building work and a telephone number at which that person may be contacted outside working hours. That sign must also state that unauthorised entry is prohibited. The sign must not be removed until all work has been completed.

(Reason: Statutory requirement.)

40. HOME BUILDING COMPENSATION FUND (CW)

No residential building work within the meaning of the Home Building Act 1989 may commence until:

- i) A contract of insurance in accordance with Part 6 of the Home Building Act 1989 is entered into and in force, where such a contract is required under that Act (this is a prescribed condition of consent pursuant to clause 981(b) Environmental Planning and Assessment Regulation 2000.
- ii) The PCA is satisfied that the principal contractor for the work is the holder of the appropriate licence and is covered by the appropriate insurance, in each case if required by the Home Building Act 1989 (unless the work is to be carried out by an owner-builder).
- iii) If the work is to be carried out by an owner builder, that the owner builder is the holder of any owner -builder permit required under the Home Building Act 1989.
- iv) Written notice of the following information has been provided to Council (this is a prescribed condition of consent pursuant to clause 98B of the Environmental Planning and Assessment Regulation 2000):
 - In the case of work for which a principal contractor is required to be appointed:
 - the name and licence number of the principal contractor; and
 - the name of the insurer by which the work is insured under Part 6 of the Home Building Act 1989.
 - In the case of work to be done by an owner-builder:
 - the name of the owner-builder; and
 - if the owner-builder is required to hold an owner-builder permit under the Home Building Act 1989, the number of the owner-builder permits.

Where Council is not the PCA, the PCA is responsible for notifying Council of the above matters.

Note: Where arrangements for doing residential building work change while the work is in progress so that the above information becomes out of date, the PCA (where not the Council) must provide Council with written notice of the updated information.

DA2019/181 - 26 Kingsland Road, Strathfield - Lot 30 DP 211 (Cont'd)

(Reason: Statutory requirement.)

41. NOTICE OF COMMENCEMENT (CW)

No work shall commence until the following details are submitted to Council:

- i) a Notice of Commencement (form will be attached with issue of a Construction Certificate or available from our website) within two (2) days of the date on which it is proposed to commence works associated with the Development Consent;
- ii) details of the appointment of a Principal Certifying Authority (either Council or another Principal Certifying Authority); and
- iii) details of the name, address and licence details of the Builder.

(Reason: Statutory requirement.)

CONDITIONS TO BE SATISFIED DURING DEMOLITION AND BUILDING WORKS (DBW)

42. FILL MATERIAL (DW)

The only waste derived material which may be received at the development site is:

- i) Virgin excavated material (within the meaning of the Protection of the Environment Operations Act 1997), and
- ii) any other waste-derived material the subject of a resource recovery exemption under cl.51A of the Protection of the Environment Operations (Waste) Regulation 2005 that is permitted to be used as fill material.

Any (b)-type material received at the development site must be accompanied by documentation certifying by an appropriately qualified environmental consultant the materials compliance with the exemption conditions; and this documentation must be provided to the Principal Certifying Authority on request.

(Reason: To ensure imported fill is of an acceptable standard for environmental protection purposes.)

43. OBSTRUCTION OF PUBLIC WAY NOT PERMITTED DURING WORKS (DW)

The public way must not be obstructed by any materials, vehicles, refuse, skips or the like, under any circumstances, without the prior approval of Council.

(Reason: To maintain public access and safety.)

44. PUBLIC INFRASTRUCTURE AND SERVICES (DW)

The applicant must comply with the requirements (including financial costs) of any relevant utility provider (e.g. Energy Australia, Sydney Water, Telstra, RMS, Council etc.) in relation to any connections, works, repairs, relocation, replacements and/or adjustments to public infrastructure or services affected by the development.

(Reason: To maintain public infrastructure and/or services.)

CONDITIONS TO BE SATISFIED PRIOR TO THE ISSUE OF AN OCCUPATION CERTIFICATE (POC)

45. ENGINEERING WORKS (CERTIFICATION OF)

DA2019/181 - 26 Kingsland Road, Strathfield - Lot 30 DP 211 (Cont'd)

Prior to occupation of the premises, a Work As Executed (WAE) Plan of all engineering and/or drainage works is to be submitted to the Principal Certifying Authority. The WAE Plan is to be certified by an Accredited Civil/Hydraulic Engineer demonstrating that:

- i) The stormwater drainage system; and/or
- ii) The car parking arrangement and area; and/or
- iii) Any related footpath crossing works; and/or
- iv) The proposed basement pump and well system; and/or
- v) The proposed driveway and layback; and/or
- vi) Any other civil works

have been constructed in accordance with the approved plans and any relevant Standards and Council policies/specifications.

For major works, such as subdivisions, works within the road reserve (requiring separate S138 approval) and as where specified by Council, a Part 4A Certificate will be required. It is strongly recommended that an Engineer supervise all engineering related works.

Where Council is not the Principal Certifying Authority, an electronic copy of the above documents is to be provided to Council, prior to the issue of any Occupation Certificate.

(Reason: Asset management.)

46. LANDSCAPING - ARBORIST'S FOLLOW UP REPORT OF TREE/S TO BE RETAINED (OC)

As part of the on-going assessment of the tree/s to be retained, the consulting arborist engaged by the applicant is to assess their health and any impacts suffered by them as a result of the proposed approved development, prior to the issue of an Occupation Certificate. Findings are to be compiled in a detailed report and submitted to the Principle Certifying Authority, which documents the following:

- i) Methods of excavation or construction used to carry out the works;
- ii) Any damage sustained by the tree/s as a result of the works;
- iii) Any subsequent remedial works required to be carried out by the consulting arborist as a result of the damage; and
- iv) Any future or on-going remedial work required to be carried out to ensure the long term retention of the tree/s.

(Reason: To ensure the survival of trees to be retained.)

47. OCCUPATION OF BUILDING (OC)

A person must not commence occupation or use (or change of use where an existing building) of the whole or any part of a new building (within the meaning of section 109H (4) of the Act) unless an Interim Occupation Certificate or Final Occupation Certificate has been issued in relation to the building or part.

The Principal Certifying Authority is required to be satisfied, amongst other things, that:

- i) All required inspections (including each applicable mandatory critical stage inspection) have been carried out; and
- ii) Any preconditions to the issue of the certificate required by a development consent have been met.

Note: New building includes an altered portion of, or an extension to, an existing building.

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(Reason: Statutory requirement.)

48. RAINWATER TANKS (OC)

Rainwater tanks must be installed on residential properties by a suitably qualified and licensed plumber and in accordance with the following:

- i) Australian/New Zealand Standard AS/NZS 3500:2003;
- ii) NSW Code of Practice Plumbing and Drainage, 2006 produced by Committee on Uniformity of Plumbing and Drainage Regulations in NSW (CUPDR); and
- iii) Council's rainwater tank policy.

Details demonstrating compliance with this condition are to be submitted to the Principal Certifying Authority, prior to the issue of any Occupation Certificate.

(Reason: To protect public health and amenity.)

49. STORMWATER - CERTIFICATION OF THE CONSTRUCTED DRAINAGE SYSTEM (OC)

The constructed stormwater system shall be certified by a suitably qualified person, in accordance with Council's Stormwater Management Code, prior to the issue of any Occupation Certificate.

(Reason: Adequate stormwater management.)

50. SWIMMING POOLS / SPAS - REGISTRATION AND REQUIREMENTS (OC)

The approved swimming pool/spa must comply with all relevant legislation including:

- i) Swimming Pools Amendment Act 1992 (as amended);
- ii) Swimming Pools Regulations 2008; and
- iii) Australian Standard AS1926.1-2012 – Safety Barriers for Swimming Pools.

Swimming pool owners must register their swimming pool or spa on the NSW Swimming Pool Register. A copy of a valid certificate of compliance or relevant occupation certificate must be attached to new residential tenancy agreements to rent property with a swimming pool or spa pool. Note: This requirement does not apply to a lot in strata or community title schemes with more than two lots.

A copy of a valid certificate of compliance or relevant occupation certificate must be attached to new residential tenancy agreements to rent property with a swimming pool or spa pool. Note: This requirement does not apply to a lot in strata or community title schemes with more than two lots; or for any off the plan contacts.

Details demonstrating compliance with this condition shall be demonstrated to the Principal Certifying Authority, prior to the issue of any Occupation Certificate.

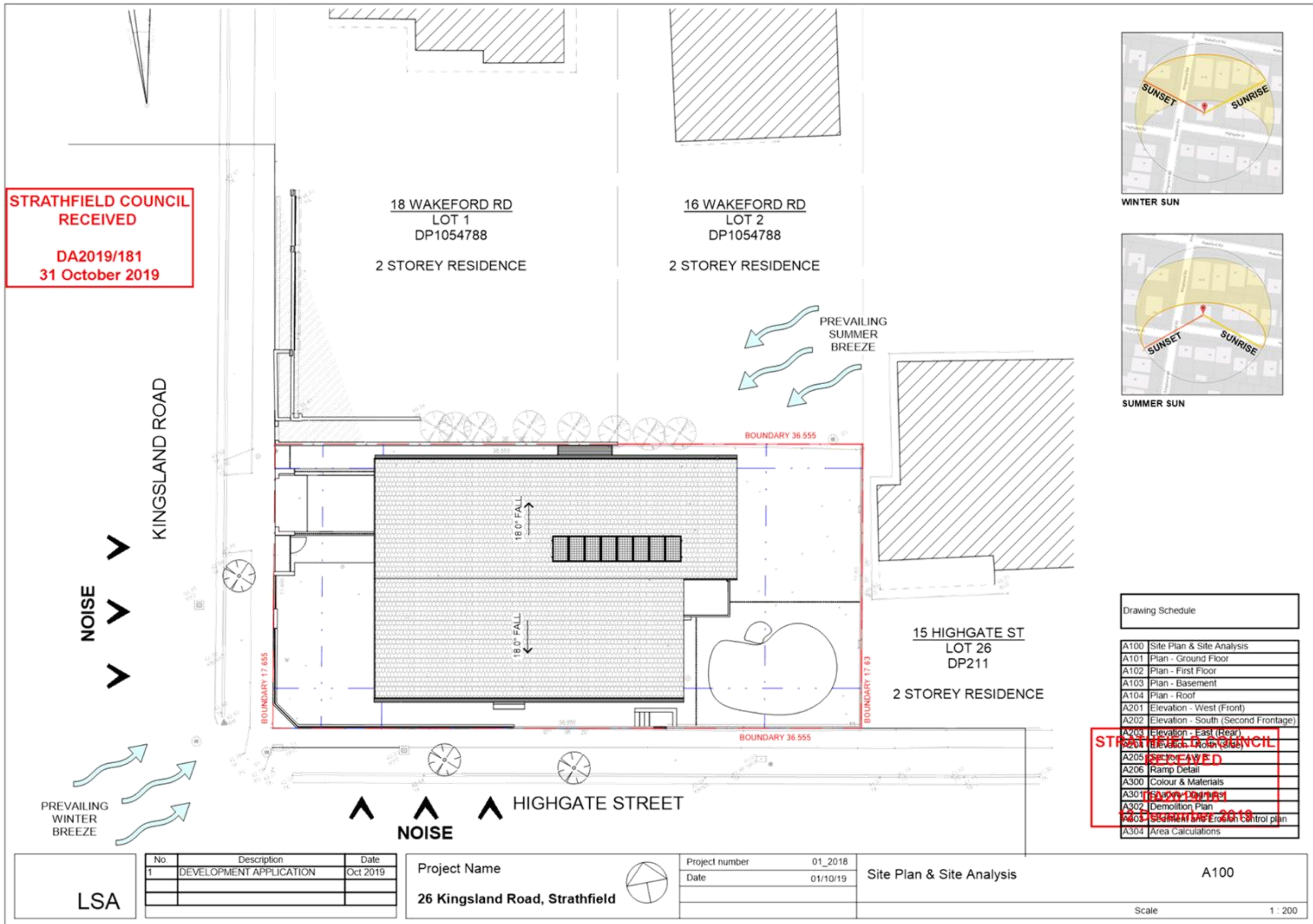
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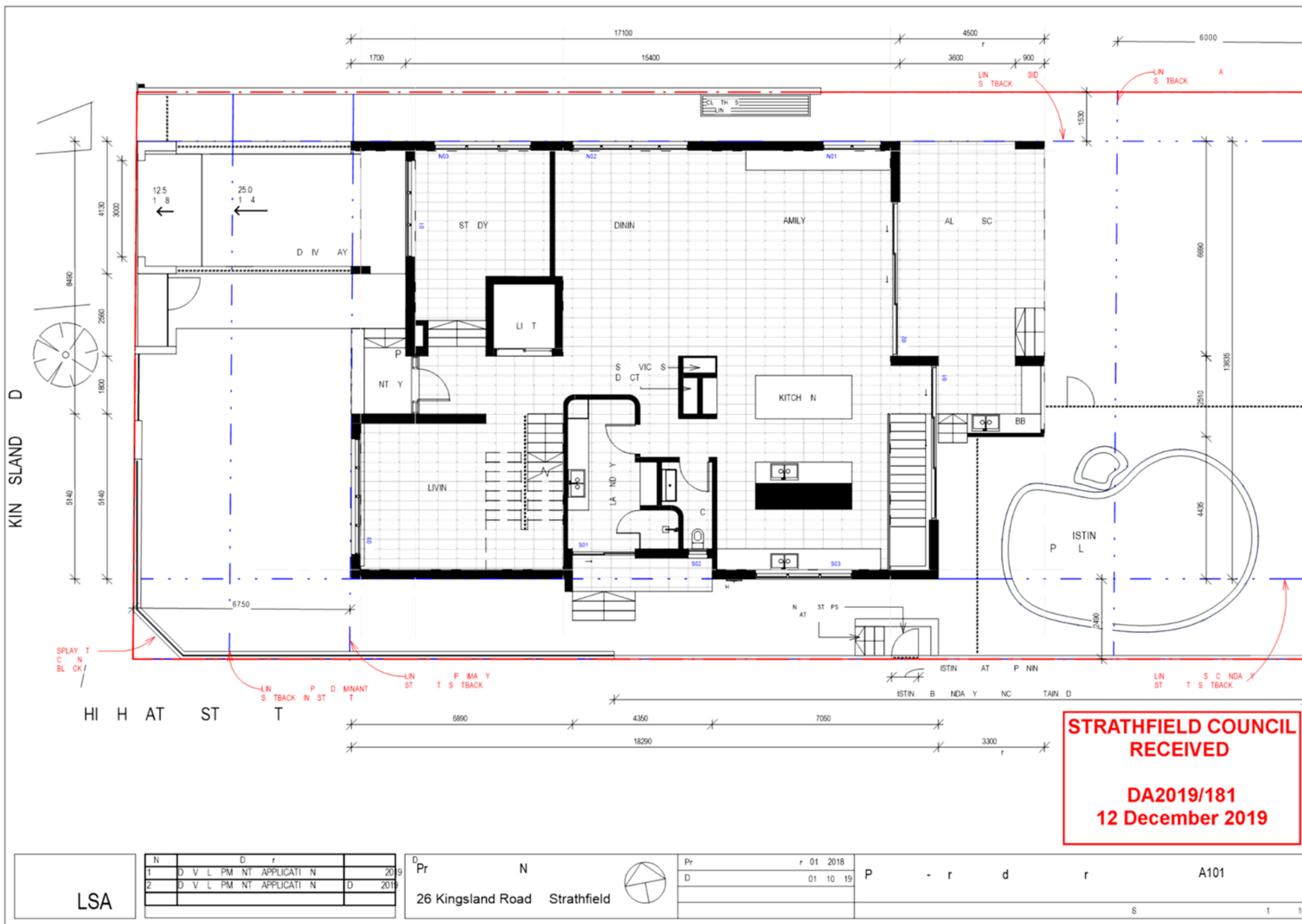
ATTACHMENTS

1. [↓](#) Architectural Plans
2. [↓](#) Landscape Plans
3. [↓](#) Stormwater Plans

DA2019/181 - 26 Kingsland Road, Strathfield - Lot 30 DP 211 (Cont'd)

- 4. [↓](#) Waste Management Plan
- 5. [↓](#) BASIX Certificate
- 6. [↓](#) NATHERS Certificate







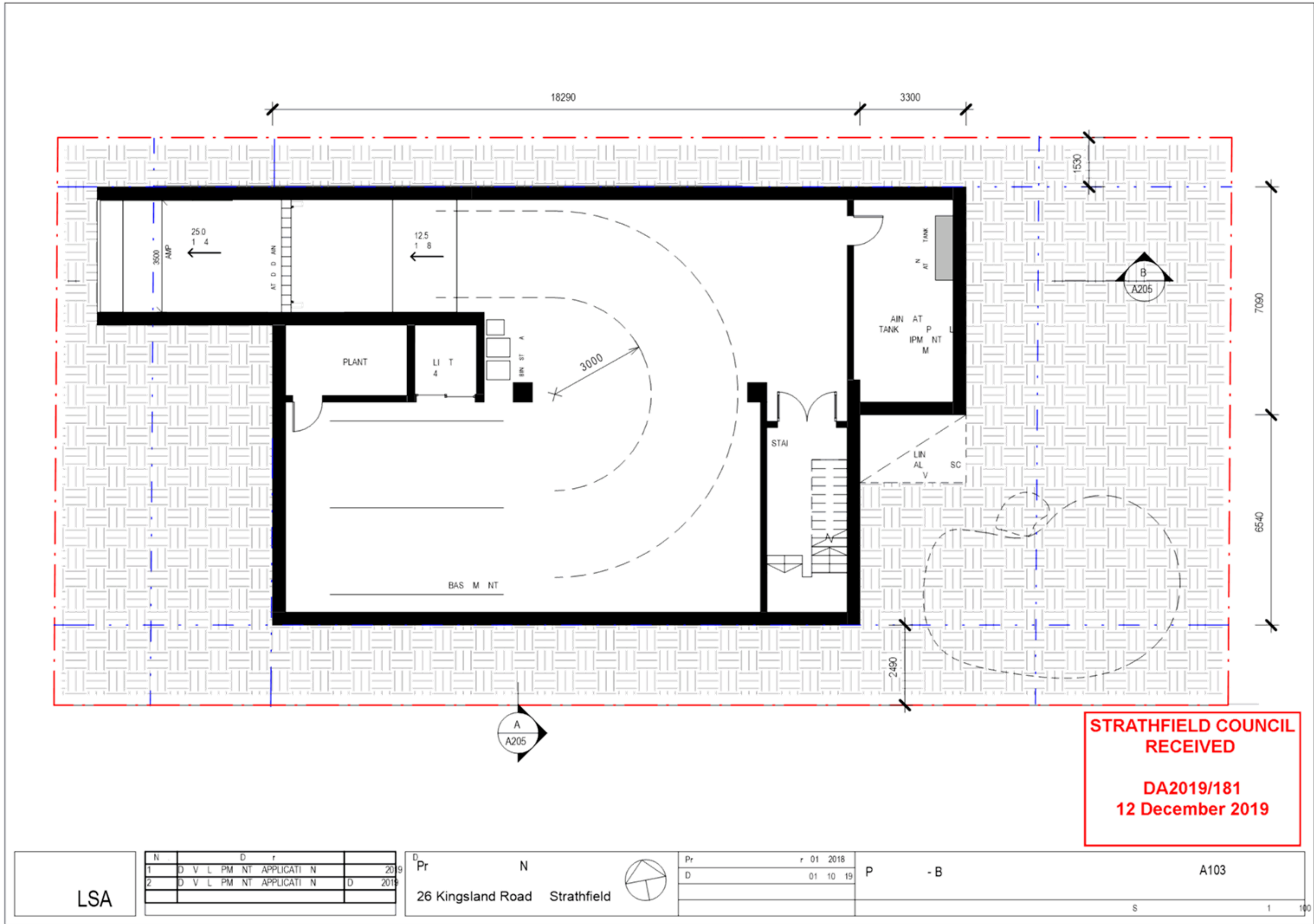
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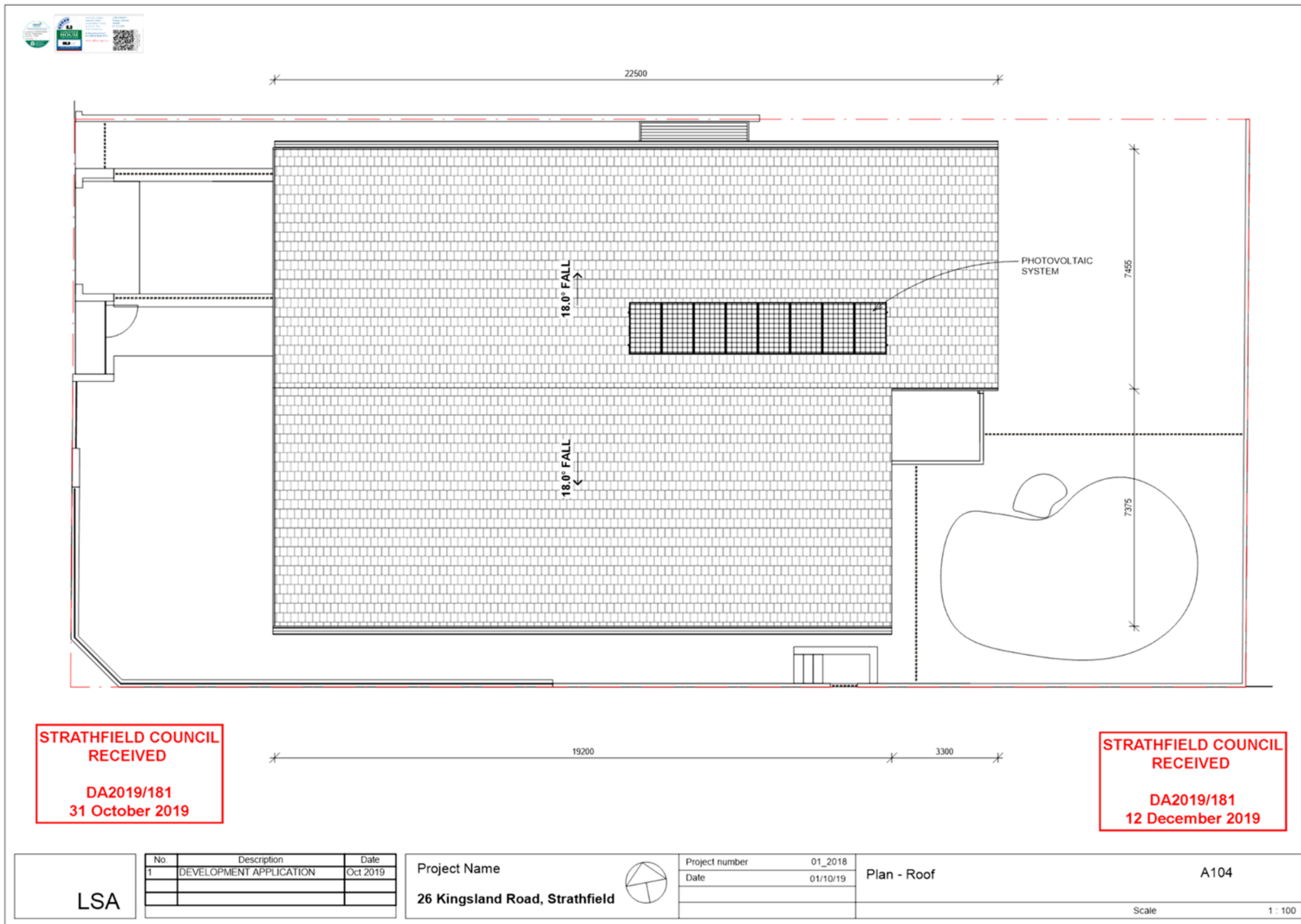
**DA2019/181
12 December 2019**

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26 Kingsland Road Strathfield

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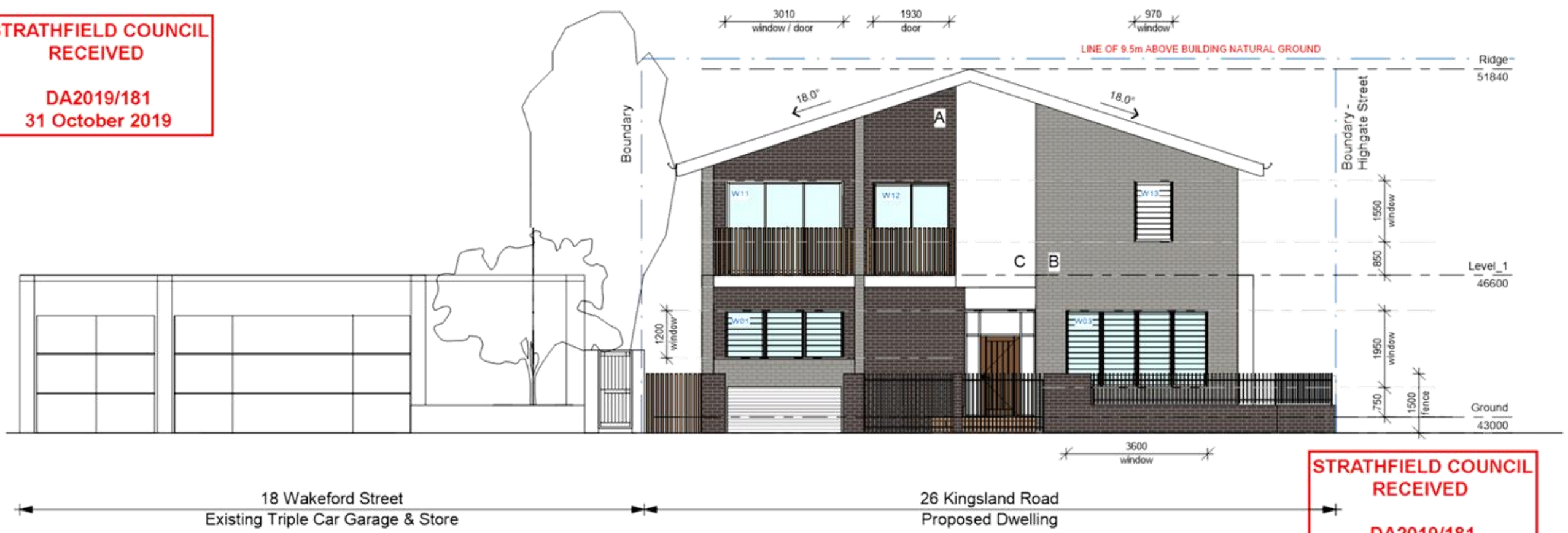






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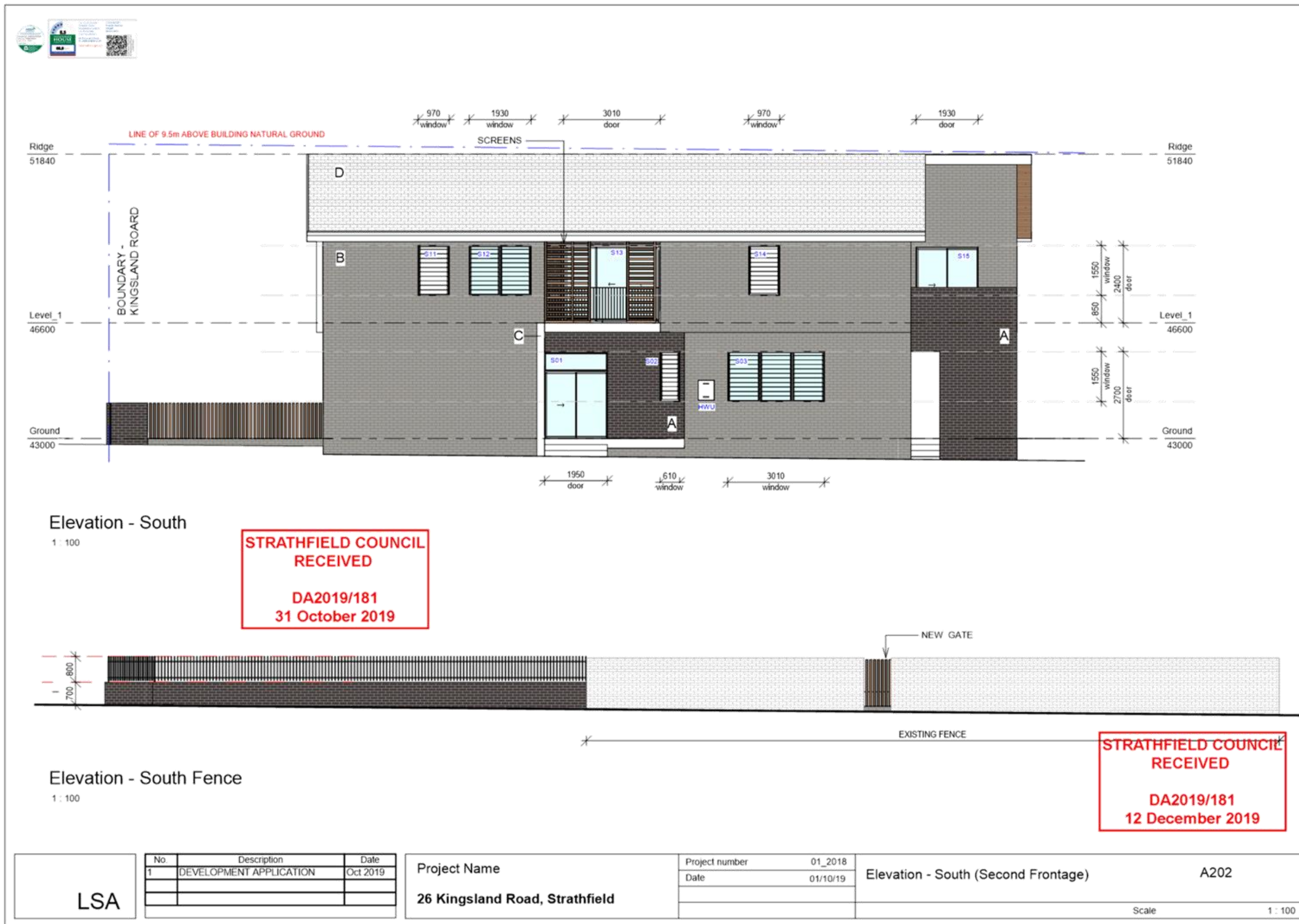
DA2019/181
31 October 2019

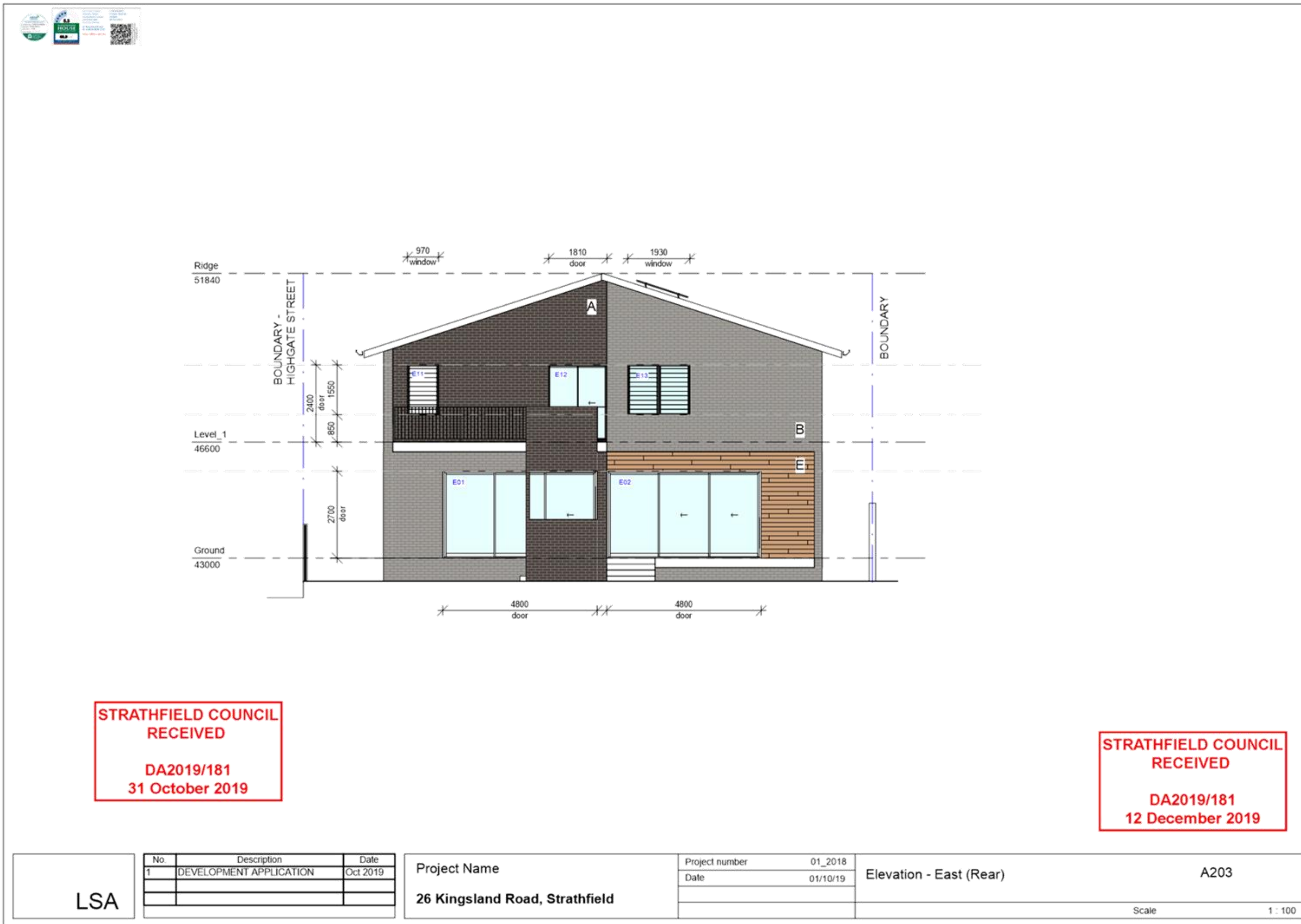


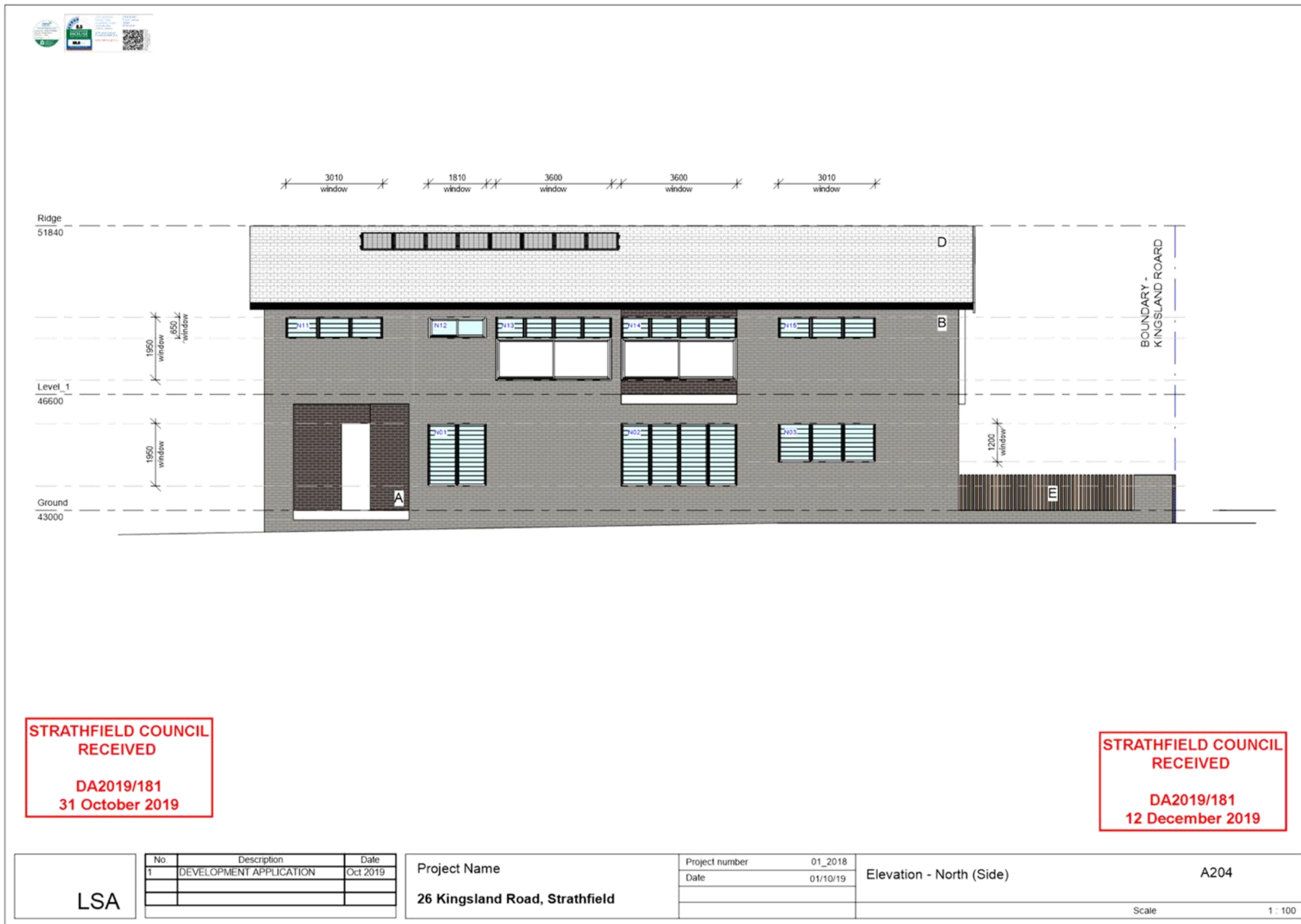
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DA2019/181
12 December 2019

LSA	No.	Description	Date	Project Name	Project number	01_2018	Elevation - West (Front)	A201
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				26 Kingsland Road, Strathfield				Scale 1 : 100

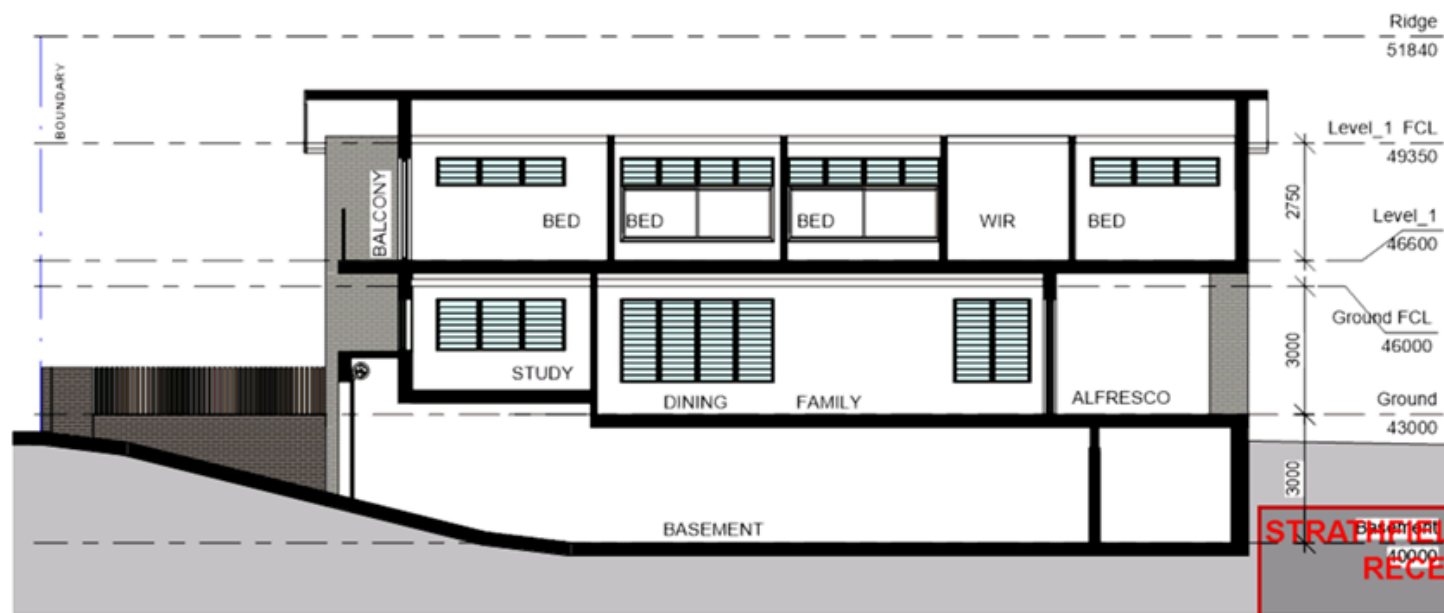








Section A
1 : 150



Section B
1 : 150

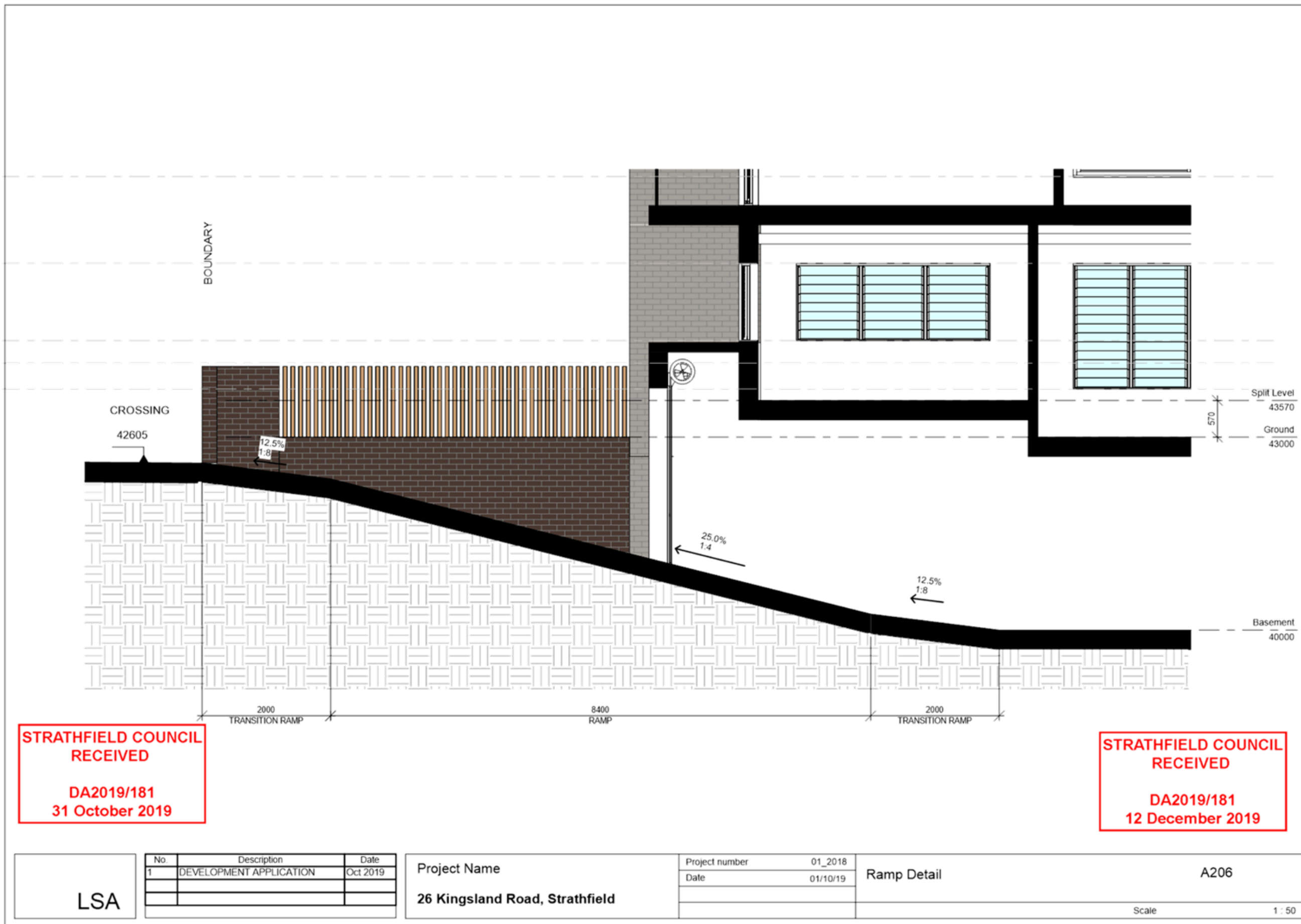
**STRATHFIELD COUNCIL
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DA2019/181
31 October 2019

**STRATHFIELD COUNCIL
RECEIVED**

DA2019/181
12 December 2019

LSA	<table border="1"> <thead> <tr> <th>No.</th> <th>Description</th> <th>Date</th> </tr> </thead> <tbody> <tr> <td>1</td> <td>DEVELOPMENT APPLICATION</td> <td>Oct 2019</td> </tr> <tr> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>		No.	Description	Date	1	DEVELOPMENT APPLICATION	Oct 2019							Project Name 26 Kingsland Road, Strathfield		Project number 01_2018 Date 01/10/19 Section A & B		A205
	No.	Description	Date																
1	DEVELOPMENT APPLICATION	Oct 2019																	
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 DA2019/181
 31 October 2019

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 DA2019/181
 12 December 2019

LSA

No.	Description	Date
1	DEVELOPMENT APPLICATION	Oct 2019

Project Name
 26 Kingsland Road, Strathfield

Project number	01_2018
Date	01/10/19

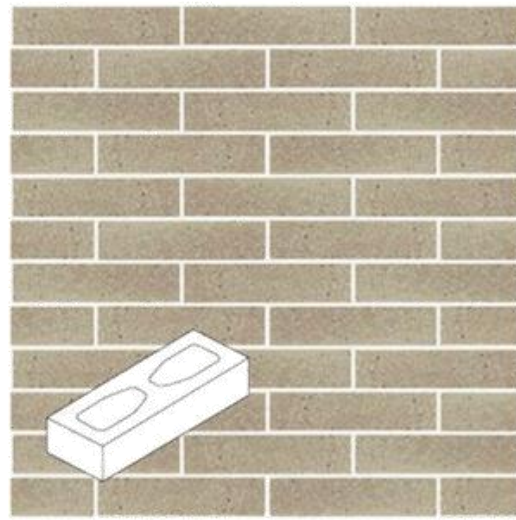
Ramp Detail
 A206
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DA2019/181
31 October 2019



A - BRICK TYPE 2



B - BRICK TYPE 1
(PRIMARY)



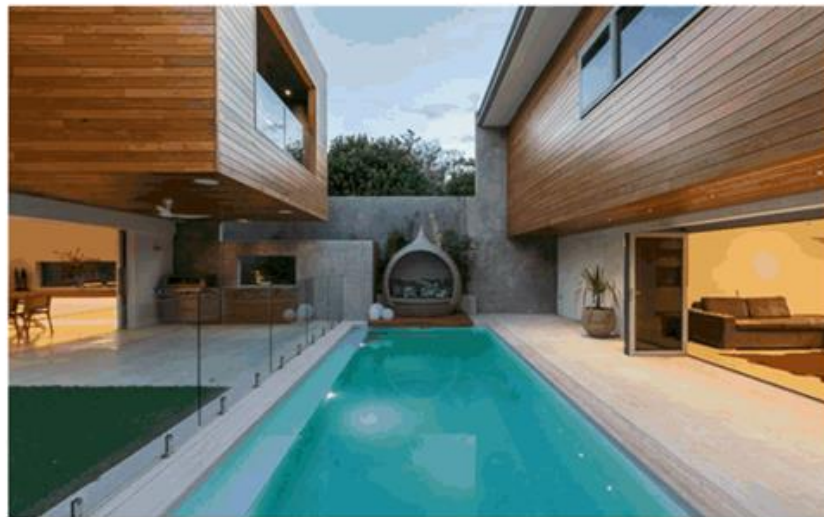
C - PREFINISHED
CONCRETE LOOK
FEATURE CLADDING



D - ROOF
TILES



GUTTERS /
DOWNPIPE /
GARAGE DOOR/
FENCE/
BALUSTRADES



E - TIMBER CLADDING | SCREENS |
BALUSTRADES



F - METAL BALUSTRADE | FENCE



ALUMINUM WINDOWS / DOORS

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No.	Description	Date
1	DEVELOPMENT APPLICATION	Oct 2019

Project Name
26 Kingsland Road, Strathfield

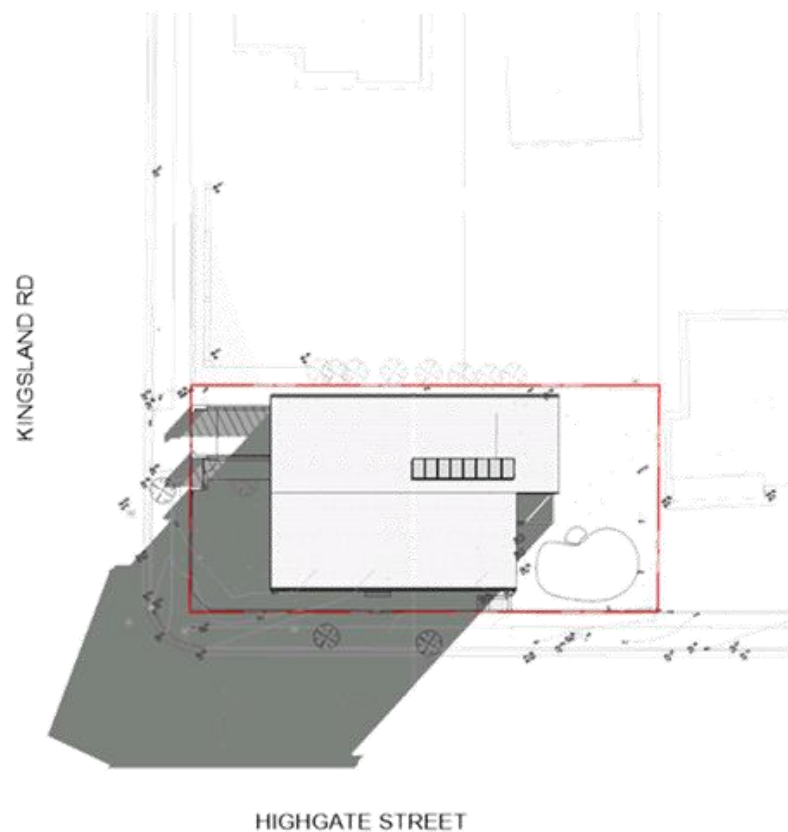
Project number	01_2018
Date	01/10/19

Colour & Materials
A300

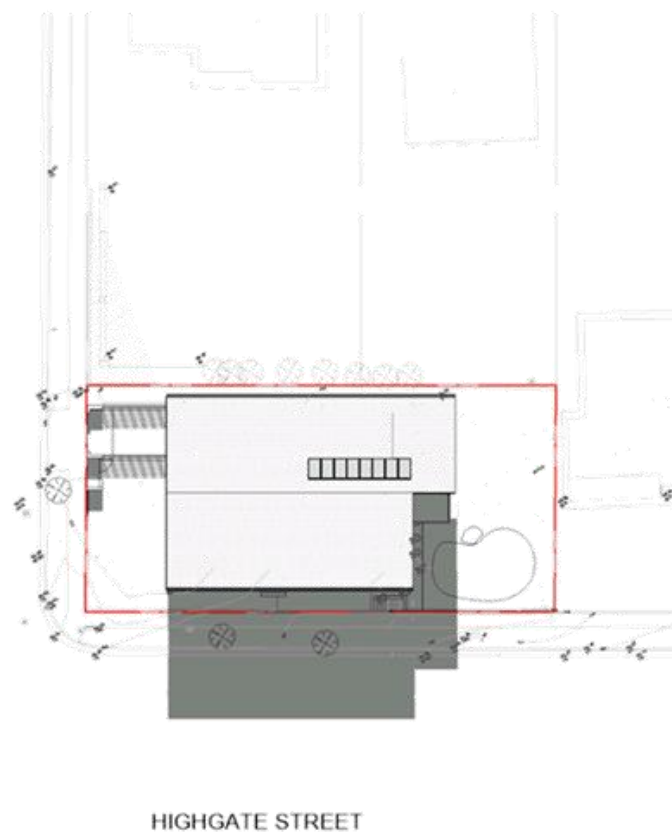
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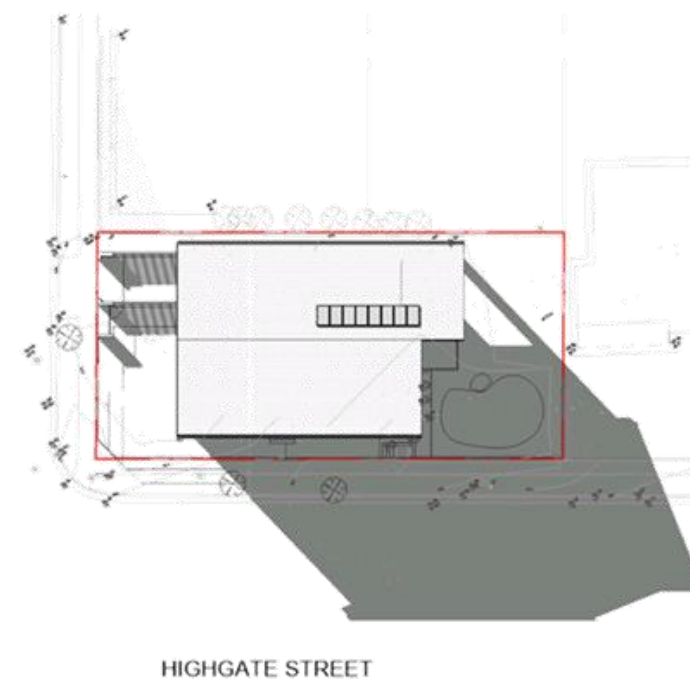
DA2019/181
31 October 2019



Shadow - June 22 9pm
1 : 500



Shadow - June 22 12pm
1 : 500



Shadow- June 22 3pm
1 : 500

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DA2019/181
12 December 2019

LSA

No.	Description	Date
1	DEVELOPMENT APPLICATION	Oct 2019

Project Name
26 Kingsland Road, Strathfield

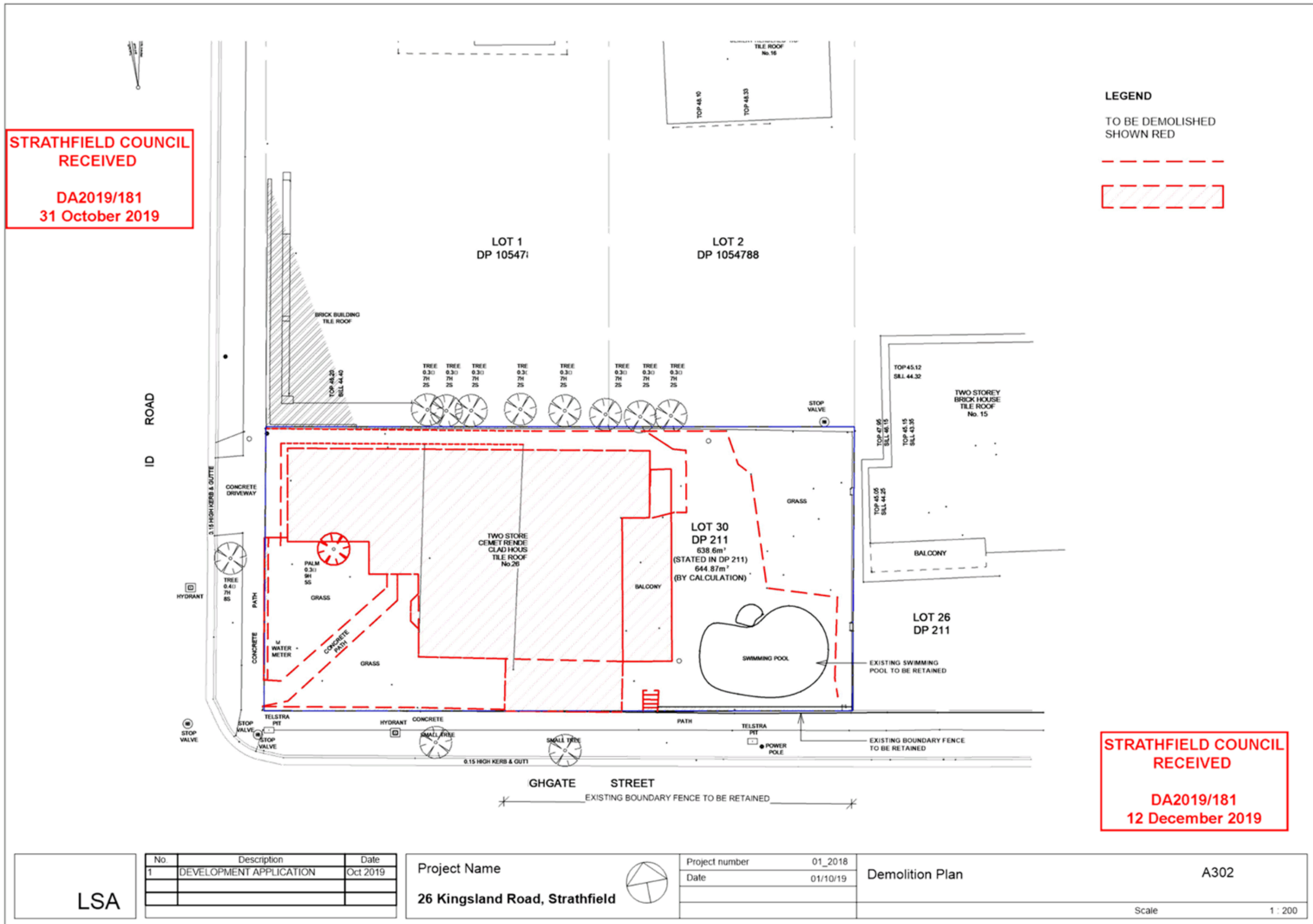


Project number	01_2018
Date	01/10/19

Shadow Diagrams

A301

Scale 1 : 500



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DA2019/181
31 October 2019

LEGEND
TO BE DEMOLISHED SHOWN RED

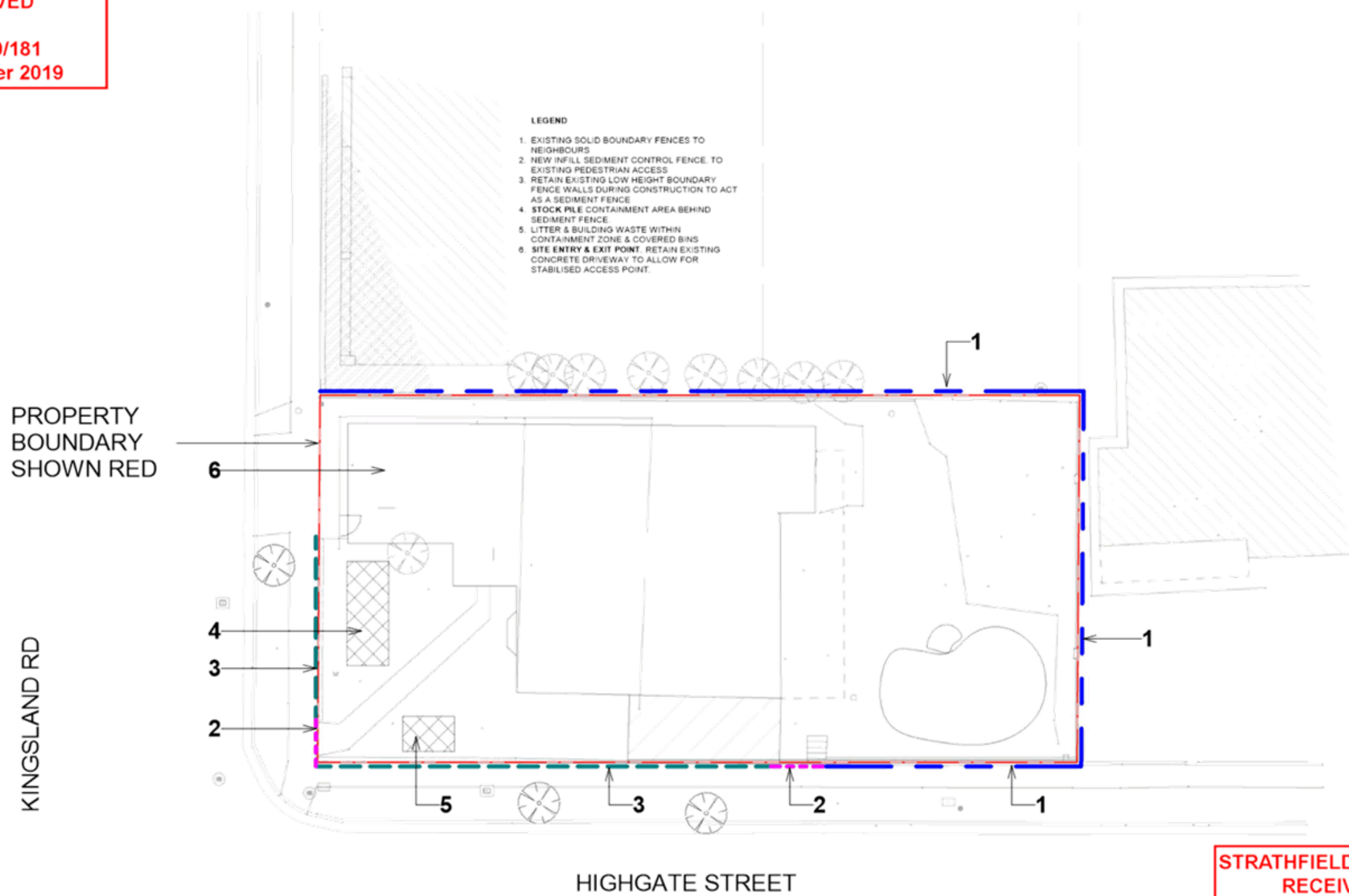
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DA2019/181
12 December 2019

LSA	No.	Description	Date	Project Name	Project number	Date	Demolition Plan	A302
	1	DEVELOPMENT APPLICATION	Oct 2019					
				Scale		1 : 200		

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DA2019/181
31 October 2019



- LEGEND**
- 1. EXISTING SOLID BOUNDARY FENCES TO NEIGHBOURS
 - 2. NEW INFILL SEDIMENT CONTROL FENCE TO EXISTING PEDESTRIAN ACCESS
 - 3. RETAIN EXISTING LOW HEIGHT BOUNDARY FENCE WALLS DURING CONSTRUCTION TO ACT AS A SEDIMENT FENCE
 - 4. STOCK PILE CONTAINMENT AREA BEHIND SEDIMENT FENCE
 - 5. LITTER & BUILDING WASTE WITHIN CONTAINMENT ZONE & COVERED BINS
 - 6. SITE ENTRY & EXIT POINT. RETAIN EXISTING CONCRETE DRIVEWAY TO ALLOW FOR STABILISED ACCESS POINT.

PROPERTY BOUNDARY SHOWN RED

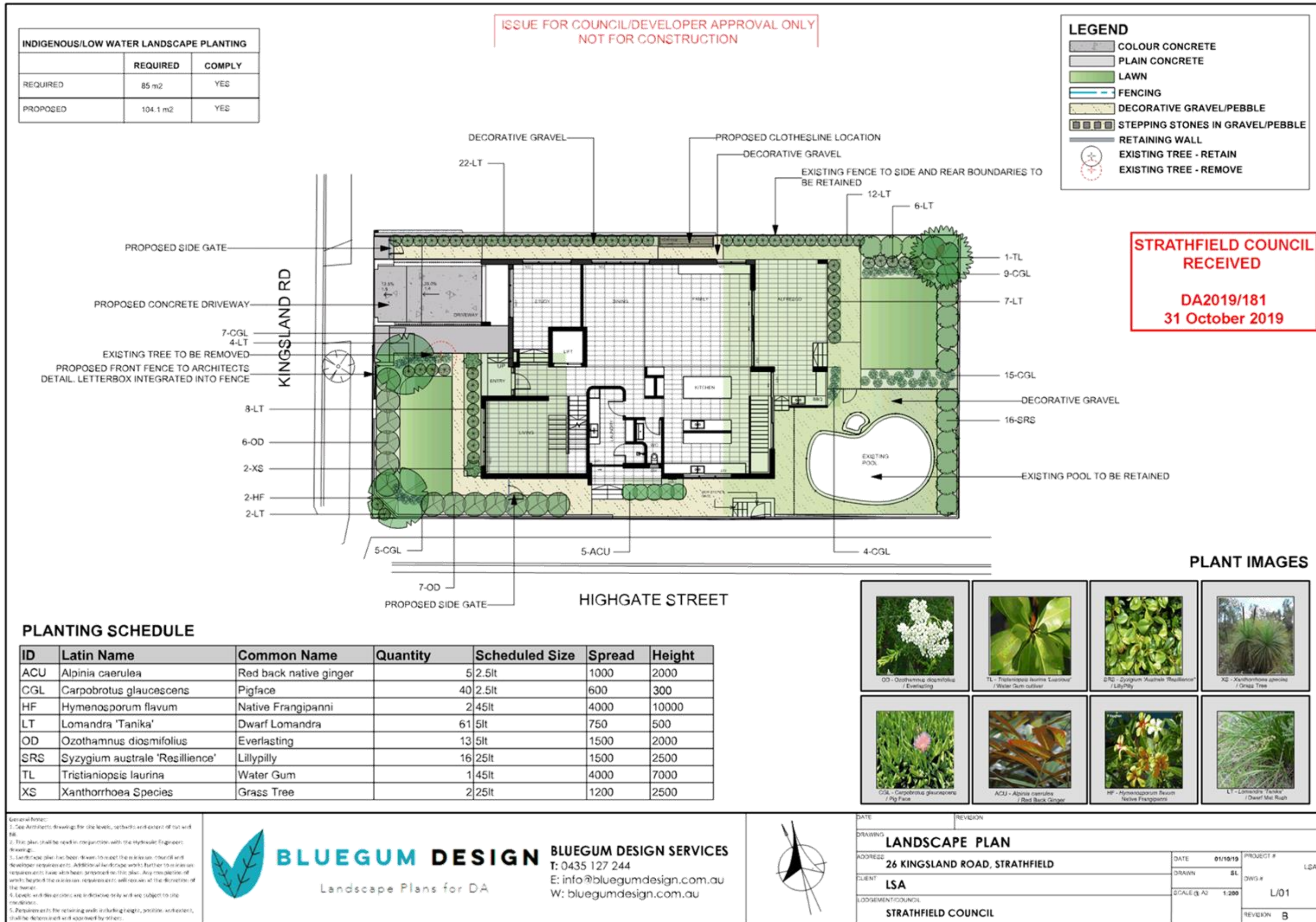
KINGSLAND RD

HIGHGATE STREET

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DA2019/181
12 December 2019

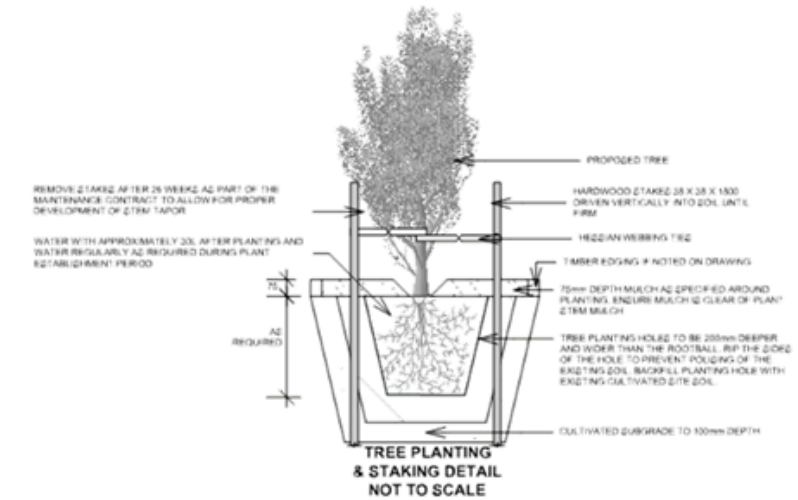
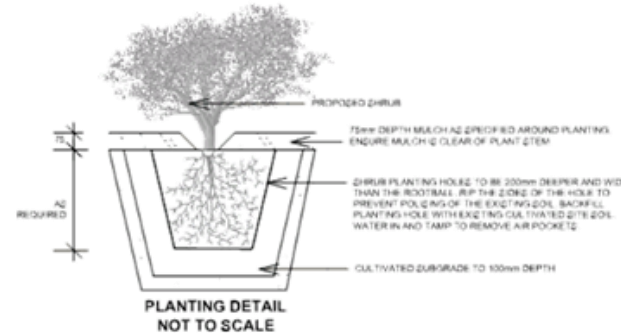
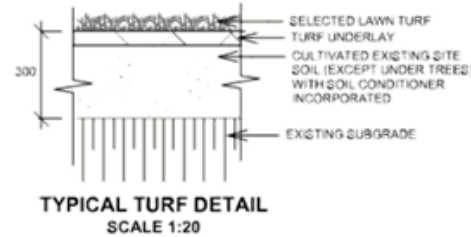
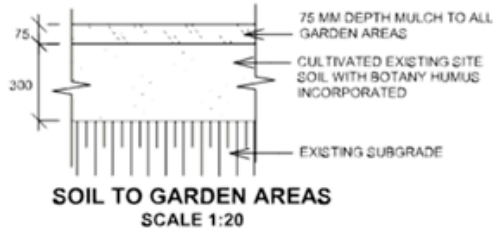
LSA	No.	Description	Date	Project Name	Project number	01_2018	Sediment and Erosion control plan	A303
	1	DEVELOPMENT APPLICATION	Oct 2019		Date	01/10/19		
				26 Kingsland Road, Strathfield			Scale	1 : 200



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DA2019/181
31 October 2019

LANDSCAPE DETAILS



OUTLINE LANDSCAPE SPECIFICATION

Tree Protection: Trees to be retained shall be protected during site works and construction by the erection of solid barricades to the specification of Council. Storage of machinery or materials beneath canopy of trees to be retained shall not be permitted. Changes to soil level and cultivation of soil beneath canopy of trees to be retained shall not be permitted unless under direct supervision of Landscape Architect. Existing trees shall be pruned to Landscape Architects onsite instructions.

Soil Preparation: Cultivate to depth of 300mm all proposed lawn & garden areas incorporating minimum 100mm depth of organic clay breaker into existing site soil. Do not cultivate beneath existing trees to be retained. In areas where fill is required gain required shapes & levels using a premium grade soil mix. In areas where excavation is required (if in clay) over excavate as required to allow for installation of 500mm depth of premium grade topsoil mix to garden areas and 300mm depth of premium grade topsoil mix to lawn areas. Undertake all required action to ensure that no rootballs of proposed plants sit in clay wells and that all garden areas and lawn areas drain satisfactorily. Note it is intended that wherever possible existing levels shall not be altered through garden and lawn areas. It is the Contractors responsibility to ensure that the end result of the project is that all lawn and garden areas drain sufficiently (both surface & subsurface), are at required finished levels and have sufficient soil depths to enable lawn and plants to thrive and grow. Should alternative works to those specified be required to achieve the above result, Contractor shall inform Builder at time of Tender and request instructions.

Lawn Edging and Stepping Stones:(i) 125 x 25mm approved tanalith impregnated pine edging shall be installed, to lines as indicated on plan and staked with approved stakes at maximum 1500mm centres at ends and changes of direction; stakes shall be nailed to edging with approved galvanised steel nails. Top of edging shall finish flush with surrounding surfaces. Top of stakes shall finish 25mm below top of edging.(ii) Contractor shall install approved bricks on edge on a minimum 100mm deep x 90mm wide concrete footing with brick set in, to lines nominated on plan as brick edging. Bricks shall be laid with a nominal 10mm wide approved coloured mortar joint. Bricks needing to be cut shall be done so with clean sharp cuts. Top of edging shall finish flush with surrounding finished surfaces. Approved sandstone stepping stones shall be positioned as indicated on plan on a 25mm river sand bed. Approved sandstone stepping stones shall be positioned as indicated on plan on a 25mm river sand bed.

Planting: Purchase plants from an approved nursery. Plants to be healthy & true to type & species. Set out plants to positions indicated on plan. Following approval, plant holes shall be dug approximately twice width and to 100mm deeper than plant rootballs that they are to receive. Base and sides of hole shall be further loosened. Fertiliser, followed by 100mm depth of topsoil mix shall then be placed into base of hole and lightly consolidated. Base of hole shall then be watered. Remove plant container and install plant into hole. Rootball shall be backfilled with surrounding topsoil and topsoil firmed into place. An approved shallow dish shall be formed to contain water around base of stem. Base of stem of plant shall finish flush with finished soil level. Once installed plant shall be thoroughly watered and maintained for the duration of the Contract.

Staking: All trees shall be staked using 2 x 38mm x 38mm x 2000mm long hardwood stakes per plant and with hessian webbing ties installed to Landscape Architect's on site instructions.

Mulching: Install 75mm depth of 25mm diameter hardwood mulch to all garden areas, coving mulch down around all plant stems & to finish flush with adjacent surfaces.

Turfing: Prepare for, level & lay cultivated Palmetto Buffalo turves to all areas nominated on plan as being lawn. Roll, water, fertilise, mow & maintain lawns as necessary until completion of maintenance period. At same time make good all existing lawn areas using same lawn type. Lawns in shade shall be over sown with an approved seed mix. Allow to retrim and returf councils nature strip as required.

General Notes:
1. See Architects drawings for site levels, setbacks and extent of cut and fill.
2. This plan shall be read in conjunction with the Hydraulic Engineers drawings.
3. Landscape plan has been drawn to meet the minimum Council and developer requirements. Additional landscape works further to minimum requirements have also been proposed on this plan. Any modification of works beyond the minimum requirements will require at the discretion of the Council.
4. Levels and elevations are indicative only and are subject to site conditions.
5. Requirements for retaining walls including height, position and extent, shall be determined and approved by others.

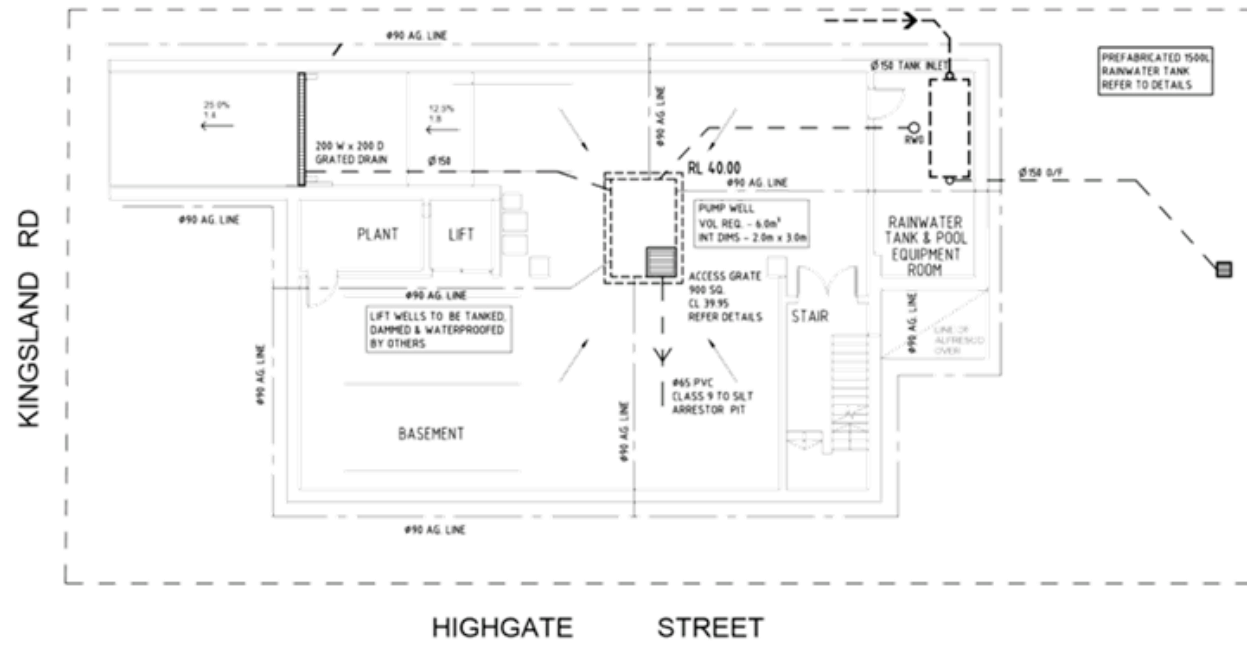


BLUEGUM DESIGN
Landscape Plans for DA

BLUEGUM DESIGN SERVICES
T: 0435 127 244
E: info@bluegumdesign.com.au
W: bluegumdesign.com.au



DATE	REVISION		
DRAWING: LANDSCAPE DETAILS			
ADDRESS	DATE	PROJECT #	LSA
26 KINGSLAND ROAD, STRATHFIELD	01/10/19		
CLIENT	DRAWN	DWG #	
LINDA SUKKAR	SL	L/02	
LODGE/COUNCIL	SCALE (S/A)	NTS	REVISION
STRATHFIELD COUNCIL			

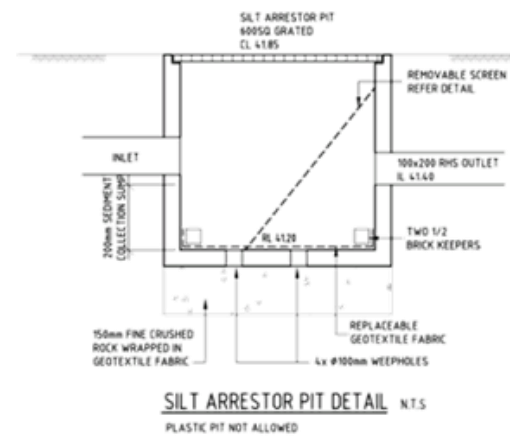


BASEMENT FLOOR DRAINAGE PLAN SCALE - 1:100

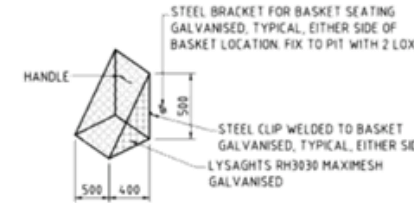
ALL GUTTERS TO BE MIN-LINE MINIMUM SIZE TO ARCHITECTURAL SPECIFICATION OR AS NOTED ON PLAN
 ALL GUTTERS TO BE PROVIDED WITH LEAF GUARD.
 ALL BALCONIES ARE TO HAVE A 65mm DIAMETER OVERFLOW PIPE OR AND 80mm SQUARE OPENING ACTING AS AN OVERFLOW
 ALL DOWNPIPES TO BE 100mm DIAMETER OR 100 x 100mm MIN. TO ARCHITECTURAL SPECIFICATIONS OR AS NOTED ON PLAN.
 PROVIDE AG. LINES BEHIND ALL RETAINING WALLS AND SUBSOIL DRAINAGE AS PER STRUCTURAL ENGINEERING DETAILS. ALL LINES TO DRAIN TO PITS.

- DENOTES PROPOSED LEVEL
- DENOTES EXISTING LEVEL
- DENOTES DOWNPIPE
- ROOFWATER ONLY
- STORMWATER ONLY

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 31 October 2019



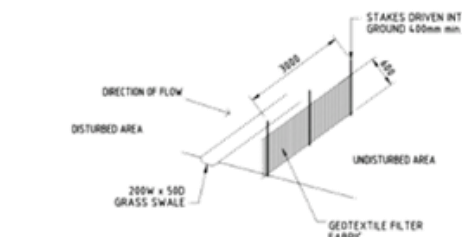
SILT ARRESTOR PIT DETAIL N.T.S.
 PLASTIC PIT NOT ALLOWED



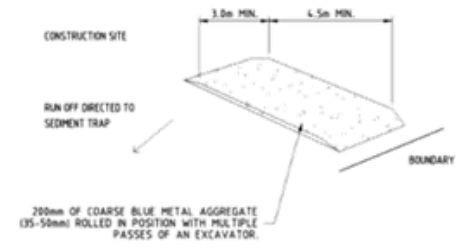
SCREEN DETAIL N.T.S.



SEDIMENT BARRIER N.T.S.



SEDIMENT FENCE DETAIL N.T.S.



CONSTRUCTION ENTRY/EXIT DETAIL N.T.S.

BASEMENT PUMPING WELL

Provide two centrifugal drainage SUMP pumps with single phase electric motor capable of discharging 2 RL/s each against a total head of (4.5m) with 10 starts per hour maximum
 Class 1 Zone 2 certified pumps for hazardous areas is required
 Switching shall provide for alternative operation of the pumps, high level switch ON/OFF, 2nd pump, and a red light alarm placed prominently in the basement area activated by high level switch ON

Basement Holding Tank
 Area draining to the garage pumping well is the driveway to the basement (150m²)
 Storage must be provided for a blackout of at least 2hrs, the 100yrs ARI 2hr storm runoff is:
 $Q = F \times C \times I \times A$
 $= 1/3600 \times 1 \times 60 \times 50$
 $= 0.83 \text{ L/s}$

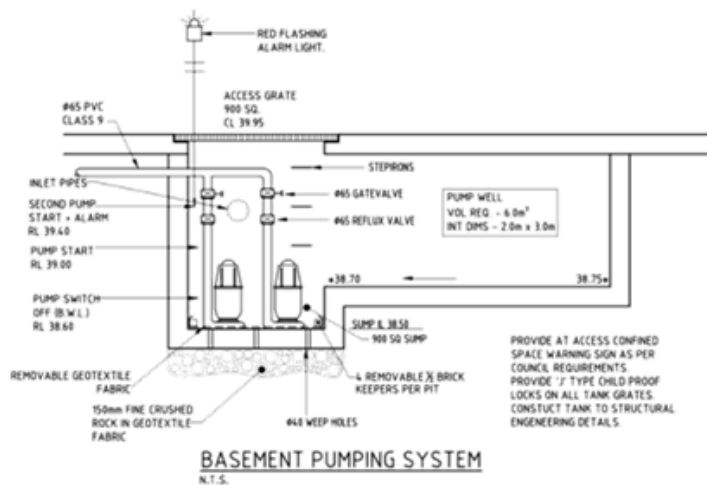
Volume accumulated
 $V = 10.83 \text{ L/s} \times 2\text{hrs} \times 3600\text{s}/1000$
 $= 6.0 \text{ m}^3$

Let the maximum discharge be for the 10 min 100yrs ARI, in the event of a stronger storm such as a 5min 100yrs ARI, the standby pump will operate with the duty pump to discharge the extra runoff. In case of a break down, storage is provided in the basement for the excess runoff.
 $Q = F \times C \times I \times A$
 $= 1/3600 \times 1 \times 207 \times 50$
 $= 2.9 \text{ L/s}$

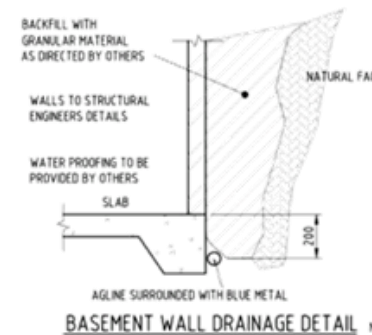
A control volume is required to prevent the pump from starting too often (< 10/hr)
 $CV = 900 \times Q_p / s$
 $= (900 \times 2.9 \text{ l}/10 \times 1000)$
 $= 0.26 \text{ m}^3$

Basement Pumping Duty Calculation (Q = 2.9 L/s)

ITEM	HEAD
Static Adjustment	3.4m
45mm Pump Fittings (L 75m (1.3m/100)	0.4m
45mm PVC class 9 (L 20m (1.3m/100)	0.3m
Sundries/about 10%	0.4m
Total	4.5m



BASEMENT PUMPING SYSTEM N.T.S.



BASEMENT WALL DRAINAGE DETAIL N.T.S.

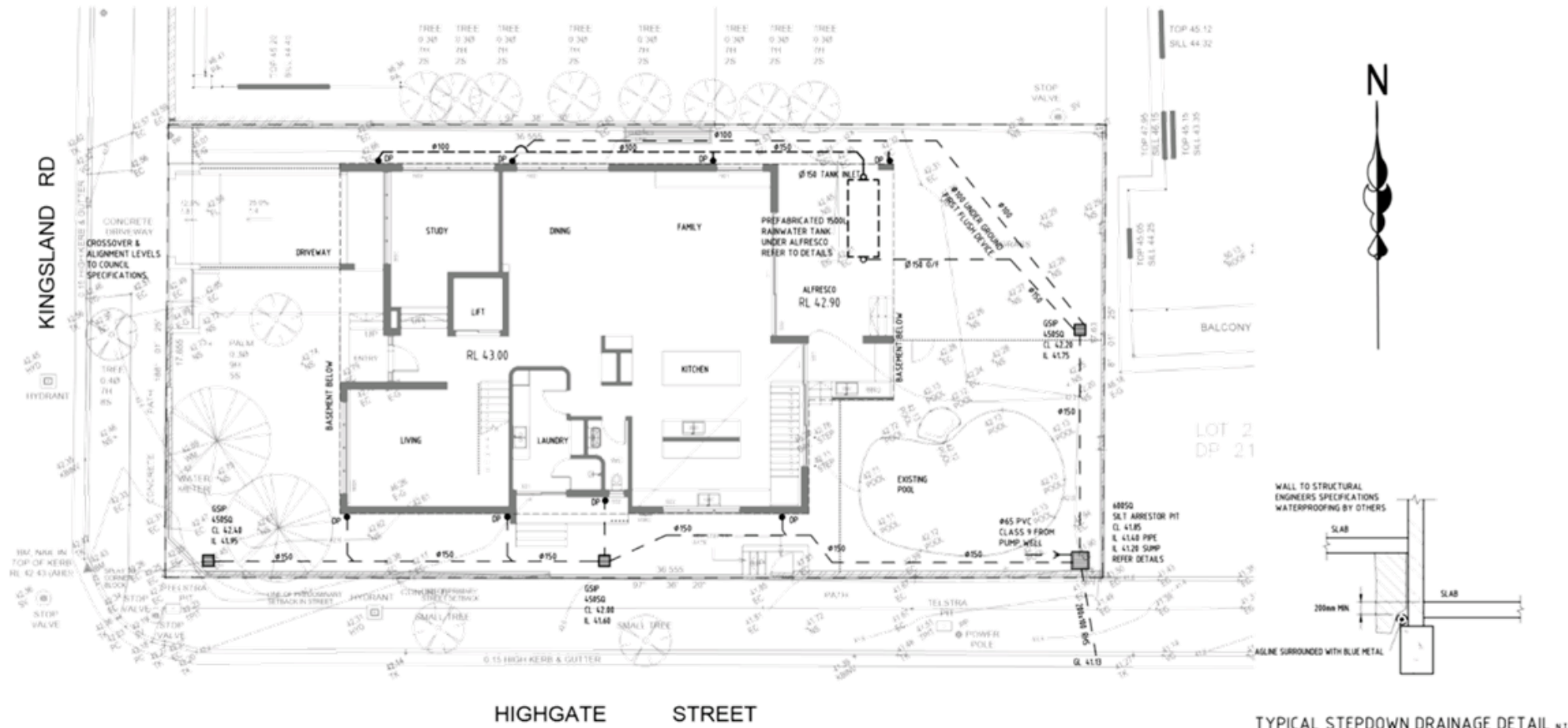
NOT FOR CONSTRUCTION

REV	DATE	AMENDMENT DESCRIPTION	DRAWN
B	27.10.2019	ISSUED TO THE ARCHITECT	JDR
A	06.09.2019	ISSUED TO THE ARCHITECT	HE

JOHN ROMANOUS & ASSOCIATES PFL LTD
 CONSULTING CIVIL & STRUCTURAL ENGINEERS ACN 054 595 005
 SUITE 20 / 322 KINGSDRIVE RD, KINGSDRIVE NSW 2208 PH (02) 83 87 66 26

PROPOSED DEVELOPMENT AT:
26 KINGSLAND ROAD, STRATHFIELD

DRAWN	CHECKED	SCALE	DATUM	DRAWING NO.	REV
HE	J.R.	AS SHOWN @A1	A.H.D	2036 - S1/3	B



NOTES - FIRST FLUSH DEVICE
 THE FOLLOWING MAINTENANCE WORK MUST BE UNDERTAKEN TO ENSURE THE DRAINAGE SYSTEM FUNCTION IS SATISFACTORY.

- INSPECTION / FLUSH LINE / CLEANING WITH WATER EVERY 6 MONTHS AND AFTER HEAVY RAINFALL
- INSPECTION / CLEANING GUTTERS AT LEAST ONCE A YEAR AND AFTER HEAVY RAINFALL OR STRONG WIND

THE ABOVE IS FOR GUIDANCE ONLY. AN ASSESSMENT OF MAINTENANCE REQUIREMENTS SHOULD BE UNDERTAKEN WHEN IN SERVICE.

ALL DOWN PIPES MUST BE uPVC WITH CEMENT SOLVENT SEALED JOINTS TO 10m ABOVE THE PERMANENT WATER LINE, OR ALL THE WAY UP TO THE UNDERSIDE OF THE EAVES. A SQUARE/CIRCULAR SECTION MAY THEN OCCUR (REFER DETAILS).

PROVIDE AN INSPECTION OPENING (OJ) ON AN EXPOSED TEE SECTION AT THE END OF EACH CHARGED LINE. A WATER TIGHT SCREW CAP BOTTOM IS EQUIPPED FOR CLEANING (REFER DETAILS).

RAINWATER RE-USE NOTES

PROVIDE SIGN IN PROMINENT LOCATION AT FRONT OF PROPERTY NOTING 'RAINWATER IN USE'

MARK TANK OUTLETS 'NON-POTABLE WATER'

RECYCLED RAINWATER TO SERVICE BUILDING AS PER THE BUILDING FOR TOILET WASHING, MACHINERY AND IRRIGATION.

OVER FLOW FROM RAINWATER TANK TO BE CONNECTED TO STORMWATER SYSTEM.

RAINWATER SUPPLY PIPE WORK ABOVE GROUND MUST BE LABELLED ALONG THEIR LENGTH WITH A PERMANENT MARKING STATING 'RAINWATER' EVERY 500mm OR LESS. BELOW GROUND RAINWATER SUPPLY PIPE WORK MUST HAVE IDENTIFICATION TAPE AT LEAST 75mm WIDE AND MARKED 'RAINWATER' ON THE RAINWATER SUPPLY PIPE AND FASTENED TO THE PIPE AT NOT MORE THAN 3m INTERVALS.

SYDNEY WATER BACKFLOW PREVENTION DEPARTMENT MUST BE CONTACTED REGARDING RECYCLED WATER ON THIS BUILDING AND FOR THE BACKFLOW PREVENTION REQUIREMENTS AND TOP-UP SYSTEM. PHONE SYDNEY WATER ON 132 892

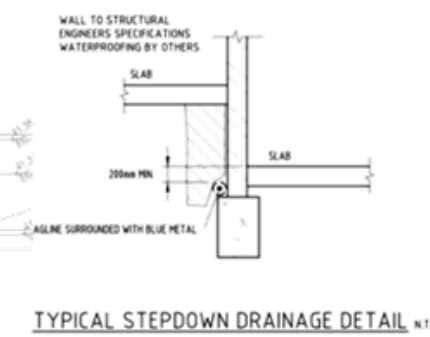
APPROPRIATE WATER METER MUST BE OBTAINED FROM SYDNEY WATER TAKING INTO ACCOUNT RAINWATER RE-USE.

ANY GARDEN OR CARWASH TAPS CONNECTED TO RECYCLED SYSTEM MUST BE LOCATED 150mm MIN ABOVE THE SURFACE.

EVERY FIXTURE SERVED FROM THE RECYCLED WATER SUPPLY MUST BE CLEARLY LABELLED 'RAINWATER'. REFER TO SIGNAGE.

FOR PERIOD OF LOW WATER LEVEL IN THE RAIN WATER TANK A CONNECTION TO THE WATER MAIN IS NEEDED AND TO BE PROVIDED IN ACCORDANCE WITH THE SYDNEY WATER REQUIREMENTS.

PUMPS AND FILTERS ON OUTLETS FROM RECYCLED SYSTEM TO BE SUPPLIED BY OTHERS AS MAY BE REQUIRED.



GROUND FLOOR DRAINAGE PLAN SCALE - 1:100

ALL GUTTERS TO BE MINI-LINE MINIMUM SIZE TO ARCHITECTURAL SPECIFICATION OR AS NOTED ON PLAN.

ALL GUTTERS TO BE PROVIDED WITH LEAF GUARD.

ALL BALCONIES ARE TO HAVE A 45mm DIAMETER OVERFLOW PIPE OR AND 80mm SQUARE OPENING ACTING AS AN OVERFLOW.

ALL DOWNPIPES TO BE 100mm DIAMETER OR 100 x 100mm MIN TO ARCHITECTURAL SPECIFICATIONS OR AS NOTED ON PLAN.

PROVIDE AG LINES BEHIND ALL RETAINING WALLS AND SUBSOIL DRAINAGE AS PER STRUCTURAL ENGINEERING DETAILS. ALL LINES TO DRAIN TO PITS.

- DENOTES PROPOSED LEVEL
- DENOTES EXISTING LEVEL
- DENOTES DOWNPIPE
- ROOFWATER ONLY
- STORMWATER ONLY

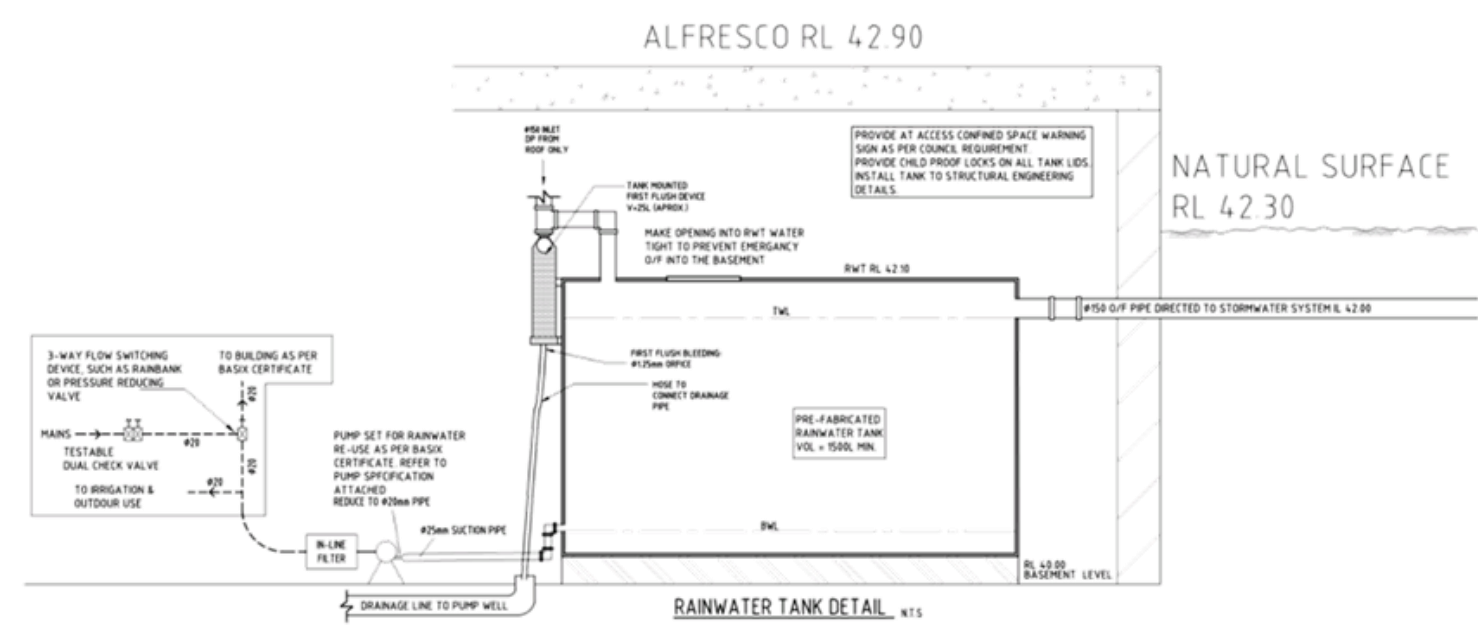
NOTE:
 THE POST DEVELOPMENT HARD SURFACE ON THIS PROPERTY MUST REMAIN UNDER 65% OTHERWISE OSD WILL APPLY TO THIS SITE.



SIGNAGE FOR RAINWATER TANKS AND OUTLETS

- DIMENSIONS: 80mm x 60mm
- BACKGROUND COLOUR SHOULD BE YELLOW.
- TEXT IS WHITE ON A BLACK BACKGROUND.
- TAP SYMBOL IS BLACK.

STRATHFIELD COUNCIL RECEIVED
 DA2019/181
 31 October 2019



NOT FOR CONSTRUCTION

REV	DATE	AMENDMENT DESCRIPTION	DRAWN
B	27.10.2019	ISSUED TO THE ARCHITECT	JDR
A	06.09.2019	ISSUED TO THE ARCHITECT	HE

JOHN ROMANOUS & ASSOCIATES PFL LTD
 CONSULTING CIVIL & STRUCTURAL ENGINEERS ACN 054 595 005
 SUITE 20 / 322 KINGSDROME RD, KINGSDROME NSW 2208 PH (02) 83 87 88 26

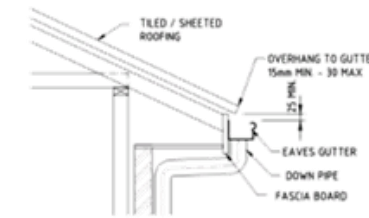
PROPOSED DEVELOPMENT AT:
26 KINGSLAND ROAD, STRATHFIELD

DRAWN	CHECKED	SCALE	DATUM	DRAWING NO.	REV
HE	J.ROMANOUS	AS SHOWN @A1	A.H.D	2036 - S2/3	B

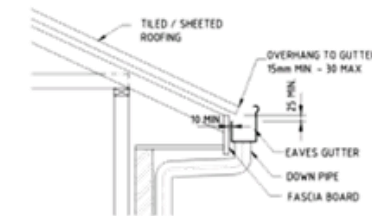


GENERAL NOTES:

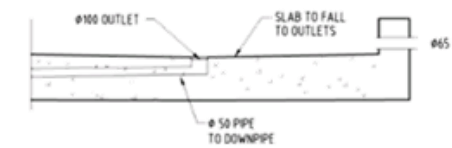
1. ALL SERVICES ARE TO BE LOCATED IN THE FIELD IN CONJUNCTION WITH A RESPONSIBLE OFFICER OF EACH RELEVANT AUTHORITY PRIOR TO COMMENCEMENT OF CONSTRUCTION
2. DRAINAGE PITS ARE TO BE A 450MM SQUARE OR LARGER AS SHOWN, AND FITTED WITH A GALVANIZED GRATE
3. ALL PITS ARE TO HAVE A GALVANIZED GRATE AND FRAME FRAME TO BE CAST INTEGRALLY WITH THE PIT
4. ALL PITS ARE TO BE BENCHED TO HALF PIPE LEVEL
5. PROVIDE STEP IRONS WHERE PIT IS DEEPER THAN 1m AT 450mm CENTRES
6. DRAINAGE PIPES SHALL BE SEWER GRADE UPVC UNLESS OTHERWISE NOTED
7. DRAINAGE PIPE SIZES ARE 100mm DIAMETER UNLESS OTHERWISE NOTED
8. ALL BARE SOIL AREAS ARE TO BE PROTECTED FROM EROSION BY TEMPORARY MEASURES AND REVEGETATED AT THE CESSATION OF CONSTRUCTION
9. THE DOWNHILL BOUNDARY OF THE SITE IS TO BE PROTECTED BY HAY BALES OR A FILTER FABRIC FENCE DURING CONSTRUCTION AS SHOWN IN THE ATTACHED DETAILS
10. THE STREET DRAINAGE PIT LOCATED DOWNHILL OF THE SITE SHALL BE PROTECTED FROM SEDIMENT WITH HAY BALES
11. A SINGLE CONSTRUCTION ENTRANCE SHALL BE ESTABLISHED IN THE MANNER SHOWN IN THE ATTACHED DETAILS
12. THIS DRAWING SHALL BE READ IN CONJUNCTION WITH ARCHITECTURAL AND STRUCTURAL ENGINEERING DOCUMENTS. ANY DISCREPANCIES SHALL BE REPORTED BY THE BUILDER TO THE ARCHITECT PRIOR TO COMMENCEMENT OF THE ITEM
13. THESE PLANS ARE DIAGRAMMATIC AND SHOW THE GENERAL LOCATION OF STRUCTURES AND PIPES. WORK SHALL BE SET OUT ON SITE BY THE SITE FOREMAN & MAY VARY FROM THE PLANS TO THE EXTENT REQUIRED TO ENSURE COMPATIBLE CONSTRUCTION OF OTHER SERVICES AND STRUCTURAL REQUIREMENTS. VARIATION IN LOCATION OF MORE THAN 10M & ANY CHANGES IN SIZE OF ANY COMPONENT NOMINATED HEREON SHALL BE REFERRED TO THE DESIGNER FOR COMMENT
14. IF IN DOUBT, ASK THE SUPERINTENDENT WHO SHALL CONSULT THE DESIGNER.



TYPICAL EAVE GUTTER DETAIL WITH LOW FRONT N.T.S.



TYPICAL EAVE GUTTER DETAIL WITH HIGH FRONT & 10mm GAP TO FACIA N.T.S.



BALCONY OUTLET DETAIL N.T.S.

NOT FOR CONSTRUCTION

REV	DATE	AMENDMENT DESCRIPTION	DRAWN
B	22.10.2019	ISSUED TO THE ARCHITECT	JDR
A	06.09.2019	ISSUED TO THE ARCHITECT	HE

JOHN ROMANOUS & ASSOCIATES PFL LTD CONSULTING CIVIL & STRUCTURAL ENGINEERS ACN 054 595 005			
SUITE 20 / 322 KINGSGROVE RD, KINGSGROVE NSW 2208 PH (02) 83 87 88 26			
PROPOSED DEVELOPMENT AT: 26 KINGSLAND ROAD, STRATHFIELD			
STORMWATER DRAINAGE/SEDIMENT CONTROL DETAILS			
DRAWN	CHECKED	SCALE	DATUM
HE	A.H.D	AS SHOWN @A1	A.H.D
DRAWING NO:			REV
2036 - S3/3			B

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DA2019/181
31 October 2019

Waste Management Plan
October 2019

Site Address:
26 Kingsland, Strathfield

Existing building(s) and/or other structure(s) on site:

- 6 Bedroom two-storey dwelling with garage

Description of proposed development:

- Demolition of two-storey dwelling
- Construction of a two-storey dwelling with basement.



Deconstruction & Demolition Phase

Material	Re-use or sell	Recycle	Dispose
Wood (lumber, timber, flooring)	Timbers, large dimension lumber, plywood, flooring, moulding, lumber longer than 3m	Unpainted and untreated wood unfit for re-use	Painted, treated or rotting timber
Windows and doors	Windows and doors in good condition	Metal frames, screens, unpainted or untreated wood	Glass, unusable painted or treated wood
Cabinets	Entire cabinets	Untreated or unpainted wood	Painted or finished wood
Plumbing products	Sinks, tubs, taps, PVC pipes in good condition	Metal pipes, toilets, taps with lead content	PVC and other plastic piping unfit for re-use, toilet seats
Plasterboard	Plasterboard in good condition	All other uncontaminated plasterboard	Contaminated
Electrical products	All if in good working order (including light fittings, heaters, air conditioners)	Metals (fixtures, conduits), wiring	Ceramic and plastic parts
Bricks and concrete	Any bricks that are in good order and not too contaminated with mortar	All bricks and concrete	n/a
Non-wood floor covering	n/a	Tiles, floating timber floor	n/a
Roofing materials (tiles, sheet steel)	All products if in good condition	Metal materials, terracotta and concrete tiles	n/a
Metal	Beams, downpipes, gutters		

Construction Phase

Material	Re-use and Recycle		Dispose
	On-Site	Off-Site	
Excavation Material	Use cut as fill	Excess excavation material to be sent to Local Recycling Yard	n/a
Bricks	Any left over bricks will be stored on site for future reuse for paving, landscaping, etc.	Excess bricks to be sent back to supplier. Broken bricks to be sent to Local Recycling Yard	n/a
Concrete		Excess concrete to be sent to Local Recycling Yard	n/a
Timber		Good/ clean pieces to be sent back to supplier. All excess material to be sent to Local Recycling Yard	n/a
Plasterboard		Good/ clean pieces to be sent back to supplier. All excess material to be sent to Local Recycling Yard	n/a

Waste Management Plan

October 2019

Material	Re-use and Recycle		Dispose
	On-Site	Off-Site	
Metals		Good/ clean pieces to be sent back to supplier. All excess material to be sent to Local Recycling Yard	n/a
Tiles		Good/ clean pieces to be sent back to supplier. All excess material to be sent to Local Recycling Yard	n/a
Packaging		All packaging to be sent to Local Recycling Yard	n/a

Recycling and Disposal Locations

Materials will be recycled and disposed of at recycling facility:

- BINGO RECYCLING CENTRE – AUBURN
- BINGO RECYCLING CENTRE – GREENACRE
- SYDNEY TRANSWASTE INDUSTRIES – HOMEBUSH WEST
- SITA - AUBURN RESOURCE RECOVERY CENTRE
- ENVIROGUARD - ERSKINE PARK

Ongoing Operation

Waste storage requirements:

- 1 x 120L general waste bin - weekly collection
- 1 x 240L recycling bin – fortnightly collection
- 1 x 240L garden vegetation – fortnightly collection
- 1 x compost bin – daily, as required

Waste storage location:

- Bins Located in Basement with good access to street collection point.
- Compost bin located in garden

BASIX[®]Certificate

Building Sustainability Index www.basix.nsw.gov.au

Single Dwelling

Certificate number: 876195S

This certificate confirms that the proposed development will meet the NSW government's requirements for sustainability, if it is built in accordance with the commitments set out below. Terms used in this certificate, or in the commitments, have the meaning given by the document entitled "BASIX Definitions" dated 06/10/2017 published by the Department. This document is available at www.basix.nsw.gov.au

Secretary
 Date of issue: Wednesday, 30 October 2019
 To be valid, this certificate must be lodged within 3 months of the date of issue.



**STRATHFIELD COUNCIL
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**DA2019/181
 31 October 2019**

Project summary		
Project name	26 Kingsland Road	
Street address	26 Kingsland Road Strathfield 2135	
Local Government Area	Strathfield Municipal Council	
Plan type and plan number	deposited 211	
Lot no.	30	
Section no.	-	
Project type	separate dwelling house	
No. of bedrooms	5	
Project score		
Water	✔ 40	Target 40
Thermal Comfort	✔ Pass	Target Pass
Energy	✔ 50	Target 50

Certificate Prepared by	
Name / Company Name:	Linda Sukkar
ABN (if applicable):	N/A

Description of project

Project address		Assessor details and thermal loads		
Project name	26 Kingsland Road	Assessor number	101225	
Street address	26 Kingsland Road Strathfield 2135	Certificate number	LYU01XJVE1	
Local Government Area	Strathfield Municipal Council	Climate zone	56	
Plan type and plan number	Deposited Plan 211	Area adjusted cooling load (MJ/m ² .year)	21	
Lot no.	30	Area adjusted heating load (MJ/m ² .year)	40	
Section no.	-	Project score		
Project type		Water	✓ 40	Target 40
Project type	separate dwelling house	Thermal Comfort	✓ Pass	Target Pass
No. of bedrooms	5	Energy	✓ 50	Target 50
Site details				
Site area (m ²)	639			
Roof area (m ²)	325			
Conditioned floor area (m2)	331.5			
Unconditioned floor area (m2)	61.4			
Total area of garden and lawn (m2)	162			

Schedule of BASIX commitments

The commitments set out below regulate how the proposed development is to be carried out. It is a condition of any development consent granted, or complying development certificate issued, for the proposed development, that BASIX commitments be complied with.

Water Commitments	Show on DA plans	Show on CC/CDC plans & specs	Certifier check
Landscape			
The applicant must plant indigenous or low water use species of vegetation throughout 85 square metres of the site.	✓	✓	
Fixtures			
The applicant must install showerheads with a minimum rating of 3 star (> 4.5 but <= 6 L/min) in all showers in the development.		✓	✓
The applicant must install a toilet flushing system with a minimum rating of 4 star in each toilet in the development.		✓	✓
The applicant must install taps with a minimum rating of 4 star in the kitchen in the development.		✓	
The applicant must install basin taps with a minimum rating of 4 star in each bathroom in the development.		✓	
Alternative water			
Rainwater tank			
The applicant must install a rainwater tank of at least 1500 litres on the site. This rainwater tank must meet, and be installed in accordance with, the requirements of all applicable regulatory authorities.	✓	✓	✓
The applicant must configure the rainwater tank to collect rain runoff from at least 165 square metres of the roof area of the development (excluding the area of the roof which drains to any stormwater tank or private dam).		✓	✓
The applicant must connect the rainwater tank to: <ul style="list-style-type: none"> • all toilets in the development • at least one outdoor tap in the development (Note: NSW Health does not recommend that rainwater be used for human consumption in areas with potable water supply.) 		✓ ✓	✓ ✓
Swimming pool			
The swimming pool must not have a volume greater than 50 kilolitres.	✓	✓	

Water Commitments	Show on DA plans	Show on CC/CDC plans & specs	Certifier check
The swimming pool must have a pool cover.		✓	
The swimming pool must be outdoors.	✓	✓	

Thermal Comfort Commitments		Show on DA plans	Show on CC/CDC plans & specs	Certifier check
Simulation Method				
The applicant must attach the certificate referred to under "Assessor Details" on the front page of this BASIX certificate (the "Assessor Certificate") to the development application and construction certificate application for the proposed development (or, if the applicant is applying for a complying development certificate for the proposed development, to that application). The applicant must also attach the Assessor Certificate to the application for an occupation certificate for the proposed development.				
The Assessor Certificate must have been issued by an Accredited Assessor in accordance with the Thermal Comfort Protocol.				
The details of the proposed development on the Assessor Certificate must be consistent with the details shown in this BASIX certificate, including the Cooling and Heating loads shown on the front page of this certificate.				
The applicant must show on the plans accompanying the development application for the proposed development, all matters which the Assessor Certificate requires to be shown on those plans. Those plans must bear a stamp of endorsement from the Accredited Assessor to certify that this is the case. The applicant must show on the plans accompanying the application for a construction certificate (or complying development certificate, if applicable), all thermal performance specifications set out in the Assessor Certificate, and all aspects of the proposed development which were used to calculate those specifications.		✓	✓	✓
The applicant must construct the development in accordance with all thermal performance specifications set out in the Assessor Certificate, and in accordance with those aspects of the development application or application for a complying development certificate which were used to calculate those specifications.			✓	✓
The applicant must construct the floors and walls of the dwelling in accordance with the specifications listed in the table below.		✓	✓	✓
Floor and wall construction		Area		
floor - concrete slab on ground		All or part of floor area square metres		

Energy Commitments	Show on DA plans	Show on CC/CDC plans & specs	Certifier check
Hot water			
The applicant must install the following hot water system in the development, or a system with a higher energy rating: gas instantaneous.	✓	✓	✓
Cooling system			
The applicant must install the following cooling system, or a system with a higher energy rating, in at least 1 living area: 3-phase airconditioning; Energy rating: EER 2.5 - 3.0		✓	✓
The applicant must install the following cooling system, or a system with a higher energy rating, in at least 1 bedroom: 3-phase airconditioning; Energy rating: EER 2.5 - 3.0		✓	✓
Heating system			
The applicant must install the following heating system, or a system with a higher energy rating, in at least 1 living area: 3-phase airconditioning; Energy rating: EER 2.5 - 3.0		✓	✓
The applicant must install the following heating system, or a system with a higher energy rating, in at least 1 bedroom: 3-phase airconditioning; Energy rating: EER 2.5 - 3.0		✓	✓
Ventilation			
The applicant must install the following exhaust systems in the development:			
At least 1 Bathroom: individual fan, ducted to façade or roof; Operation control: manual switch on/off		✓	✓
Kitchen: individual fan, ducted to façade or roof; Operation control: manual switch on/off		✓	✓
Laundry: individual fan, ducted to façade or roof; Operation control: manual switch on/off		✓	✓
Artificial lighting			
The applicant must ensure that the "primary type of artificial lighting" is fluorescent or light emitting diode (LED) lighting in each of the following rooms, and where the word "dedicated" appears, the fittings for those lights must only be capable of accepting fluorescent or light emitting diode (LED) lamps:			
• at least 6 of the bedrooms / study;		✓	✓
• at least 3 of the living / dining rooms;		✓	✓
• the kitchen;		✓	✓

Energy Commitments	Show on DA plans	Show on CC/CDC plans & specs	Certifier check
<ul style="list-style-type: none"> • all bathrooms/toilets; • the laundry; • all hallways; 		✓ ✓ ✓	✓ ✓ ✓
Natural lighting			
The applicant must install a window and/or skylight in 4 bathroom(s)/toilet(s) in the development for natural lighting.	✓	✓	✓
Swimming pool			
The applicant must install the following heating system for the swimming pool in the development (or alternatively must not install any heating system for the swimming pool): electric heat pump		✓	
The applicant must install a timer for the swimming pool pump in the development.		✓	
Alternative energy			
The applicant must install a photovoltaic system with the capacity to generate at least 2.6 peak kilowatts of electricity as part of the development. The applicant must connect this system to the development's electrical system.	✓	✓	✓
Other			
The applicant must install a fixed outdoor clothes drying line as part of the development.		✓	

Legend

In these commitments, "applicant" means the person carrying out the development.

Commitments identified with a ✓ in the "Show on DA plans" column must be shown on the plans accompanying the development application for the proposed development (if a development application is to be lodged for the proposed development).

Commitments identified with a ✓ in the "Show on CC/CDC plans and specs" column must be shown in the plans and specifications accompanying the application for a construction certificate / complying development certificate for the proposed development.

Commitments identified with a ✓ in the "Certifier check" column must be certified by a certifying authority as having been fulfilled, before a final occupation certificate (either interim or final) for the development may be issued.

Nationwide House Energy Rating Scheme* Certificate



Certificate Number: LYUO1XJVE1

Date of Certificate: 29 Oct 2019

Star rating: 5.3

**STRATHFIELD COUNCIL
RECEIVED**

**DA2019/181
31 October 2019**

Assessor details

Accreditation number: 101225
 Name: Pranab chakma
 Organisation: Paul & c Associates
 Email: info@basixcertifier.com.au
 Phone: 0490511593
 Declaration of interest: No potential conflicts of interest to declare
 Software: FirstRate5: 5.2.10b (3.13)
 AAO: ABSA

5.3
The more stars the more energy efficient

NATIONWIDE HOUSE ENERGY RATING SCHEME

Predicted annual energy load for heating and cooling based on standard occupancy assumptions

60.9 MJ/m²

For more information on your dwelling's rating see: www.nathers.gov.au

Overview

Dwelling details

Address: 26 Kingsland Road, Strathfield
 Suburb: Strathfield
 State: NSW Postcode: 2135
 Type: New Home NCC Class: Class 1a
 Lot/DP number: 30/211 NatHERS climate zone: 56
 Exposure: suburban

Key construction and insulation materials

(see following pages for details)

Construction: Wall: DOUBLE BRICK FINISH
 Roof: TILE ROOF
 Floor: CONCRETE
 Insulation: R 1.5 GROUND FLOOR
 Wall: EXTERNAL
 Roof: R 6 & R 3.5
 Floor: R 1.5 -GROUND FLOOR ONLY
 Glazing: ALUMINIUM CLEAR

Ceiling penetrations

(see following pages for details)

Sealed: 95
 Unsealed: 0
 TOTAL: 95

NOTE: This total is the maximum number of ceiling penetrations allowed to a ceiling (under a roof) for this certificate. **If this number is exceeded in construction then this certificate IS NOT VALID and a new certificate is required. Loss of ceiling insulation for the penetrations listed has been taken into account with the rating.

Principal downlight type: LED

Net floor area (m²)

Conditioned: 331.5
 Unconditioned: 61.4
 Garage: 222.9
 TOTAL: 615.8

Annual thermal performance loads (MJ/m²)

Heating: 39.9
 Cooling: 21
 TOTAL: 60.9

Window selection - default windows only

Note on allowable window values: Only a 5% tolerance to the nominated SHGC window values shown on page 2 can be used with this rating.

Note: Only a +/-5% SHGC tolerance is allowed with this rating.

NB: This tolerance ONLY applies to SHGC, the U-value can always be lower but not higher than the values stated on page 2.

If any of the windows selected are outside the 5% tolerance then this certificate is no longer valid and the dwelling will need to be rerated to confirm compliance.

Scan to access this certificate online and confirm this is valid.



<https://www.fr5.com.au/QRCodeLanding?PublicId=LYUO1XJVE1>

Plan documents

Plan ref/date: -
 Prepared by: -

* Nationwide House Energy Rating Scheme (NatHERS) is an initiative of the Australian, state and territory governments. For more details see www.nathers.gov.au

Nationwide House Energy Rating Scheme* Certificate

Certificate Number: LYUO1XJVE1

Date of Certificate: 29 Oct 2019

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Building Features

Windows type and performance value

Window ID	Window type	U-value	SHGC
ALM-002-03 A	Aluminium B SG High Solar Gain Low-E	5.4	0.58
ALM-002-01 A	Aluminium B SG Clear	6.7	0.7

Windows schedule

Window ID	Window no.	Height (mm)	Width (mm)	Orientation	Zone name	Outdoor shade
ALM-002-03 A	W03	1950	3600	WNW	LIVING	No
ALM-002-01 A	S02	1550	610	SSW	WC	Yes
ALM-002-03 A	S03	1550	3010	SSW	KITCHEN	No
ALM-002-03 A	E01	2700	4800	ESE	KITCHEN	Yes
ALM-002-01 A	N01	1950	1810	NNE	DINNING+FAMILY	No
ALM-002-01 A	N02	1950	3600	NNE	DINNING+FAMILY	No
ALM-002-03 A	E02	2700	4800	ESE	DINNING+FAMILY	Yes
ALM-002-03 A	W01	1200	3010	WNW	STUDY	Yes
ALM-002-01 A	N03	1200	3010	NNE	STUDY	No
ALM-002-01 A	S01	2700	1950	SSW	LDY	Yes
ALM-002-01 A	N14	1950	3600	NNE	BED2	No
ALM-002-01 A	N13	1950	3600	NNE	BED3	No
ALM-002-01 A	N11	650	3010	NNE	BED4	No
ALM-002-01 A	S15	2400	1930	SSW	BED4	No
ALM-002-03 A	E13	1550	1930	ESE	BED4	No
ALM-002-01 A	E11	1550	970	ESE	BATH	Yes
ALM-002-01 A	S11	1550	970	SSW	BATH	No
ALM-002-01 A	W13	1550	970	WNW	BATH	No
ALM-002-01 A	N12	650	1810	NNE	ENS	No
ALM-002-01 A	W11	2400	3010	WNW	BED	Yes
ALM-002-01 A	N15	650	3010	NNE	BED	No
ALM-002-01 A	W14	2400	3010	WNW	BED5	Yes
ALM-002-01 A	S14	1550	970	SSW	BED5	No
ALM-002-03 A	S12	1550	1930	SSW	HALL	No
ALM-002-03 A	W12	2400	1930	WNW	HALL	Yes
ALM-002-01 A	S13	2400	3010	SSW	corridor	Yes
ALM-002-03 A	E12	2400	1810	ESE	corridor	No
ALM-002-01 A	N16	1550	970	NNE	HALL	Yes

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Nationwide House Energy Rating Scheme* Certificate

Certificate Number: LYUO1XJVE1

Date of Certificate: 29 Oct 2019

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Building Features

Roof windows and skylight type and performance value

ID	Window type	U-value	SHGC
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Roof window and skylight schedule

ID	Roof window/ skylight no.	Area (m ²)	Orientation	Zone name	Outdoor shade	Indoor shade/ diffuser
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External wall type

Type	Insulation	Wall wrap
1: FR5 - AAC Block		No
2: DOUBLE BRICK-PC	Polyurethane rigid foamed aged: R1.5 (R1.5)	No
3: DOUBLE BRICK-PC		No

External wall schedule

Wall type	Area (m ²)	Orientation	Zone name	Fixed shade	Eaves
1: FR5 - AAC Block	41.8	SSW	GARAGE	No	No
1: FR5 - AAC Block	16.4	ESE	GARAGE	Yes	No
1: FR5 - AAC Block	41.9	NNE	GARAGE	No	No
1: FR5 - AAC Block	30.7	WNW	GARAGE	No	No
1: FR5 - AAC Block	8.1	SSW	PLANTROOM -BASEMENT	Yes	No
1: FR5 - AAC Block	14.6	ESE	PLANTROOM -BASEMENT	No	No
1: FR5 - AAC Block	8.2	NNE	PLANTROOM -BASEMENT	No	No
1: FR5 - AAC Block	0.4	WNW	PLANTROOM -BASEMENT	Yes	No
2: DOUBLE BRICK-PC	17	SSW	LIVING	No	No
2: DOUBLE BRICK-PC	1.5	ESE	LIVING	Yes	No
2: DOUBLE BRICK-PC	4.8	NNE	LIVING	Yes	No
2: DOUBLE BRICK-PC	12.3	WNW	LIVING	No	No
2: DOUBLE BRICK-PC	2.5	SSW	WC	Yes	No
2: DOUBLE BRICK-PC	17.9	SSW	KITCHEN	No	No
2: DOUBLE BRICK-PC	17.2	ESE	KITCHEN	Yes	No
2: DOUBLE BRICK-PC	3.3	NNE	KITCHEN	Yes	No
2: DOUBLE BRICK-PC	1.7	WNW	KITCHEN	Yes	No
2: DOUBLE BRICK-PC	28.3	NNE	DINNING+FAMILY	No	No

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Nationwide House Energy Rating Scheme* Certificate

Certificate Number: LYUO1XJVE1

Date of Certificate: 29 Oct 2019

□ Star rating: 5.3



Building Features

2 : DOUBLE BRICK-PC	17.6	ESE	DINNING+FAMILY	Yes	No
2 : DOUBLE BRICK-PC	14.4	WNW	STUDY	Yes	No
2 : DOUBLE BRICK-PC	1.1	SSW	STUDY	Yes	No
2 : DOUBLE BRICK-PC	2.9	WNW	STUDY	Yes	No
2 : DOUBLE BRICK-PC	11.5	NNE	STUDY	No	No
2 : DOUBLE BRICK-PC	9.6	SSW	LDY	Yes	No
2 : DOUBLE BRICK-PC	0.7	NNE	ENTRY	Yes	No
2 : DOUBLE BRICK-PC	5	WNW	ENTRY	Yes	No
3 : DOUBLE BRICK-PC	10.5	NNE	BED2	Yes	No
3 : DOUBLE BRICK-PC	3	WNW	BED3	Yes	No
3 : DOUBLE BRICK-PC	9.7	NNE	BED3	No	No
3 : DOUBLE BRICK-PC	1	SSW	WIR	Yes	No
3 : DOUBLE BRICK-PC	10.2	NNE	BED4	No	No
3 : DOUBLE BRICK-PC	11.7	SSW	BED4	Yes	No
3 : DOUBLE BRICK-PC	16.7	ESE	BED4	No	No
3 : DOUBLE BRICK-PC	12.8	ESE	BATH	Yes	No
3 : DOUBLE BRICK-PC	0.9	NNE	BATH	Yes	No
3 : DOUBLE BRICK-PC	6.2	SSW	BATH	No	No
3 : DOUBLE BRICK-PC	10.1	SSW	BATH	No	No
3 : DOUBLE BRICK-PC	9.7	WNW	BATH	No	No
3 : DOUBLE BRICK-PC	7.8	NNE	ENS	No	No
3 : DOUBLE BRICK-PC	10.8	WNW	BED	Yes	No
3 : DOUBLE BRICK-PC	3	ESE	BED	Yes	No
3 : DOUBLE BRICK-PC	12.5	NNE	BED	No	No
3 : DOUBLE BRICK-PC	12.9	WNW	BED5	Yes	No
3 : DOUBLE BRICK-PC	10	SSW	BED5	No	No
3 : DOUBLE BRICK-PC	6.8	SSW	HALL	No	No
3 : DOUBLE BRICK-PC	13	ESE	HALL	Yes	No
3 : DOUBLE BRICK-PC	6.1	WNW	HALL	Yes	No
3 : DOUBLE BRICK-PC	10.6	SSW	corridor	Yes	No
3 : DOUBLE BRICK-PC	4.8	ESE	corridor	Yes	No
3 : DOUBLE BRICK-PC	3.9	NNE	HALL	Yes	No
3 : DOUBLE BRICK-PC	7	WNW	HALL	No	No

Internal wall type

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Nationwide House Energy Rating Scheme* Certificate

Certificate Number: LYUO1XJVE1

Date of Certificate: 29 Oct 2019

□ Star rating: 5.3



Building Features

Type	Area (m ²)	Insulation
1 : FR5 - Internal Plasterboard Stud Wall	297.6	
2 : DOUBLE BRICK-PC	11.5	Polyurethane rigid foamed aged: R1.5 (R1.5)

Floors

Location	Construction	Area (m ²)	Sub floor ventilation	Added insulation	Covering
GARAGE	FR5 - CSOG: Slab on Ground	222.9	Enclosed	R0.0	none
PLANTROOM -BASEMENT	FR5 - CSOG: Slab on Ground	20.7	Enclosed	R0.0	none
LIVING	FR5 - 200mm concrete slab	29.3	Enclosed	R1.5	Tiles
WC	FR5 - 200mm concrete slab	3.6	Enclosed	R1.5	Tiles
KITCHEN	FR5 - 200mm concrete slab	49.8	Enclosed	R1.5	Tiles
DINNING+FAMILY	FR5 - 200mm concrete slab	70.5	Enclosed	R1.5	Tiles
STUDY	FR5 - 200mm concrete slab	21.8	Enclosed	R1.5	Tiles
LIFT	FR5 - 200mm concrete slab	4	Enclosed	R1.5	Tiles
LDY	FR5 - 200mm concrete slab	13.1	Enclosed	R1.5	Tiles
ENTRY	FR5 - 200mm concrete slab	7.7	Enclosed	R1.5	Tiles
BED2	FR5 - 200mm concrete slab	20.4	Enclosed	R0.0	Timber
BED3	FR5 - 200mm concrete slab	22.2	Enclosed	R0.0	Timber
WIR	FR5 - 200mm concrete slab	1.4	Enclosed	R0.0	Timber
WIR	FR5 - 200mm concrete slab	9.2	Enclosed	R0.0	Timber
BED4	FR5 - 200mm concrete slab	24	Enclosed	R0.0	Timber
BATH	FR5 - 200mm concrete slab	10.7	Enclosed	R0.0	Tiles
LIFT	FR5 - 200mm concrete slab	3.9	Enclosed	R0.0	Tiles
BATH	FR5 - 200mm concrete slab	13.4	Enclosed	R0.0	Tiles
ENS	FR5 - 200mm concrete slab	1.3	Enclosed	R0.0	Tiles
ENS	FR5 - 200mm concrete slab	4.6	Enclosed	R0.0	Tiles
BED	FR5 - 200mm concrete slab	18.5	Enclosed	R0.0	Timber
BED5	FR5 - 200mm concrete slab	0.4	Elevated	R0.0	Timber
BED5	FR5 - 200mm concrete slab	17.1	Enclosed	R0.0	Timber
HALL	FR5 - 200mm concrete slab	29.1	Enclosed	R0.0	Timber
corridor	FR5 - 200mm concrete slab	17	Enclosed	R0.0	Timber
HALL	FR5 - 200mm concrete slab	3.5	Elevated	R0.0	Timber
HALL	FR5 - 200mm concrete slab	1.3	Enclosed	R0.0	Timber

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Nationwide House Energy Rating Scheme* Certificate

Certificate Number: LYUO1XJVE1

Date of Certificate: 29 Oct 2019

□ Star rating: 5.3



Building Features

Ceiling type

Location	Material	Added insulation	Roof space above
GARAGE	FR5 - 200mm concrete slab	R1.5	No
GARAGE	FR5 - 200mm concrete slab	R1.5	No
GARAGE	FR5 - 200mm concrete slab	R1.5	No
GARAGE	FR5 - 200mm concrete slab	R1.5	No
GARAGE	FR5 - 200mm concrete slab	R1.5	No
GARAGE	FR5 - 200mm concrete slab	R1.5	No
GARAGE	FR5 - 200mm concrete slab	R1.5	No
GARAGE	FR5 - 200mm concrete slab	R1.5	No
GARAGE	Plasterboard	R0.0	No
PLANTROOM -BASEMENT	Plasterboard	R0.0	No
LIVING	FR5 - 200mm concrete slab	R0.0	No
LIVING	FR5 - 200mm concrete slab	R0.0	No
LIVING	FR5 - 200mm concrete slab	R0.0	No
LIVING	FR5 - 200mm concrete slab	R0.0	No
WC	FR5 - 200mm concrete slab	R0.0	No
WC	Plasterboard	R3.5	No
KITCHEN	FR5 - 200mm concrete slab	R0.0	No
KITCHEN	FR5 - 200mm concrete slab	R0.0	No
KITCHEN	FR5 - 200mm concrete slab	R0.0	No
KITCHEN	FR5 - 200mm concrete slab	R0.0	No
KITCHEN	Plasterboard	R3.5	No
DINNING+FAMILY	FR5 - 200mm concrete slab	R0.0	No
DINNING+FAMILY	FR5 - 200mm concrete slab	R0.0	No
DINNING+FAMILY	FR5 - 200mm concrete slab	R0.0	No
DINNING+FAMILY	FR5 - 200mm concrete slab	R0.0	No
DINNING+FAMILY	FR5 - 200mm concrete slab	R0.0	No
DINNING+FAMILY	FR5 - 200mm concrete slab	R0.0	No
DINNING+FAMILY	FR5 - 200mm concrete slab	R0.0	No
DINNING+FAMILY	FR5 - 200mm concrete slab	R0.0	No
DINNING+FAMILY	Plasterboard	R3.5	No
STUDY	FR5 - 200mm concrete slab	R0.0	No
STUDY	FR5 - 200mm concrete slab	R0.0	No
LIFT	FR5 - 200mm concrete slab	R0.0	No
LDY	FR5 - 200mm concrete slab	R0.0	No

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Nationwide House Energy Rating Scheme* Certificate

Certificate Number: LYUO1XJVE1

Date of Certificate: 29 Oct 2019

□ Star rating: 5.3



Building Features

LDY	FR5 - 200mm concrete slab	R0.0	No
LDY	Plasterboard	R3.5	No
ENTRY	FR5 - 200mm concrete slab	R0.0	No
BED2	Plasterboard	R6.0	Yes
BED3	Plasterboard	R6.0	Yes
WIR	Plasterboard	R6.0	Yes
WIR	Plasterboard	R6.0	Yes
BED4	Plasterboard	R6.0	Yes
BATH	Plasterboard	R6.0	Yes
LIFT	Plasterboard	R6.0	Yes
BATH	Plasterboard	R6.0	Yes
ENS	Plasterboard	R6.0	Yes
ENS	Plasterboard	R6.0	Yes
BED	Plasterboard	R6.0	Yes
BED5	Plasterboard	R6.0	Yes
HALL	Plasterboard	R6.0	Yes
corridor	Plasterboard	R6.0	Yes
HALL	Plasterboard	R6.0	Yes
HALL	Plasterboard	R6.0	Yes

Ceiling penetrations

Location	Number	Type	Width (mm)	Length (mm)	Seal/ unsealed
LIVING	10	Downlights	50	50	Sealed
WC	1	Exhaust Fans	150	150	Sealed
KITCHEN	15	Downlights	50	50	Sealed
DINNING+FAMILY	15	Downlights	50	50	Sealed
STUDY	6	Downlights	50	50	Sealed
LDY	1	Exhaust Fans	150	150	Sealed
ENTRY	4	Downlights	50	50	Sealed
BED2	4	Downlights	50	50	Sealed
BED3	4	Downlights	50	50	Sealed
BED4	4	Downlights	50	50	Sealed
BATH	1	Exhaust Fans	150	150	Sealed
BATH	1	Exhaust Fans	150	150	Sealed
ENS	1	Exhaust Fans	150	150	Sealed
BED	4	Downlights	50	50	Sealed

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Nationwide House Energy Rating Scheme* Certificate



Certificate Number: LYUO1XJVE1

Date of Certificate: 29 Oct 2019

□ Star rating: 5.3

Building Features

BED5	4	Downlights	50	50	Sealed
HALL	8	Downlights	50	50	Sealed
corridor	8	Downlights	50	50	Sealed
HALL	4	Downlights	50	50	Sealed

Ceiling fans

Location	Number	Diameter (mm)

Roof type

Material	Added insulation	Roof colour
Ceil: Ceiling	0.0	medium
Slab:Slab - Suspended Slab : 200mm: 200mm Suspended Slab	0.0	medium
Disc:Attic-Discontinuous	1.5	dark

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Nationwide House Energy Rating Scheme* Certificate

Certificate Number: LYUO1XJVE1

Date of Certificate: 29 Oct 2019

□ Star rating: 5.3



Additional Information

ALL INSULATION TYPE REPLACABLE WITH SIMILAR R-VALUE. ALL WINDOW FRAME & GLASS REPLACABLE WITH SIMILAR U-VALUE & SHGC.

Explanatory notes

About this report

Residential energy ratings address the quality of the building fabric i.e. walls, windows, floors and roof/ceilings. Ratings do not cover the energy or water efficiency of appliances including heating and cooling, hot water, dishwashers, ovens, fridges, TVs etc. or solar panel or water tank requirements. The efficiency or specification of these items is generally covered by other regulations, standards or guidelines.

General Information

A NatHERS House Energy Rating is a comprehensive, dynamic computer modelling evaluation of the floorplans, elevations and specifications to predict an energy load of a home. Not all of us use our homes in the same way, so ratings are generated using standard assumptions. This means homes can be compared across the country.

The actual energy consumption of your home may vary significantly from the predicted energy load figures in this report depending on issues such as the size of your household and your personal preferences, e.g. in terms of heating or cooling.

While the figures are an indicative guide to energy use, they can be used as a reliable guide for comparative purposes between different house designs and for demonstrating that the design meets the required regulatory compliance.

Homes that are energy efficient use less energy, are warmer in winter, cooler in summer and cost less to run. The higher the star rating the more energy efficient.

This NatHERS House Energy Rating report was carefully prepared by your assessor on the basis of comprehensive modelling using standard procedures to rate your home using an underlying engine developed by the Australian Commonwealth Scientific and Industrial Research Organisation (CSIRO).

All information relating to energy loads presented in this report is based on a range of standard assumptions in order to allow for comparisons with reports prepared for other homes and to demonstrate minimum regulatory compliance. The standard assumptions include figures for occupancy, indoor air temperature and are based on a unique climate file for your region.

Accredited Assessors

To ensure you get a high-quality, professional NatHERS House Energy Rating report, you should always use an accredited assessor, accredited assessors are members of a professional body called an Assessor Accrediting Organisation (AAO).

AAOs have specific quality assurance processes in place and continuing professional development requirements to maintain a high and consistent standard of assessments across the country. Non-accredited assessors do not have this level of quality assurance or any on-going training requirements.

If you have any questions or concerns about this report, please direct them to your assessor in the first instance.

If your assessor is unable to address your questions or concerns, please contact their AAO listed under 'assessor details'. You can also find a range of information about accredited assessors on the AAO websites.

Disclaimer

The energy values quoted are for comparison purposes only; they are not a prediction of actual energy use. This rating only applies to the floor plan, construction details, orientation and climate as submitted and included in the attached drawing set that bears a stamp with the same number as this certificate. Changes to any of these details could affect the rating.

Contact

For more information on the Nationwide House Energy Rating Scheme (NatHERS), visit www.nathers.gov.au

For more information on energy efficient design and insulation visit www.yourhome.gov.au

* Nationwide House Energy Rating Scheme (NatHERS) is an initiative of the Australian, state and territory governments. For more details see www.nathers.gov.au

TO: Strathfield Local Planning Panel Meeting - 6 February 2020
REPORT: SLPP – Report No. 2
SUBJECT: 2019/010/1 - PLANNING PROPOSAL: 11-23 THE BOULEVARDE,
STRATHFIELD - LOT 102 IN DP 597302 & LOT 21 IN DP 623899
DA NO. 2019/010/1

SUMMARY

Proposal: Planning proposal to amend the height of buildings (clause 4.3) and floor space ratio (Clause 4.4) development standards applicable to the site under SLEP 2012, and increase the cap on residential accommodation permitted on the site under Clause 6.8 of SLEP 2012.

Applicant: Memocorp Australia
Owner: Memo Corporation Australia Pty Ltd
Date of lodgement: 24/09/2019
Assessment officer: PF
Zoning: B3 Commercial Core - SLEP 2012
Heritage: Not a heritage item or located within a conservation area.
Located in the vicinity of the following listed items:

- i) State Heritage Listed Strathfield Railway Station Group (SHR01252; Item I199 under Schedule 5 of SLEP 2012) and Strathfield rail underbridges (SHR01055)
- ii) Two storey shops at 35-39 The Boulevarde (Item I200 under SLEP 2012)
- iii) Victorian Shop (“Keary’s Corner”) at 39 The Boulevarde (Item I201 under SLEP 2012)
- iv) The Boulevarde Retail Conservation Area (“C15” on SLEP Heritage Map)

Flood affected: No

RECOMMENDATION OF OFFICER: **THE PLANNING PROPOSAL SHOULD NOT PROCEED TO A GATEWAY DETERMINATION.**

1. EXECUTIVE SUMMARY

1. On 24 September 2019, a planning proposal was lodged (Application No. 2019/010/1) in respect of land at 11-23 The Boulevarde, Strathfield.
2. The site is irregular in shape and has an area of 11,253m². The site has frontages to Churchill Avenue to the north, Redmyre Road to the south, and sectional frontages to Orso Lane and The Boulevarde to the east. The site is within walking distance of Strathfield train station and bus interchange and is located in close proximity to schools, health facilities, and services.
3. The site is currently developed with an eight-storey commercial office tower on the eastern portion of the site and commercial shopping centre (“Strathfield plaza”) comprised of ground floor retail and two levels of car parking above for 500 cars.

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4. The site is located in the Strathfield Town Centre, which encompasses land in Strathfield, Burwood, and Canada Bay LGA's. Council's existing site-specific DCP for the Strathfield Town Centre is obsolete. While there is currently no Masterplan for the Town Centre, the preparation of a Masterplan is a short-term action adopted in Council's *2030 Community Strategic Plan* and identified in Council's Draft Strategic Planning Statement in response to community consultation.
5. The site is currently zoned B3 Commercial Core. The planning proposal does not seek to alter this.
6. A maximum building height of 54m (Clause 4.3, SLEP 2012) and maximum floor space ratio (FSR) of 3:1 (Clause 4.4, SLEP 2012) applies to the site.
7. By virtue of the site being included in "Area 2" on the FSR Map, the provisions of *Clause 4.4B: Exceptions to floor space ratio (Strathfield Town Centre)*, *Clause 6.7: Design Excellence for Strathfield Town Centre*, and *Clause 6.8: Additional provisions for development in Strathfield Town Centre* under SLEP 2012 apply to the site.
8. Under Clause 4.4B, a bonus FSR of up to 5:1 applies to the site. The FSR may be increased to 7.5:1 if the building meets the design criteria specified in Clause 6.7. Clause 6.8 prescribes that the part of the building used for residential accommodation is not to exceed 35% of the floor space of the building.
9. The proposal seeks to amend *Strathfield Local Environmental Plan 2012*, as follows:
 - Amend *Clause 4.3: Height of Buildings* to increase the maximum permitted building height to **156m**.
 - Amend *Clause 4.4: Floor Space Ratio* to increase the maximum permitted FSR to **9.5:1**.
 - Remove the application of *Clause 4.4B: Exceptions to floor space ratio (Strathfield Town Centre)* to the site.
 - Amend *Clause 6.8: Additional provisions for development in Strathfield Town Centre* to increase the maximum part of the building permitted to be used for residential accommodation to **70%** of the floor space.
10. A concept development scheme has been submitted with the planning proposal for a mixed-use development comprised of five towers of varying heights made up of 65.8% residential, 33.5% commercial (17.9% retail, 15.6% office space), and 0.65% dedicated as community space. The mix of uses is distributed across the towers and stacked to provide retail activation at the ground plane, commercial offices above, and residential located at the upper levels. A public plaza with through site links that connects to a new transport hub and the existing train station is also proposed.
11. The form and massing of the concept scheme is considered excessive and incongruous with the built form of the surrounding area. Concerns are also raised regarding the suitability of the proposed land use mix in the Strathfield Town Centre zoned B3 Commercial Core.
12. The planning proposal is generally aligned with the overarching vision of the Greater Sydney Region Plan, *A Metropolis of Three Cities*, to create a 30-minute city that connects people from where they live to jobs, business, schools, health facilities, and services. The proposal would deliver additional housing supply in accordance with *A Metropolis of Three Cities* and the *Eastern City District Plan*.

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13. However, there are no mitigating circumstances or unique site attributes that support consideration of the planning proposal on a piecemeal basis without a Masterplan for the Strathfield Town Centre. To do otherwise, would result in a sub-optimal planning outcome.
14. The absence of planned government investment in Strathfield Train Station at this time further reinforces the need to pool resources together to revitalise the Centre through a planned and coordinated approach to improve connectivity to rail, investigate new opportunities for sustainable transport options, and upgrade the public domain through effective placemaking with fine grain fabric and human scale. This sentiment is echoed in short-term actions in Council's *2030 Community Strategic Plan* and Draft Strategic Planning Statement to revitalise the Strathfield Town Centre and integrate transport services.
15. As such, the proposal is not supported to proceed to a Gateway determination because it lacks strategic and site-specific merit.

2. PURPOSE

The purpose of this Report is to inform the Strathfield Local Planning Panel of a Planning Proposal submitted by Memo Corporation Australia Pty Ltd for 11-23 The Boulevarde Strathfield. A copy of the Planning Proposal is included at **Attachment 1**.

3. BACKGROUND**3.1 SITE DESCRIPTION****Location**

The site known as 11-23 The Boulevarde, Strathfield comprises two lots (Lot 21 in DP623899 and Lot 102 in DP597302), is irregular in shape, and has a total area of approximately 11,253m². The site has frontages to Churchill Avenue, The Boulevarde, Redmyre Road, and Orso Lane.

The site is located within the Strathfield Town Centre (**Figure 1**), which encompasses land within the Strathfield, Burwood and Canada Bay Local Government Areas (LGA's). The municipal boundary between Strathfield and Burwood LGA's runs down the centre line of The Boulevarde with the bulk of the commercial centre falling in Strathfield Council area. The site is located on the southern side of Strathfield Railway Station, approximately 10km west of the Sydney CBD and 12km from Parramatta CBD. The site is within walking distance of Strathfield train station and bus interchange (**Figures 2 & 3**).

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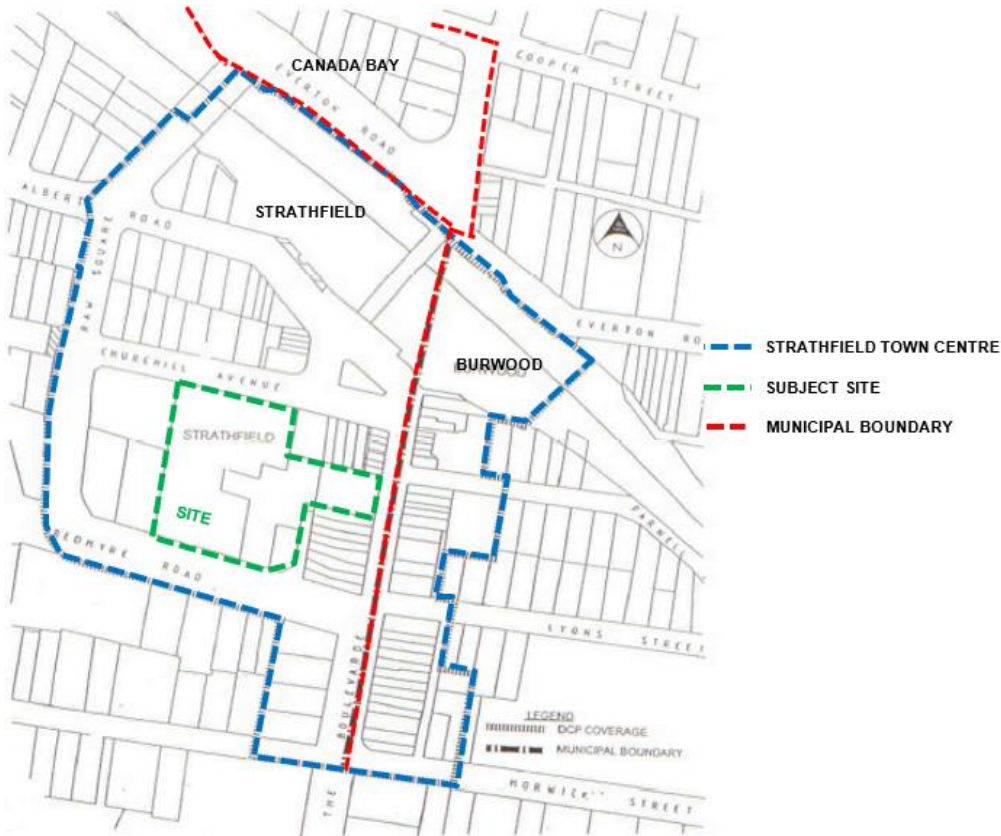


Figure 1. Location of subject site within the Strathfield Town Centre as defined in DCP 13: Strathfield Town Centre.

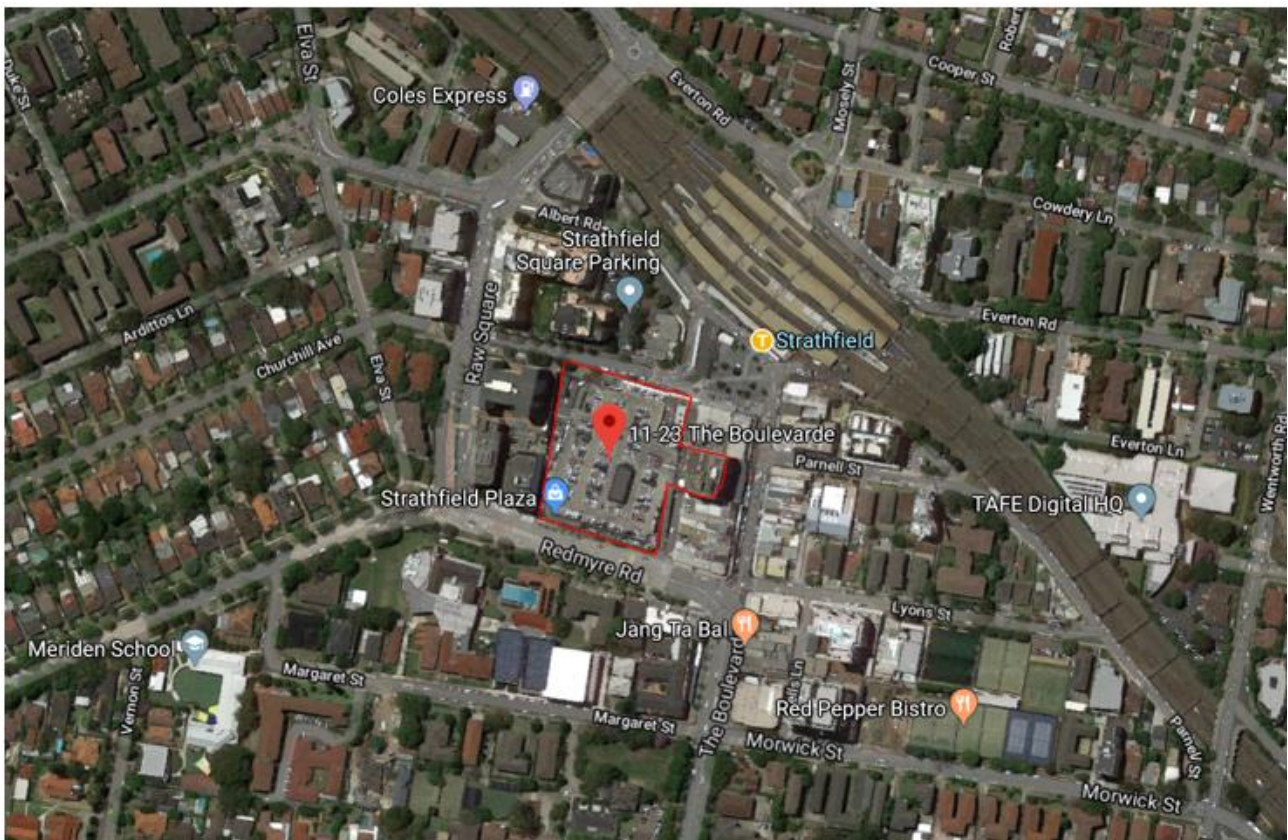


Figure 2. Aerial view showing the location of the subject site (Source: Google Maps).

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Figure 3. Subject site (Source: Google Maps).

Existing buildings

The site is currently developed with an eight-storey commercial office tower on the eastern portion of the site and commercial shopping centre (“Strathfield plaza”) comprised of ground floor retail and two levels of car parking above including at roof level (500 spaces). Pedestrian access is obtained from Redmyre Road and The Boulevard and vehicular access from Churchill Avenue and Redmyre Road via Orso Lane.



Figure 4. Subject site (Source: Grimshaw – Urban Design Report).

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Figure 5. Pedestrian entry to Strathfield Plaza from Churchill Avenue and commercial tower on the eastern portion of the site in the background.



Figure 6. Churchill Avenue pedestrian entry to Strathfield Plaza looking south.

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Figure 7. Pedestrian entry to Strathfield Plaza from The Boulevard and commercial office tower above, on the eastern portion of the site.

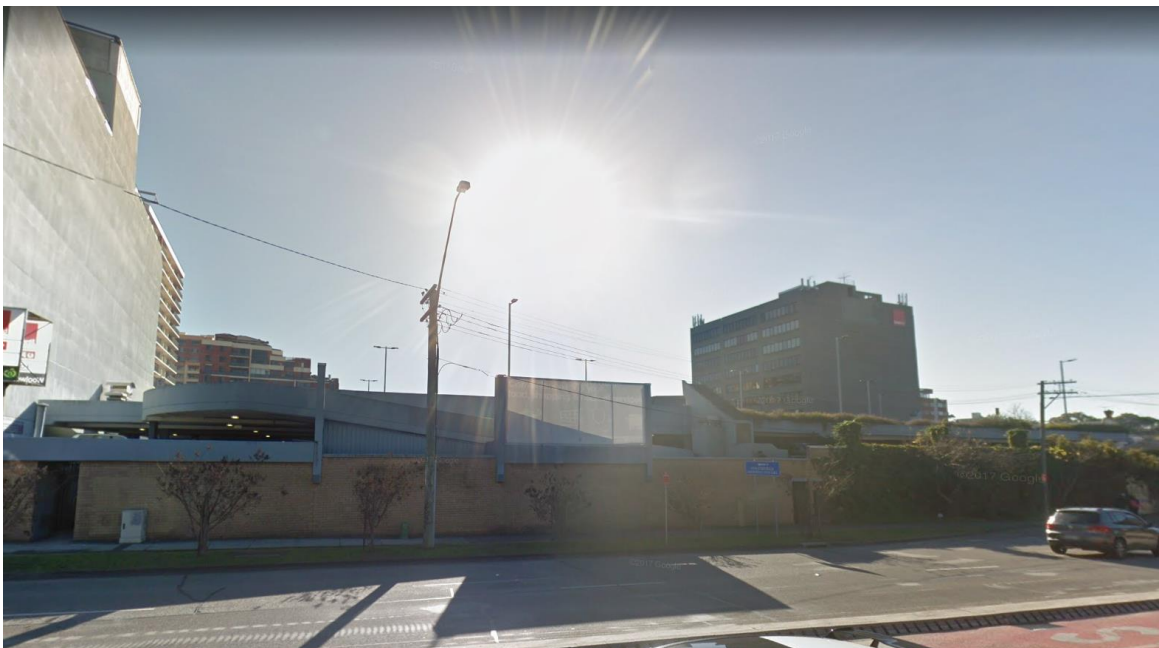


Figure 8. Rear of the subject site fronting Redmyre Road. Vehicular access to the plaza car park is obtained from Redmyre Road via Orso Lane.

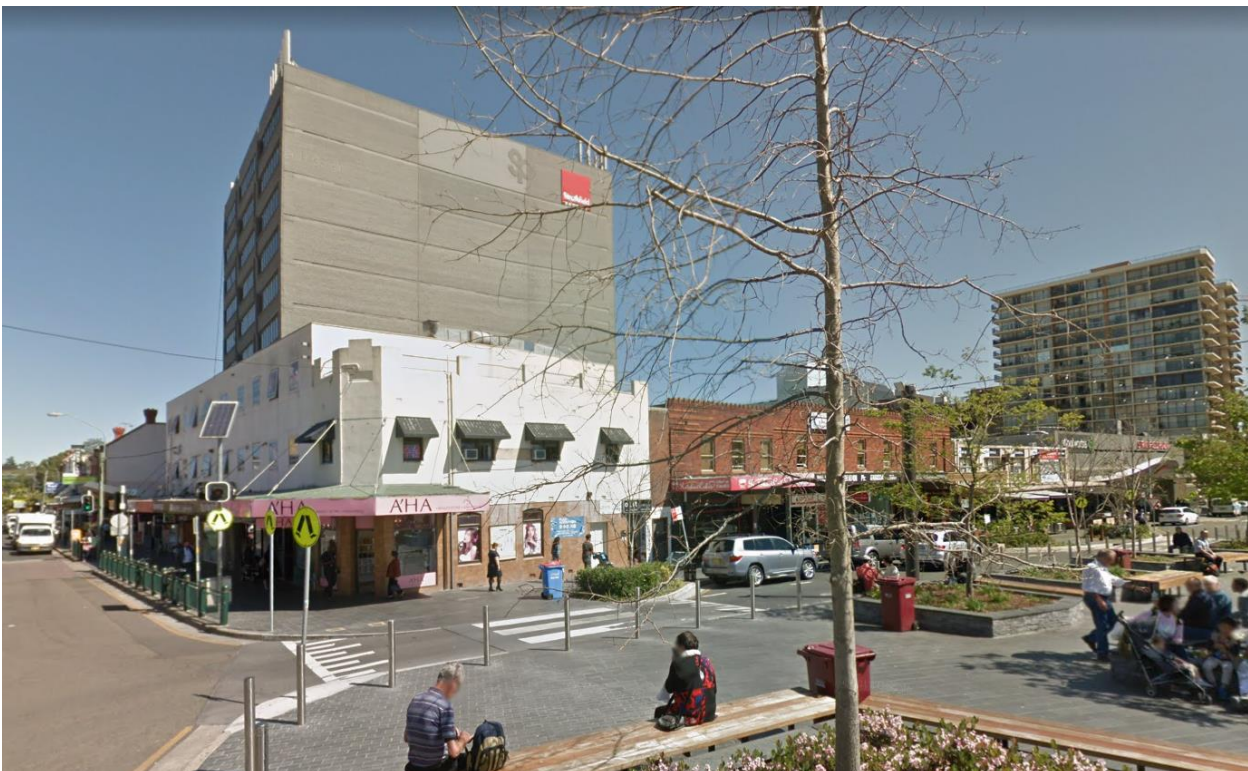
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Figure 9. Existing loading dock and vehicular access to Strathfield Plaza from Churchill Avenue. A survey plan has not been submitted; however, it appears that an easement has been obtained over the adjoining land for access.

To the north

Existing development to the north of the site on land at 1, 3-9 The Boulevard & 2-10 Churchill Avenue, Strathfield is comprised of 2-storey commercial buildings (**Figure 10**). Approval has been granted by the Sydney Central Planning Panel for the construction of an 11-storey commercial/retail development on the site (**Figure 11**).



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Figure 10. Existing 2-storey commercial buildings adjoining the site to the north at 1, 3-9 The Boulevard and 2-10 Churchill Avenue, Strathfield.



Figure 11. Artist impression of the approved development at 1, 3-9 The Boulevard and 2-10 Churchill Avenue, Strathfield and land use breakdown inset (Source: Integrated Design Group Architects).

Development further to the north of the site comprises the public domain of Strathfield Square providing connection to Strathfield train station and bus interchange (**Figure 12**).



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Figure 12. *Strathfield square public domain to the north of the site, providing connection to Strathfield train station and bus interchange.*

Existing development to the north-east on the eastern side of The Boulevard (Burwood LGA) is characterized by existing 2-storey traditional commercial/shop-top housing development (**Figure 13**). This land is zoned B4 Mixed Use.



Figure 13. *Existing development on the eastern side of The Boulevard (Burwood LGA) and Strathfield Train Station in background.*

Existing development to the north of Strathfield Plaza comprises a public at-grade car park and 2-storey commercial buildings (**Figure 14**). To the rear of the commercial buildings, approval has been granted for the construction of a 9-storey mixed use development at 4-10 Albert Road comprised of car parking at ground, mezzanine and Level 1, commercial uses at levels 2-3 and serviced apartments at levels 4-7 (**Figures 15 & 16**).



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Figure 14. At-grade public parking area and adjoining 2-storey commercial development to the north of Strathfield Plaza.



Figure 15. Existing development (restaurant) on land at 4-10 Albert Road.



Figure 16. Artist impression of the approved 9-storey mixed use development on a triangular shaped allotment at 4-10 Albert Road (Albert Road elevation).

To the north-east of the site is the bus interchange (**Figure 17**). This continues to Albert Road (**Figure 18**).

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Figure 17. Southern portion of the bus interchange to the north-east of the site.



Figure 18. Bus interchange on Albert Road.

Existing development to the north-west of Strathfield Plaza significantly increases in density as it approaches the prominent corner site at the intersection of Churchill Avenue and Raw Square (**Figure 19**).

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Figure 19. Existing development to the north-west of Strathfield Plaza increased in density towards the intersection of Churchill Avenue and Raw Square.

The gateway to Strathfield Town Centre where Raw Square intersects with Churchill Avenue and Albert Road to the north-west and west of the site is defined by high-rise mixed-use towers (predominantly residential) with 3-storey podiums to the street alignment and ground floor retail uses (**Figure 20**).



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Figure 20. *The Gateway to Strathfield Town Centre on the Corner of Albert Road and Raw Square. Traffic flow is restricted to one-way movements through the Centre. The prominence of the built-form at the entry (estimated construction early 1990s) is clearly visible.*

Outside of the Centre, 11 storey mixed-use developments have been constructed on land at 38-10 Albert Road and 23-25 Churchill Avenue, Strathfield on land zoned B4 Mixed Use (**Figure 21**).



Figure 21. *11-storey mixed use developments constructed outside the Town Centre at the intersection of Raw Square and Albert Road.*

To the west

To the immediate west of the site is an existing 16-storey residential tower at 30-34 Churchill Avenue, Strathfield (*Strathfield Plaza Tower*) (**Figure 22**). While no survey has been submitted, it appears that access to the shopping centre car park is obtained over the adjoining land at 30-34 Churchill Avenue. To the rear of Strathfield Plaza Tower and to the west of the site fronting Redmyre Road are commercial office buildings of 6 and 7-storeys (**Figure 23**).

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Figure 22. Strathfield plaza tower on the adjoining land to the west.



Figure 23. Existing commercial buildings to the west of the site fronting Redmyre Road.

Churchill Avenue to the west of Raw Square is generally characterised by low-density residential development. The prominence of existing built-form on the corner of Churchill Avenue and Raw Square is visible from the top of Churchill Avenue to the west of Raw Square (**Figure 24**).

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Figure 24. The prominence of existing built-form on the corner of Churchill Avenue and Raw Square (pedestrian Gateway to Strathfield Town Centre) is visible from the top of Churchill Avenue to the west of Raw Square.

To the south

The site adjoins two-storey shop-top housing to the south, which form The Boulevard Retail Conservation Area (**Figure 25**). The two corner sites on the corner of Redmyre Road and The Boulevard are listed as heritage items of local significance. On the southern side of Redmyre Road to the south-west of the site is Meriden school (**Figure 26**). Low-density residential development is located further to the south-west.



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Figure 25. *Development fronting the Boulevard within ‘The Boulevard Retail Conservation Area to the immediate south of the site.*



Figure 26. *Meriden School*

To the east

Land to the east of the site on the eastern side of The Boulevard is located within Burwood LGA and is zoned B4 Mixed Use. These sites are occupied by traditional 1 and 2-storey commercial buildings fronting The Boulevard (**Figure 27**).



Figure 27. *Existing development to the east of the site.*

3.2 RELEVANT APPLICATION HISTORY

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Planning Proposals			
Ref	Address	Description	Status
PP_2017_STRAT_004_001	2-6 Pilgrim Avenue, 9 & 11-13 Albert Road, Strathfield	Planning proposal to amend SLEP 2012 to increase the height and FSR standards applying to the site (280 dwellings). Proposed to increase FSR from 3.5:1 to 5:1 and increase maximum height from 35m to 54m. Site is zoned B4 mixed use. Concept development scheme includes four buildings ranging from 10 to 15 storeys. Predominantly residential with ground floor commercial tenancies.	Post Exhibition
Development Applications			
DA No	Address	Description	Status
Sydney Central Planning Panel Ref. 2016SYE067	1, 3-9 The Boulevard & 2-10 Churchill Avenue, Strathfield	Demolition of existing buildings and construction of an 11-storey mixed-use development containing 1,853m ² retail floor space, 4,951m ² commercial floor space, one level containing plant (Level 10) and 24 residential units.	Approved 16/11/2017 by Sydney Central Planning Panel – 2016SYE067)
DA138/2017	4-10 Albert Road, Strathfield	Demolition of existing structures and construction of a 9-storey mixed use development. Ground floor retail; parking at-grade, mezzanine and level 1; office space at levels 2 and 3; 12 x 2-bed serviced apartments in total across levels 5-7.	Approved 05/07/2018 by the Strathfield Local Planning Panel

The relevant planning proposal and development application history relied upon by the applicant is shown on the plan of Strathfield Town Centre, below (**Figure 28**). The most recent approval within the Town Centre is for an 11-story commercial development on the adjoining land to the north at 1, 3-9, and 2-10 Churchill Avenue, Strathfield.

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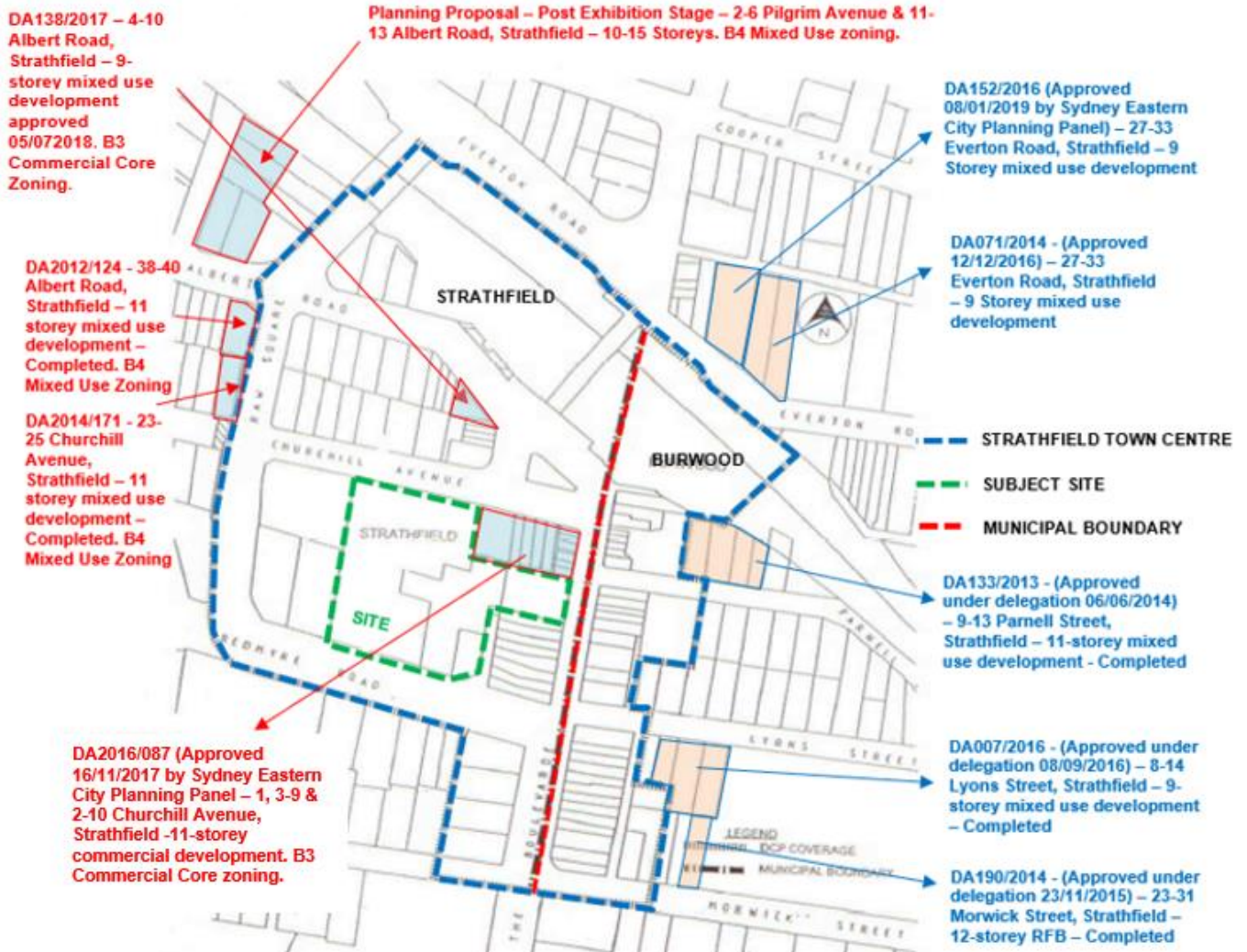


Figure 28. Recent development applications and planning proposals relied upon by the Applicant to demonstrate the transition occurring in proximity to the site. Note: The sites are described in the applicant’s statement as being located within the Strathfield Town Centre. Only, the approved development at 1, 3-9 The Boulevard & 2-10 Churchill Avenue, Strathfield is located within the Town Centre.

3.3 CURRENT PLANNING CONTROLS

3.3.1 Strathfield Local Environmental Plan (SLEP) 2012

Land Use Table

SLEP 2012	Zoning	Objectives	Figure
Land Use Table	B3 Commercial Core	<ul style="list-style-type: none"> To provide a wide range of retail, business, office, entertainment, community and other suitable land uses that serve the needs of the local and wider community. To encourage appropriate employment opportunities in accessible locations. To maximise public transport patronage and encourage walking and cycling. 	29

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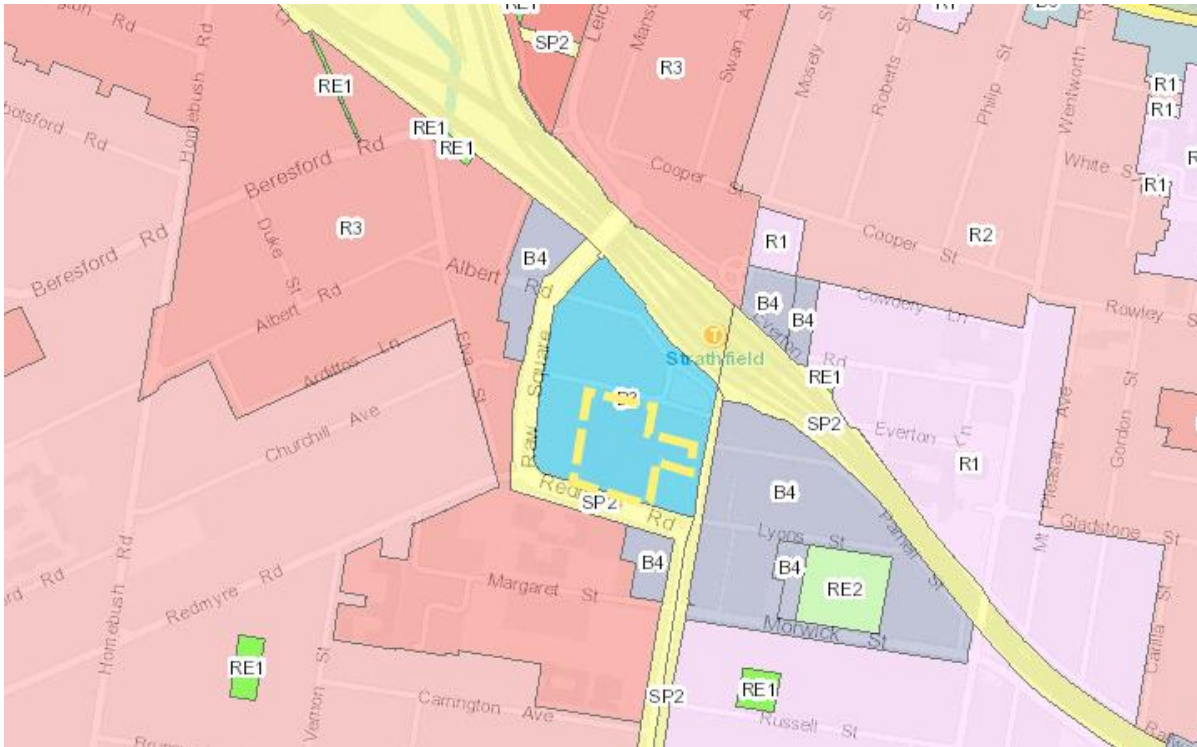


Figure 29. Land Zoning Map – SLEP 2012 (Source: www.planningportal.nsw.gov.au).

Part 4: Principal development standards

SLEP 2012	Development Standard	Control	Figure
Clause 4.3	Height of Buildings	54m	30
Clause 4.4	Floor Space Ratio	3:1	31
Clause 4.4B	Exceptions to Floor Space Ratio (“Area 2” Strathfield Town Centre)	FSR may exceed 3:1 if the size of the lot exceeds 1,500m ² , but: (a) Must not exceed 5:1, or (b) If the building meets the design excellence criteria specified in clause 6.7, must not exceed 7.5:1.	32

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Figure 30. Height of Buildings Map – SLEP 2012 (Source: www.planningportal.nsw.gov.au).

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Figure 31. FSR Map – SLEP 2012 (Source: www.planningportal.nsw.gov.au).

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Figure 32. FSR Map showing the subject site within Area 2 (Source: www.legislation.nsw.gov.au).

Part 5: Miscellaneous Provisions

Clause 5.10 – Heritage conservation

The site is located within the vicinity of land within a heritage conservation area (The Boulevard Retail Conservation Area), listed items of local significance at 35-39 The Boulevard, and state significant Strathfield Railway Station (Figure 33).

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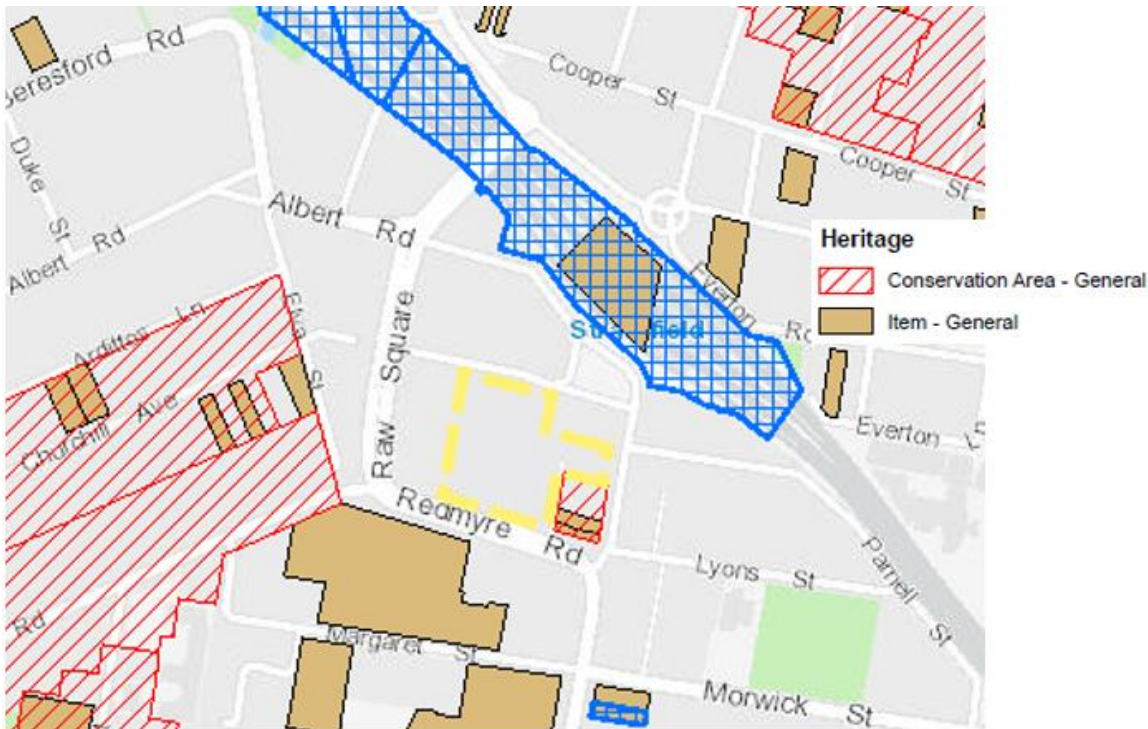


Figure 33. Heritage Map – SLEP 2012 (Source: www.planningportal.nsw.gov.au).

Part 6: Additional Local Provisions

Clause 6.7 - Design excellence for Strathfield Town Centre

- 1) The objective of this clause is to deliver the highest standard of architectural and urban design.
- 2) Applies to “Area 2” on the FSR Map.
- 3) Consent must not be granted unless the consent authority is satisfied that the proposal:
 - a) *has a high standard of architectural, landscape and urban design, will be constructed with quality materials and has a high standard of detailing that reflects the building type, location and the surrounding buildings, and*
 - b) *will significantly improve the quality and amenity of the public domain through the form, external appearance and ground level detailing of the development that addresses any heritage or streetscape issues, and*
 - c) *will not detrimentally impact on the amenity of the surrounding area, nor on any view corridors, vistas or landmark locations, and*
 - d) *will not detrimentally impact on pedestrian movements and experience, but will reinforce the public transport interchange as the focal point of movement for the area and facilitate the ease of such movement, and*
 - e) *is designed to encourage an integrated land use mix that reflects the desired future high-quality town centre that supports a vibrant economic location, a lively social and community hub, and a diversity of public open spaces at the ground level, as well as the roof and other levels of the building, and*
 - f) *includes building massing, modulation and bulk that is appropriate in the context of surrounding buildings in terms of its separation, setback, building street height and amenity and its relationship to such buildings, and*
 - g) *has achieved a high level of ecologically sustainable design, including low-energy or passive design, and minimises environmental impacts such as overshadowing, wind effects and reflectivity, and*
 - h) *supports designed sustainable urban mobility through a high-quality pedestrian environment, high quality provision for cycling infrastructure, high quality service access, circulation and vehicular design.*

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- 5) Consent must not be granted unless an architectural design competition that is consistent with the Design Excellence Guidelines has been held in relation to development that is greater than 30m in height and has a capital value of more than \$20,000,000.
- 6) The indicative concept scheme does not meet the architectural design competition exemption criteria under subclause (6).

Note: *Design Excellence Guidelines* means the Design Excellence Guidelines adopted by the Council before the commencement of this Plan, or if none have been adopted by the Council, the Design Excellence Guidelines issues by the Director-General and in force as at the commencement of this Plan. Council has not adopted any design excellence guidelines.

Clause 6.8: Additional provisions for development in Strathfield Town Centre

Clause 6.8 of SLEP 2012 applies additional provisions for development in the Strathfield Town Centre. Specifically, clause 6.8 requires that the part of the building that will be used for residential accommodation will not exceed 35% of the floor space of the building.

3.4 STRATEGIC PLANNING CONTEXT

3.4.1 Greater Sydney Region Plan: A Metropolis of Three Cities (March 2018)

The planning proposal is generally aligned with the overarching vision of the Greater Sydney Region Plan, *A Metropolis of Three Cities* (the Plan), to create a 30-minute city that connects people from where they live to jobs, business, schools, health facilities, and services (**Figures 34 & Figure 35**).

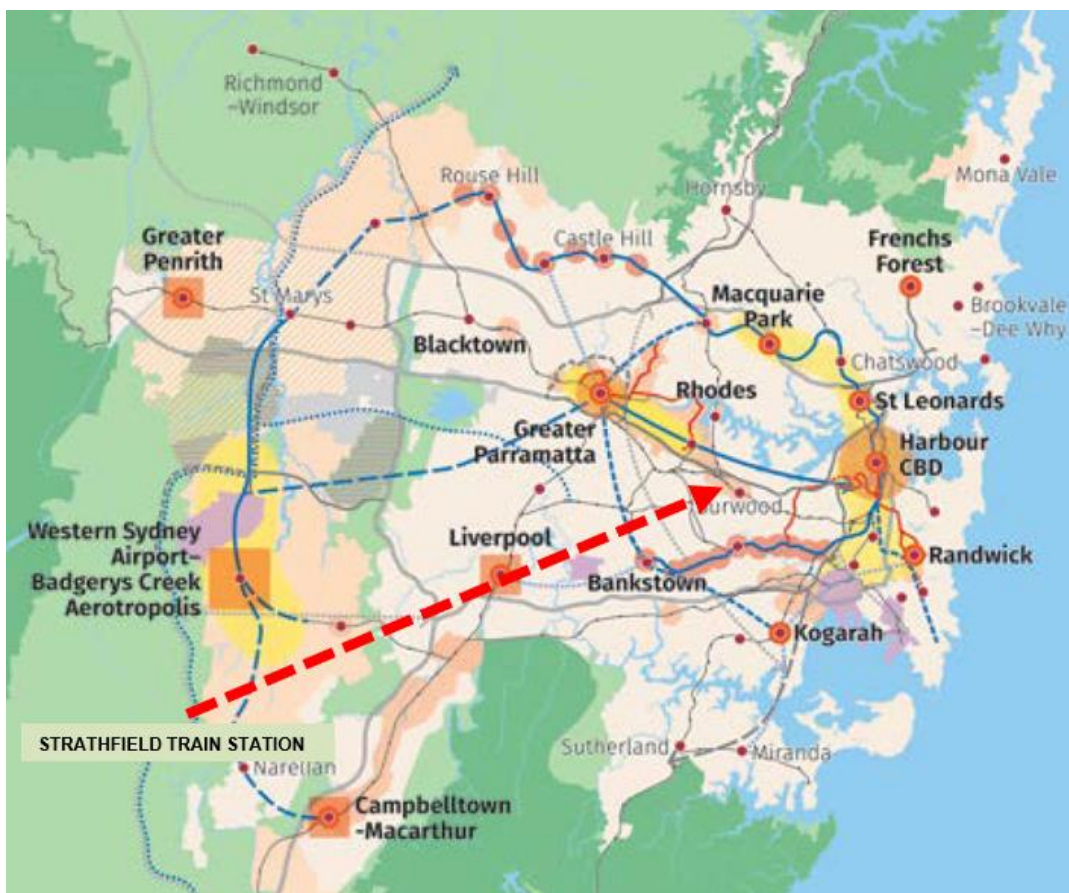


Figure 34. Spatial characteristics and connectivity in the 'Metropolis of Three Cities' (Source: Greater Sydney Region Plan: A Metropolis of Three Cities – Connecting people).

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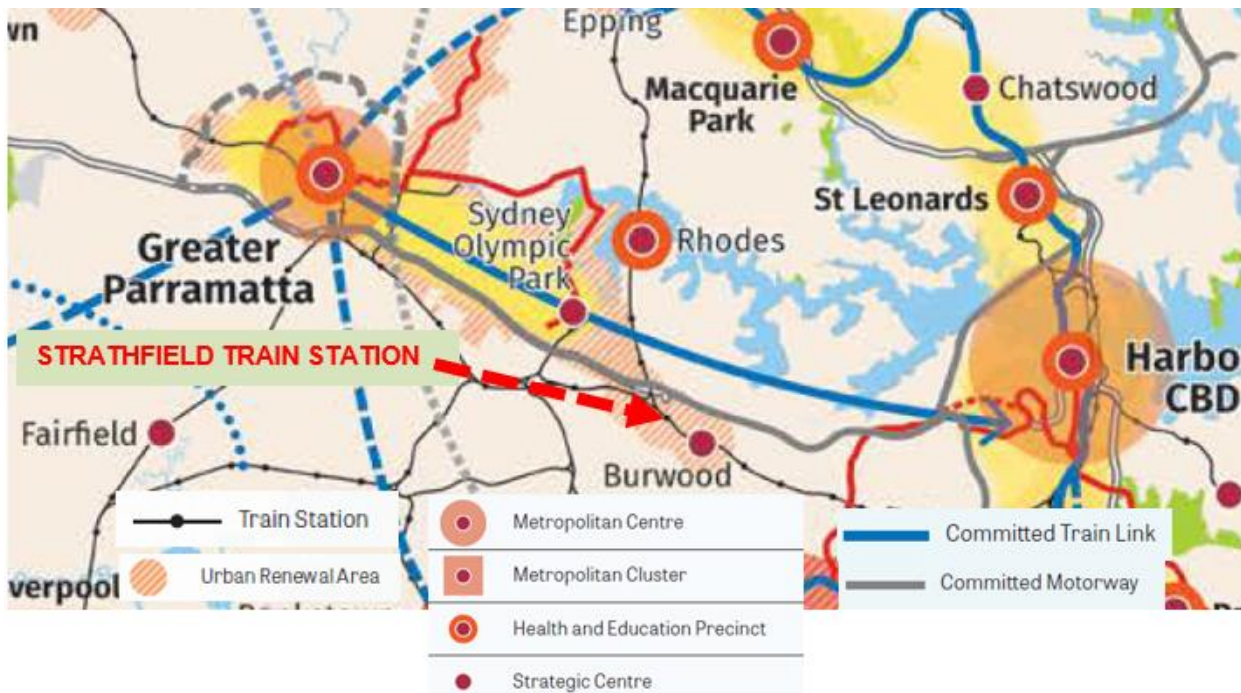


Figure 35. Strathfield Train Station's location within an 'Urban Renewal Area' between the two Metropolitan Centres of the Harbour/CBD and Greater Parramatta.

The subject site is located within an 'Urban Renewal Area' between the two (2) 'strategic centres' of Burwood and Sydney Olympic Park. In established areas like Strathfield Train Station and surrounds, the focus of urban renewal is transformative as sites are redeveloped to transition from suburban to walkable urban centres connected by high frequency public transport services with enhanced interchanges.

The subject site is located within walking distance of Strathfield Train Station and bus interchange. These attributes support additional density on the site under the planning proposal. However, in my opinion, redeveloping the site in isolation and allowing the planning proposal to proceed to a Gateway determination without a Masterplan for the Strathfield Town Centre as a whole, would deliver a sub-optimal planning outcome. There is no public benefit to be gained by accelerating this process.

Alignment with investment in infrastructure is another key feature of *Urban Renewal Areas*. No upgrades are planned for Strathfield Station. A new Metro Station is planned at 'Strathfield North' and is intended to improve connections to key employment and education precincts and take existing pressure off Strathfield Station as well as help service the growing Homebush Precinct.

The absence of planned government investment in Strathfield Train Station at this time further reinforces the need to pool resources together to revitalise the Centre through a planned and coordinated approach to improve connectivity to rail, investigate new opportunities for sustainable transport options, and upgrade the public domain through effective placemaking with fine grain fabric and human scale. This sentiment is echoed in short-term actions in Council's *2030 Community Strategic Plan* and Draft Strategic Planning Statement to revitalise the Strathfield Town Centre and integrate transport services.

While the strongly weighted residential component of the indicative concept scheme is aligned with the Plan's vision to create liveable and connected communities, the current B3 *Commercial Core* zoning of the site does not accord with this. A B4 Mixed Use zoning is likely to be more appropriate.

3.4.2 Eastern City District Plan (March 2018)

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The Eastern City District Plan (March 2018) (the District Plan) provides a short-term (20 year) plan to manage growth and implement the Greater Sydney Region Plan at the district level.

Strathfield along with Homebush and Burwood is identified as a *Planned Precinct* to deliver additional capacity for housing supply. In this regard, the planning proposal aligns with the District Plan. Albeit, the planning proposal has not demonstrated site-specific merit for delivering additional housing supply, noting housing targets are set for the precinct as a whole.

Strathfield Town Centre, of which the site is only part of, is the gateway to high-frequency rail and bus interchanges with access generally via the public domain of Raw Square. The overarching goal for redevelopment of the Centre should be to create a seamless transition to public transport for residents within the Centre and the surrounding area. As such, to consider planning proposals on a piecemeal basis without a coordinated approach would deliver a sub-optimal planning outcome.

One of the principles for local centres under the District Plan is to “*protect or expand retail and/or commercial floor space*”. The Strathfield Town Centre is the major commercial centre within the Strathfield LGA. The proposal to replace the 35% cap on residential with 70% residential in a B3 Commercial Core zone site does not accord with his.

3.4.3 Strathfield Residential Land Use Study (2011)

The Strathfield Residential Land Use Study (2011) is a local government strategy that identifies opportunities to increase capacity for housing in the Eastern City District. While the study was prepared in 2011, it identifies the need for a review of Council’s planning controls in the Town Centre.

A key finding of the study is the identification of land around the Parramatta Road corridor and rail line corridors as the most suitable to deliver additional housing supply.

The Land Use Study recommends the portion of the site occupied by Strathfield Plaza for increased residential capacity in the Strathfield Town Centre. The Study recommends rezoning of the Strathfield Plaza Site to B4 Mixed Use Residential, and allowing a maximum FSR up to 6.5:1 (FSR of 3:1 + 3.4 bonus) and maximum building height of 22 storeys (90m).

Development constraints identified in the Study to residential development in the Strathfield Town Centre include rail noise and traffic noise from The Boulevarde.

3.4.4 Strathfield 2030 Community Strategic Plan (Adopted 7/05/2019)***Key comments:***

- The major commercial centre is in the Strathfield Town Centre (s5.6).
- The Strathfield Plaza site is a large, relatively underutilised site that provides the opportunity for renewal.
- Development constraints include a substantial number of heritage items and conservation areas to the immediate south and west of the town centre.

Demography

- As at 30 June 2017, the estimated residential population of the Strathfield Council area was 43,585. It is estimated that by 2031, the population will number over 60,000 people. The areas of highest population increase are in the transport corridors of Homebush, Homebush

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West and Strathfield Town Centre due to the building of new units on land rezoned for medium to high-rise development.

Issues that are important to the Community

Transport

- “Traffic congestion and local traffic movement is creating difficulties moving around, particularly around the Strathfield Town Centre.”

Town Centres

- The community identified that town centres and villages need to be revitalised and more engaging and welcoming as residents travel outside of the LGA for shopping and eating experiences due to lack of diversity in shops and businesses and there are perceptions of limited activation of commercial or retail strips. **It was regularly commented that Strathfield Town Centre needs revitalisation and integrated transport.”**
- Strathfield 2030 **Goal 3.1: Engaging and activated public places.** Action endorsed in the 2018/2019 Operational Plan to “**Develop plans for upgrading Strathfield Town Centre and integrating transport services**”.

3.4.5 Strathfield 2040 Draft Local Strategic Planning Statement (October 2019)

Council’s 2040 Draft Strategic Local Planning Statement defines the 20-year land use vision for the LGA following public consultation with the community and aims to give effect to the *City Eastern District Plan*. The Statement has not been formally endorsed at this stage.

Relevant Actions (0-5 years) under the Strategic Planning Statement as they relate to the Strathfield Town Centre are listed below:

Action	Description	Timing for Delivery
A5	Work collaboratively with Burwood Council, Transport for NSW, RMS, and the Department of Planning and Environment to identify opportunities for infrastructure upgrades within the Strathfield Town Centre such as active travel paths, electric vehicle charging stations, energy efficient lighting, water bubblers and shade.	Medium term
A6	Work collaboratively with Burwood Council in the planning and future direction of the Burwood Strategic Centre to ensure that future land uses do not compete with the function of the Strathfield Town Centre and Strathfield residents benefit from jobs provided within the Burwood Strategic Centre.	Medium term
A17	Prepared a Pedestrian Access and Mobility Plan (PAMP) for Strathfield Town Centre	Short term
A50	Ensure that high density residential development is limited to the Parramatta Road Urban Transformation Corridor, Liverpool Road Corridor and the Strathfield Town Centre.	Immediate term
A61	Investigate options to attract and retain a mix of retail and office development in the Strathfield Town Centre	Immediate term

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A63	<p>Work collaboratively with Burwood Council and key landowners to prepare a Masterplan for Strathfield Town Centre to fulfil its potential and strengthen its economic role within the LGA and District. In particular to:</p> <ul style="list-style-type: none"> • Capitalise on its unique offerings as a cultural centre and day/night time capacity • Focus on human (fine grain) scale public place that encourages diversity and social connectivity • Embrace the Centre's cultural offering • Deliver diverse service/retail/dining/entertainment and commercial options • Enhance the Centre's night time economy • Attract and retain high quality office space • Improve pedestrian and mobility accessibility • Ensure development to the heritage of Strathfield Station • Deliver parking that can be adapted to other future uses. • Provide well-designed built and natural shade 	Immediate to Medium term
A79	<p>Work collaboratively with Burwood Council to develop a Place Strategy (in conjunction with a Masterplan) for the Strathfield Town Centre with a focus on enhancing the Centre's food and entertainment offering (diversity), while still providing services to meet the community's needs.</p>	Short to Medium term

Key comments

- *“Strathfield Town Centre is shared amongst Strathfield, Canada Bay and Burwood LGAs. Future planning for the Town Centre should be in collaboration with Canada Bay and Burwood Councils to ensure that it fulfils its potential by providing a greater diversity of uses, services, dining, and entertainment choices, which complement the adjoining strategic centres of Burwood and Rhodes. It is important that these centres provide the services that our residents need and that they are activated to provide additional jobs and services in the future, with improved connectivity to nearby housing and open space.”*
- *“Our community supports a revitalised Strathfield Town Centre with a diversified range of uses and better integrated transport options. Council to work collaboratively with neighbouring Councils and land owners to ensure that the Strathfield Town Centre fulfils its potential as an active, vibrant and well-designed centre.”*

Upcoming studies and Strategies

- Strathfield Town Centre Masterplan
- Strathfield Town Centre Place Strategy

3.4.6 Strathfield – An Economic Land Use and Employment Strategy (June 2010)

Key comments:

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- Spatial strategy identified for Strathfield town centre - To consolidate retail, commercial and service provision; encourage entertainment and recreation facilities.
- Strathfield Town Centre has the potential to play a more prominent role as a hub for local cultural and business activity - Opportunities include extending the centre beyond its current boundaries, diversifying the mix of uses and variety of retailing available within the Centre and improving local transport connections with surrounding suburbs and employment lands.
- Strathfield Town Centre is divided between Strathfield, Canada Bay and Burwood LGAs. Within Strathfield's portion of the centre there is Strathfield Plaza, main street retailing and some high -density housing development. Strathfield is hemmed in by low density residential development. Strathfield Town Centre is focused on the railway station. Road connections are strong to other parts of Sydney. Bicycle accessibility is weak.
- Implement a coordinated approach to planning – Council should work with Canada Bay and Burwood Councils to ensure an integrated approach is pursued for Strathfield Town Centre to address cross-boundary issues.

3.4.7 DCP 13 - Strathfield Town Centre

The provisions contained in the site-specific DCP for the Strathfield Town Centre (Adopted September 2005) are obsolete.

4. PLANNING PROPOSAL

4.1 PROPOSED AMENDMENTS TO SLEP 2012

Part 4: Principal development standards

SLEP 2012	Standard	Existing Control	Proposed
Clause 4.3	Height of Buildings	54m	156m
Clause 4.4	Floor Space Ratio	3:1	9.5:1
Clause 4.4B	Exceptions to Floor Space Ratio ("Area 2" Strathfield Town Centre)	FSR may exceed 3:1 if the size of the lot exceeds 1,500m ² , but: (c) Must not exceed 5:1, or (d) If the building meets the design excellence criteria specified in clause 6.7, must not exceed 7.5:1.	Remove the application of Clause 4.4B
6.7	Design excellence for Strathfield Town Centre		The planning proposal seeks to amend Clause 6.7 to include "Area 4". However, no amendments are required to clause 6.7 as the site is located in "Area" 2 to which this clause already applies.
6.8	Additional provisions for development in Strathfield Town Centre		Amend Clause 6.8 to increase the cap on residential

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		accommodation permitted on the site up to 70%.
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4.2 OBJECTIVES

Key components of the concept development scheme:

- *“A vibrant and active retail plaza at the ground and lower floors with provision for supermarkets, specialty retail, restaurants and cafes.*
- *A publicly accessible through site link and plaza, providing much needed open space for the Town Centre, activating the ground plane and facilitating direct pedestrian connectivity between Strathfield Station and wider precinct.*
- *A commercial office campus, with versatile floorplates to support a broad range of market requirements, interconnected by landscape terraces and communal meetings spaces.*
- *Five (5) residential towers ranging in height from 13 to 38 storeys providing approximately 753 apartments of varying sizes, typologies, and layouts including one, two, three and four -bedroom units with rooftop communal open spaces.*
- *Provision for 10% of the uplift in GFA to be dedicated as ‘key worker’ subsidised rental housing for a period of 10 years.*
- *Dedication of a 700m² community centre.*
- *A new Transport Hub incorporating a bus interchange, taxi/ride-share drop-off and pick-up, and bicycle parking with direct connections to the existing Strathfield Station, facilitating and encouraging use of sustainable transport options.”*

4.3 NUMERICAL BREAKDOWN OF INDICATIVE DEVELOPMENT CONCEPT

Site Area: 11,253m ²		
Component	GFA (m ²)	Percentage
Retail	19,185m ²	17.9%
Commercial	16,666	15.6%
Residential	70,456m ² (753 units)	65.8%
Community	700m ²	0.65%
Total	107,007m²	100%
Tower	Storeys	
1	36 (143m)	
2	38 (156m)	
3	38 (156m)	
4	22 (95.6m)	
5	14 (57.2m – 67.46m)	
Car parking (7 Basement Levels)		
Component	Spaces	Percentage
Retail/commercial	964	54%
Residential	825 residential / residential visitor spaces	46%
Public parking spaces	Nil	0%
Total	1,789 spaces	100%
Bicycle parking	TBC	TBC
Motorbike spaces	TBC	TBC

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Apartment mix		
Studio	38	5%
1-bed	166	22%
2-bed	406	54%
3-bed	131	17%
4-bed	12	32%

4.4 PUBLIC BENEFIT

No Draft Voluntary Planning Agreement has been submitted with the planning proposal.

The proponent is prepared to enter into discussions with Council, subject to Gateway approval, to contribute to a range of direct and indirect Public Benefits in association with this Planning Proposal, which may include:

“Direct

- *Delivery of a new vibrant Public Plaza dissecting the site north-south and east-west to be accessible 24 hours a day, 7 days a week.*
- *Delivery of a new integrated public transport hub.*
- *Delivery of a 700m² multi-purpose community facility.*
- *Delivery of affordable housing for key workers – equivalent to 10% of the gross floor area uplift;*

Indirect

- *Embellishment of Churchill Avenue to create an extended public realm and share-way, connecting Strathfield Square with Strathfield Plaza.*
- *Creation of a subterranean pedestrian link, creating a safe, secure and direct connection between Strathfield Station and the Strathfield Central transport hub.*
- *Creation of a new footbridge crossing over Redmyre Road, facilitating improved public access to the Strathfield Central transport hub and Strathfield Station.”*

5. ASSESSMENT OF THE PLANNING PROPOSAL

5.1 SECTION A – NEED FOR THE PLANNING PROPOSAL

Q1. Is the planning proposal a result of an endorsed local strategic planning statement, strategic study or report?

The planning proposal aims to give effect to an action in Council's 2030 Community Strategic Plan (Adopted 6 June 2018) to upgrade Strathfield Town Centre, as follows:

Theme	Civic Pride and Place Management
Goal	Engaging and activated public places
Strategy	Revitalize Town and Village centres, develop and implement place promotion strategies and support productive and well managed local businesses.
Action <i>(Operational Plan 2018-2019)</i>	Develop plans for upgrading Strathfield Town Centre and integrating transport services.

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The planning proposal seeks to give effect to relevant objectives and directions contained in *Greater Sydney Region Plan: A Metropolis of Three Cities (GSC, March 2018)* and *Eastern City District Plan (GSC, March 2018)*, specifically to deliver additional housing supply, choice and affordability, with access to jobs, services and public transport.

Q2. *Is the planning proposal the best means of achieving the objectives or intended outcomes, or is there a better way?*

Due to the significance of Strathfield Town Centre as the LGAs major commercial centre and public transport gateway to access jobs, schools, and services, and inclusion of land contained in Canada Bay and Burwood LGA's, a planned and collaborative approach is needed to upgrade Strathfield Town Centre and integrate transport services. This approach is consistent with Planning Priority E2 under the *Eastern City District Plan*.

A masterplan to establish an appropriate mix of land use and fine grain design for the Strathfield Town Centre as a whole is required to ensure its redevelopment occurs in a planned and coordinated way. The masterplan should be informed by detailed economic, social and environmental studies which consider the cumulative impacts of redevelopment of the centre as a whole, having regard to housing and job supply targets set by state-led strategic planning documents, and alignment with Council's 2030 *Community Strategic Plan* and Council's strategic planning statement. A collaborative approach to integrating and improving existing transport services is also required.

Following this, the SLEP 2012 would need to be updated to implement the Strathfield Town Centre Masterplan including amendments to land use zoning, maximum floor space ratio (FSR), building height, and site-specific provisions. The existing site-specific DCP for the Strathfield Town Centre would also need to be updated.

The preparation of a masterplan for the Strathfield Town Centre and integration of transport services has been brought to the forefront as a short-term action under Council's Community Strategic Plan and Draft Strategic Planning Statement. In this regard, **the planning proposal is not considered to be the best, most efficient and the most effective approach to delivering the desired outcome.**

5.2 SECTION B – RELATIONSHIP TO STRATEGIC PLANNING FRAMEWORK

Q3. *Will the planning proposal give effect to the objectives and actions of the applicable regional, or district plan or strategy (including any exhibited draft plans or strategies)?*

Assessment Criteria

(a) *Does the proposal have strategic merit?*

The planning proposal is generally aligned with the overarching vision of the Greater Sydney Region Plan, *A Metropolis of Three Cities* (the Plan), to create a 30-minute city that connects people from where they live to jobs, business, schools, health facilities, and services. This is primarily due to the site's strategic location within the Strathfield Town Centre around an existing transport node. As such, the suitability of site to deliver additional housing supply (Objectives 10 and 11 under *A Metropolis of Three Cities*, and Planning Priority E5 under the *Eastern City District Plan*) is acceptable under the planning proposal.

Concerns are raised, however, regarding the suitability of the land use mix and proposed building envelope under the indicative concept scheme. As per previous comments, a planned and coordinated approach is needed to establish a Masterplan for the Centre as a whole to ensure that growth is aligned with infrastructure (Planning Priority's E1 and E2, *Eastern City District Plan*;

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Objectives 3, 4 & 5, *A Metropolis of Three Cities*) and opportunities for place-making are maximised (Planning Priority's E3, E4, E5 and E6, *Eastern City District Plan*; Objectives 6, 7, 12 & 13, *A Metropolis of Three Cities*). To do otherwise, would deliver a sub-optimal planning outcome.

As such, the proposal is not considered to demonstrate strategic planning merit.

(b) Does the proposal have site-specific merit, having regard to the following?

- ***the natural environment (including known significant environmental values, resources or hazards); and***
- ***the existing uses, approved uses, and likely uses of land in the vicinity of the proposal; and***
- ***the services and infrastructure that are or will be available to meet the demands arising from the proposal and any proposed financial arrangements for infrastructure provision***

There are no known significant environmental values, resources or hazards that would preclude the provision of additional density on the site. Noise impacts from traffic to residential accommodation is noted as a potential development constraint, albeit, mitigation measures can be incorporated at the design phase to address this issue.

No change is proposed to the B3 Commercial Core zoning of the site, which applies to all land within the Strathfield Town Centre. The indicative concept development scheme is considered to be inconsistent with the objectives of the B3 Zone, in the following manner:

- the concept scheme does not provide an appropriate mix of land uses to meet the needs of the local and wider community;
- the proposal does not maximise employment opportunities in an accessible location; and
- the proposed provision of off-street parking (1,789 spaces) does not maximise public transport patronage and encourage walking and cycling.

The planning proposal seeks to amend clause 6.8 of the SLEP 2012 to increase the cap on residential accommodation from 35% to 70%. Under the concept scheme, the planning proposal would facilitate delivery of a mixed-use development comprised of five towers made up of 65.8% residential, 33.5% commercial (17.9% retail, 15.6% office space), and 0.65% dedicated as community space. The mix of uses will be distributed across the towers and stacked to provide retail activation at the ground plane, commercial offices above, and residential located at the upper levels (**Figure 36**).

Land surrounding the Centre is zoned B4 Mixed Use. Land zoned B4 on the Centre periphery may be more suitable to an uplift in residential density in order to retain the commercial focus and provision of retail and services within the Town Centre.

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Figure 36. Indicative development concept under Planning Proposal (Source: Grimshaw).

The form and composition of the concept development scheme relative to existing buildings within the Strathfield Town Centre is depicted in the photomontage at **Figure 37**. Existing gateway buildings of approximately 16-storeys, generally in accordance with the 54m LEP height control, would lose their prominence as the Gateway to the Centre.



Figure 37. Photomontage of the indicative development concept as viewed from the north.

The form and massing of the concept scheme is excessive and incongruous with the built form of the surrounding area, particularly in relation to existing two-storey traditional commercial buildings to the south of the site in The Boulevard Retail Conservation Area (**Figure 38**).

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Figure 38. Massing model – View from Redmyre Road looking North East (Source: Urban Design Report, Grimshaw)

In my opinion, there are no unique attributes of the site or sufficient planning grounds that support the proposed uplift in height and FSR ahead of the preparation of a Masterplan for the Strathfield Town Centre. The current scheme is not considered to promote a sustainable capacity for the Centre, particularly in terms of the demand on existing services and infrastructure provision. In this regard, the planning proposal lacks site-specific merit.

As noted in Council's Strategic Plan, priority concerns identified by the community following extensive public consultation includes existing traffic issues in the Strathfield Town Centre arising from the one-way flow of traffic, poor integration of existing public transport, lack of diversity in retail and services, and need to revitalise the Centre. This has led to Council adopting the short-term action in its *2030 Community Strategic Plan* to prepare a Masterplan for Strathfield Town Centre in collaboration with Burwood and Canada Bay Councils. This is also identified as a priority action under Council's *Draft Strategic Planning Statement*. As such, it is recommended that the planning proposal does not proceed to a Gateway determination.

Q4. Will the planning proposal give effect to a Council's endorsed local strategic planning statement, or another endorsed local strategy or strategic plan?

Council's Draft Strategic Planning Statement has not been formally endorsed at this stage.

The planning proposal aims to give effect to the strategy to revitalise the Strathfield Town Centre. As per previous comments, the sites strategic location around an existing centre and transport node makes it a suitable location to deliver additional housing supply and jobs. Albeit, the timing and approach is not supported ahead of a detailed Masterplan for the site.

Q5. Is the planning proposal consistent with applicable State Environmental Planning Policies?

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SEPP 65 – Design Quality of Residential Apartment Development & Apartment Design Guide (ADG)

The submitted Urban Design Report provides an indicative ADG compliance assessment for the concept development scheme. This indicates that the proposal is generally able to satisfy the requirements of SEPP 65/ADG with regard to privacy, solar access, communal open space and cross ventilation.

Due to additional development constraints for residential development such as building separation requirements under the ADG, use of land within the Centre cannot be optimised without a Masterplan for the Strathfield Town Centre.

SEPP (Infrastructure) 2007

The Planning Proposal is capable of achieving consistency with SEPP (Infrastructure) 2007. The concept scheme is deemed traffic generation development under the iSEPP and is required to be referred to Roads and Maritime Services (Clause 104). The consent authority would also need to be satisfied of the relevant matters under Clause 101 as the site has frontage to a classified road.

SEPP (Affordable Rental Housing) 2009

The proposal includes provision of affordable rental housing for a period of 10 years. The provision of affordable rental housing is consistent with strategic planning documents *A Metropolis of Three Cities* and *Eastern City District Plan*.

Q6. Is the planning proposal consistent with applicable Ministerial Directions (s9.1 directions)?

Direction 1.1 – Business and Industrial Zones

The planning proposal is not considered to give effect to the objectives of this direction to:

- (a) encourage employment growth in suitable locations (the planning proposal does not seek to maximise opportunities for employment growth);*
- (b) protect employment land in business and industrial zones; and*
- (c) support the viability of identified centre.*

Direction 2.3 – Heritage Conservation

The concept development scheme under the planning proposal is not considered to give effect to the objective of this direction to conserve items, areas and places of environmental heritage significance, specifically, in relation to The Boulevarde Retail Conservation Area, listed items at 35-39 The Boulevarde of local significance, and state significant Strathfield Railway Station.

The concept development scheme does not provide an appropriate visual and physical separation and transition in bulk and scale to the adjoining heritage conservation area and listed items to the south of the site. The form and massing of the concept scheme would dominate the landscape and detract from the listed items in the vicinity.

Direction 3.1 – Residential Zones

Direction 3.1 does not apply as the planning proposal has not been prepared by a planning authority.

Direction 3.4 – Integrating Land Use and Transport

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The planning objectives under Direction 3.4 are to:

- (a) improve access to housing, jobs and services by walking, cycling and public transport;*
- (b) increase the choice of available transport and reduce dependence on cars;*
- (c) reduce travel demand including the number of trips generated by development and the distances travelled, especially by car;*
- (d) support the efficient and viable operation of public transport services; and*
- (e) provide for the efficient movement of freight.*

The concept development scheme makes provision for 1,789 car parking spaces (Levels B3-B7) and includes some motorcycle parking provision. A new Transport Hub incorporating a bus interchange, taxi /ride-share drop-off and pick-up, and bicycle parking with direct connections to the existing Strathfield Station is also proposed.

Based on the land use mix under the concept development scheme, the car parking requirement is estimated to be 2,310 spaces comprised of 839 spaces for the residential component (825 proposed), and 1471 spaces for the commercial/retail component (964 proposed). No public parking or parking for the dedicated community space is proposed.

The submitted Traffic and Parking report prepared by Cardno (dated 24/09/2019) indicates that the planning proposal car parking provision exceeds the expected peak parking demand. The report also states that the location of the site within 100m of rail and bus networks will reduce the sites reliance on private motor vehicles while allowing future residents easy and efficient access into the rest of Sydney.

The car parking provision does not reflect the strategic location of the site serviced by high frequency public transport that can move large numbers of people. As such, the proposal undermines the intent of policies to reduce car dependence and connect Sydney, as per Direction 3.4.

A Masterplan is needed as part of a collaborative approach to investigating sustainable transport options and investment to upgrade existing transport services to meet future demand.

Direction 7.1 – Implementation of A Plan for Growing Sydney

Direction 7.1 applies to land in Strathfield LGA. The Plan's primary objective is to investigate the potential for growth and future infrastructure needs of land that will shape the growth of Sydney. In Strathfield LGA application of the Plan is generally limited to growth in former industrial areas, specifically between Concord West, Strathfield and Flemington Railway stations.

In established precincts close to existing transport nodes, the focus of transformative urban renewal is to revitalise existing centres. The proposal is generally consistent with this.

5.3 SECTION C – ENVIRONMENTAL, SOCIAL AND ECONOMIC IMPACT

Q7. Is there any likelihood that critical habitat or threatened species, populations or ecological communities, or their habitats, will be adversely affected as a result of the proposal?

No. The subject site is not affected by critical habitat or threatened species, populations or ecological communities, or their habitats.

Q8. Are there any other likely environmental effects as a result of the planning proposal and how they are proposed to be managed?

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The planning proposal would not give rise to any environmental effects including flooding, land slip, bushfire hazard and the like. The infrastructure report prepared by Cardno identifies that the subject site and surrounding land and roads are affected by the 1% AEP flood, however, this is not a constraint to future redevelopment.

An acoustic report has not been submitted with the concept development scheme. The impact of noise from Strathfield railway station and surrounding roads on residential accommodation can be adequately resolved through appropriate floor layouts away from high noise sources and inclusion of noise mitigation measures.

Q9. Has the planning proposal adequately addressed any social and economic effects?

A Social and Economic Assessment Report accompanies the planning proposal.

The lack of a Masterplan for the Centre undermines the validity of the Report in terms of assessing the cumulative economic and social impacts arising from the planning proposal.

Q10. Is there adequate public infrastructure for the planning proposal?

The increase in density on the site and land use composition will generate additional demand on infrastructure including public transport, roads, utilities, waste management and recycling services, essential services such as health, education and emergency services.

In terms of the localised traffic network, concerns are raised over congestion and parking. Traffic is restricted to one-way flow within the Centre from Albert Road along Churchill Avenue to Raw Square. It is not considered sufficient for the planning proposal to identify where there may be an expected shortfall in service provision and what studies may be required to identify the extent of that shortfall and what agencies to consult as part of that process, pre-Gateway Determination.

A needs analysis to identify gaps in service provision needs to inform preparation of a Masterplan for the Strathfield Town Centre.

Q11. What are the views of state and Commonwealth public authorities consulted in accordance with the Gateway determination?

Not applicable at this time.

6. CONCLUSION

The location of Strathfield Town Centre within walking distance of Strathfield train station and bus interchange and close proximity to schools, retail, facilities, and services supports its location for increased density in alignment with the *Eastern City District Plan* (March 2018) and *Greater Sydney Region Plan: A metropolis of Three Cities*.

However, a coordinated approach is required to ensure redevelopment of the LGAs major commercial centre occurs in a planned and coordinated way. A Masterplan which establishes the vision and fine-grain scale for the Centre is required along with collaboration between Strathfield, Burwood, and Canada Bay Councils. The masterplan should be informed by detailed economic, social and environmental studies which consider the cumulative impacts of redevelopment of the centre as a whole having regard to housing and job supply targets and planning priorities and objectives under *A Metropolis of Three Cities* and the *Eastern City District Plan*.

There are no mitigating circumstances or unique site attributes that support the planning proposal at this time, ahead of immediate actions endorsed in Council's *2030 Community Strategic Plan* and identified in Council's *Draft Strategic Planning Statement*, to revitalise the Strathfield Town Centre

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including the preparation of a Masterplan, and integrate transport services. To do otherwise, would result in a sub-optimal planning outcome.

As such, the planning proposal is not supported to proceed to a Gateway Determination.

RECOMMENDATION

- A. The planning proposal should not proceed to a Gateway determination because the proposal lacks both strategic and site-specific merit.

ATTACHMENTS

1. [↓](#) Planning Proposal
2. [↓](#) Draft Concept Architectural Plans



STRATHFIELD CENTRAL PLANNING PROPOSAL

11-23 THE BOULEVARDE,
STRATHFIELD

PREPARED FOR
MEMOCORP AUSTRALIA
SEPTEMBER 2019

URBIS

URBIS STAFF RESPONSIBLE FOR THIS REPORT WERE:

Group Director	John Wynne
Associate Director	Samantha Wilson
Consultant	Grace Macdonald
Project Code	P11524
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1. INTRODUCTION

This Planning Proposal has been prepared by *Urbis Pty Ltd* on behalf of *Memocorp Australia* (the Proponent) and seeks to initiate the preparation of a Local Environmental Plan amendment for the land known as **Strathfield Central, 11-23 The Boulevard, Strathfield** (the site).

The Planning Proposal seeks to amend the height of building and floor space ratio development standards applicable to the site, under the *Strathfield Local Environmental Plan 2012* (SLEP 2012), in accordance with Section 3.33 of the *Environmental Planning and Assessment Act 1979* (EP&A Act). It also proposes to increase the cap on residential accommodation permitted on the site.

The intended outcome of this Planning Proposal is to amend SLEP 2012 as follows:

- Amend the applicable maximum height of buildings development standard, under *Clause 4.3: Height of buildings*, to permit buildings with a height of up to **156 metres**.
- Amend the applicable maximum floor space ratio development standard, under *Clause 4.4 Floor Space Ratio*, to permit a floor space ratio of **9.5:1** and identify Strathfield Central as "Area 4".
- Remove the application of *Clause 4.4B Exceptions to floor space ratio (Strathfield Town Centre)*.
- Amend *Clause 6.7 Design excellence for Strathfield Town Centre* to include "Area 4" on the Floor Space Ratio Map.
- Amend *Clause 6.8: Additional provisions for development in Strathfield Town Centre* on "Area 4" to increase the cap on residential accommodation permitted on the site to 70%.

These amendments facilitate redevelopment of Strathfield Central for a landmark mixed-use development, as illustrated in the Urban Design Report prepared by *Grimshaw Architects* (refer **Appendix A**), which will incorporate the following:

- A vibrant and active retail plaza at the ground and lower floors with provision for supermarkets, speciality retail, restaurants and cafes.
- A publicly accessible through site link and plaza, providing much needed open space for the Town Centre, activating the ground plane and facilitating direct pedestrian connectivity between Strathfield Station and the wider precinct.
- A commercial office campus, with versatile floorplates to support a broad range of market requirements, interconnected by landscaped terraces and communal meetings spaces.
- Five residential towers ranging in height from 13 to 38-storeys providing approximately 753 apartments of varying sizes, typologies, and layouts including one, two, three and four bedroom units with rooftop communal open spaces.
- Provision for 10% of the uplift in gross floor area to be dedicated as 'key worker' subsidised rental housing for a period of 10 years.
- Dedication of a 700m² community centre.
- A new Transport Hub incorporating a bus interchange, taxi / ride-share drop-off and pick-up, and bicycle parking with direct connections to the existing Strathfield Station, facilitating and encouraging use of sustainable transport options.

Figure 1 – Photomontage of Strathfield Central



Source: Grimshaw

The proposed redevelopment, in association with adjacent development prospects, provides the unique opportunity to create a vibrant, community focused, Town Centre. The public benefits of such a development include:

- Providing a catalyst for urban renewal of the Strathfield Town Centre;
- Revitalising the commercial core to provide goods and services to residents and visitors;
- Provision of significantly improved local employment opportunities;
- Contribution to the Greater Sydney Commission's vision for a 30-minute city;
- Providing a multi-modal transportation hub with integration into the town centre;
- Providing opportunities for improvement to the wider public domain including the creation of a new open space and shared zone;
- Delivering residential housing in response to the identified need, outlined by State and local planning strategies, situated close to facilities and services;
- Provision of a range of dwelling sizes and configurations in close proximity to transport, schools, open space, retail and support services; and
- Utilisation of existing infrastructure.

This Planning Proposal has been prepared in accordance with Section 3.33 of the EP&A Act with consideration of the NSW Department of Planning, Industry and Environment (DPIE) 'A guide to preparing planning proposals' and 'A guide to preparing local environmental plans', August 2016.

This Planning Proposal is structured as follows:

- **Chapter 2: Site Analysis** – provides a description of the site and context.
- **Chapter 3: Strategic Planning Context** – provides a summary of the relevant strategic planning policies and directions.
- **Chapter 4: Statutory Planning Context** – provides a summary of the relevant statutory planning legislation controls.
- **Chapter 5: Development Concept** – provides a description of the proposed concept design.
- **Chapter 6: The Case for Change** – summarises the compelling reasons why Strathfield Council should resolve to support the Planning Proposal and initiate the required amendments to the planning legislation.
- **Chapter 7: Planning Proposal**, including:
 - Part 1 – A statement of the objectives and intended outcomes of the proposed instrument.
 - Part 2 – An explanation of the provisions that are to be included in the proposed instrument.
 - Part 3 – The justification for those objectives, outcomes and the process for their implementation.
 - Part 4 – Maps, where relevant, to identify the intent of the planning proposal and the area to which it applies.
 - Part 5 – Details of the community consultation that is to be undertaken on the planning proposal.
 - Part 6 – A project timeline to detail the anticipated timeframe for the plan making process.

The Planning Proposal is supported by the following technical documentation:

- Urban Design Report prepared by *Grimshaw Architects* (**Appendix A**);
- Social and Economic Impact Assessment prepared by *HillPDA* (**Appendix B**);
- Traffic and Transport Assessment by *Cardno* (**Appendix C**);
- Infrastructure Services Assessment by *Cardno* (**Appendix D**);
- Heritage Impact Study by *Urbis* (**Appendix E**); and
- Wind Assessment prepared by *Windtech* (**Appendix F**).

2. SITE ANALYSIS

2.1. THE SITE AND EXISTING DEVELOPMENT

The site is known as **Strathfield Central, 11-23 The Boulevard, Strathfield** and has a total area of 11,253m². It is legally described as Lot 21 in DP623899 and Lot 102 in DP597302. The site is currently comprises a commercial shopping centre and eight-storey commercial office tower.

The site is situated approximately 10 kilometres west of the Sydney CBD on the southern side of the Strathfield railway station. It is bound by a bus interchange to the north, The Boulevard and shop top housing to the east, Redmyre Road to the south, and a 16-storey residential flat building including 3 levels of podium car parking to the west (refer Figure 2). The site is within walking distance to the Strathfield railway station and bus interchange.

Figure 2 – Site Context Plan



Source: Urbis

2.2. LOCALITY DESCRIPTION

The site is centrally located within the Strathfield Town Centre. The site adjoins traditional two-storey shop top housing to the east. Whereas the western side transitions to high density residential flat buildings. Directly to the north-east of the site, Strathfield Square provides a pedestrian-friendly plaza with landscaping, a feature fountain and seating connecting the shopping centre with the bus-train interchange.

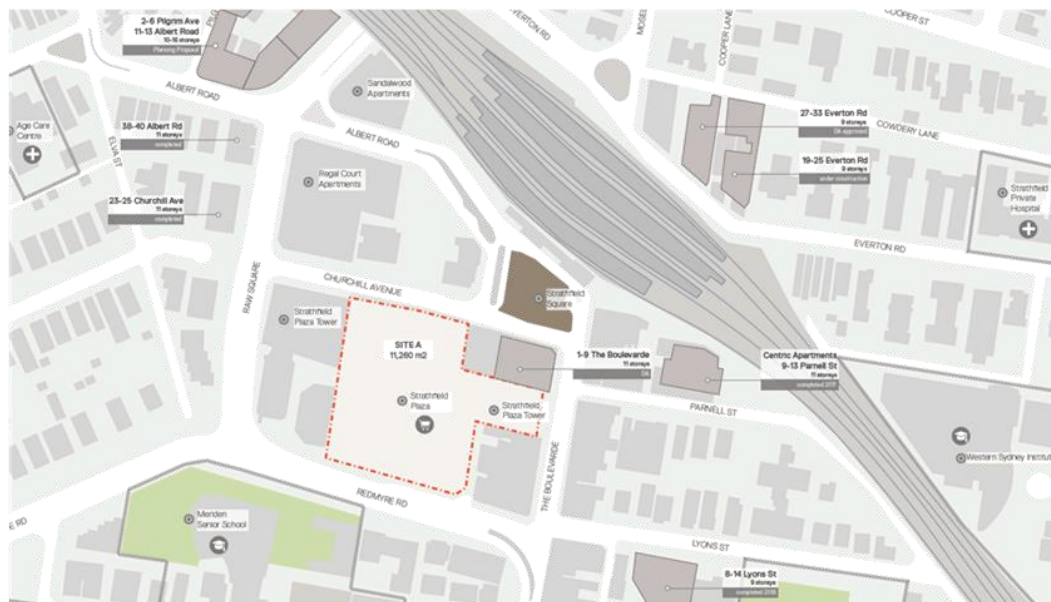
The surrounding locality is characterised by a range of residential, and small-scale retail and business uses. The locality can be described as an area undergoing transition from traditional local centre dominated by shop top housing to an area supporting a higher order centre serving local residents and regional visitors complemented with higher density mixed-use development surrounding the commercial core.

Recent development applications and planning proposals demonstrate the transition occurring within Strathfield Town Centre. They include:

- 2-6 Pilgrim Avenue and 11-13 Albert Road, 10-16 storeys, Planning Proposal
- 38-40 Albert Road, 11 storeys, completed
- 23-25 Churchill Avenue, 11 storeys, completed
- 27-33 Everton Road, 9 storeys, DA approved
- 19-25 Everton Road, 9 storeys, under construction
- Centric Apartments, 9-13 Parnell Street, 11 storeys, completed 2017
- 1-9 The Boulevard, 11 storeys, DA approved
- 8-14 Lyons Street, 9 storeys, completed 2018
- 23-31 Morwick Street, 12 storeys, completed 2016

The site is located in close proximity to a range of educational, community, health and recreation facilities, and parklands (refer Figure 3).

Figure 3 – Local Context Plan



Source: Grimshaw

2.3. SURROUNDING ROAD NETWORK

Strathfield Central is connected by a network of State and local roads. The key road network surrounding the subject site consists of:

- Raw Square: State road with a two-way carriageway with generally two lanes in each direction.
- Churchill Avenue: Local road that runs in an east-west direction between Strathfield Square and Homebush Road. It accommodates two lanes of traffic.
- Redmyre Road: State road configured as two lanes in each direction with additional auxiliary lands provided at the intersections.
- The Boulevarde: State road with two lanes in both directions for traffic movement.
- Strathfield Square: Local road with a divided carriageway with two lanes separated by a bus terminal and both lanes restricted to one-way flow only.
- Albert Road: Regional road with two-way carriageway.

The site can readily access the major east-west routes of the M4 Western Motorway and Parramatta Road (Great Western Highway) via Raw Square to Leicester Avenue.

The M4 Western Motorway is undergoing a significant upgrade, which will ultimately connect the existing motorway to the Sydney Airport and the M5 Motorway. The M4 East Tunnels from Strathfield to Ashfield opened in July 2019.

2.4. PUBLIC TRANSPORT

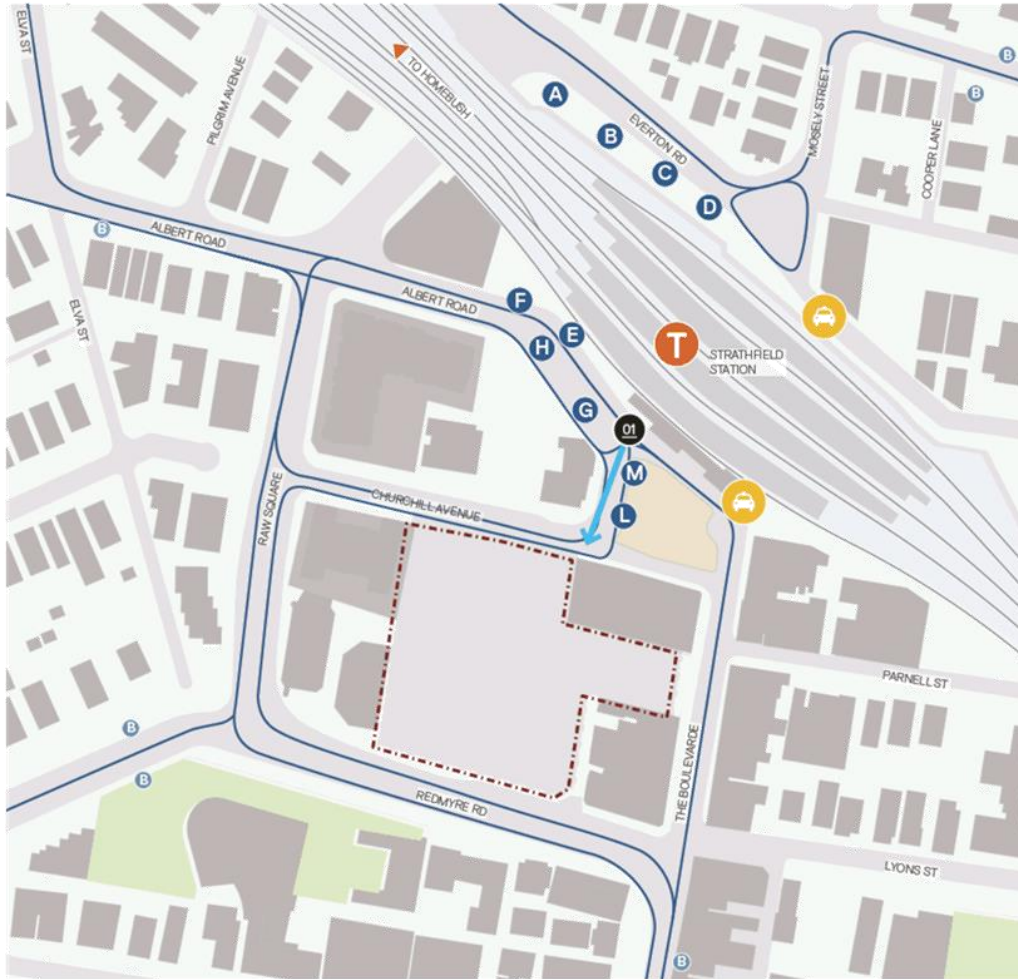
The site is well located to the public transport networks. It is within 100m walking distance of the Strathfield Railway Station, which is located on the Main Northern and Main Western railways lines. Strathfield Railway Station forms a major junction for regional and suburban rail services with connections to Sydney CBD, Parramatta CBD, Liverpool, Penrith, Epping, Blue Mountains and Newcastle. Services on this line generally operate with headways of 5 to 10 minutes in each direction during peak periods and 10 to 15 minutes in each direction outside peak hours. Location of the station is shown on Figure 4.

Local and regional bus services through the areas are provided by Sydney Buses. There are bus stops on both sides of the Strathfield Railway Station, with the main bus interchange on the southern side. Bus services in the areas include the following routes:

- Route 407 – Strathfield to Burwood
- Route 408 – Rookwood Cemetery to Burwood via Flemington
- Route 415 – Campsie to Chiswick
- Route 450 – Strathfield to Hurstville
- Route 458 – Ryde to Burwood
- Route 480 – Strathfield to Central Pitt Street via Homebush Road
- Route 483 – Strathfield to Central Pitt Street via South Strathfield
- Route 525 – Burwood to Parramatta via Sydney Olympic Park
- Route 526 – Rhodes Shopping Centre to Burwood
- Route 913 – Strathfield to Bankstown
- Route 914 – Strathfield to Greenacre
- Route M90 – Liverpool to Burwood

Strathfield Railway Station provides a major intermodal transport interchange for local and regional bus services operating in the area.

Figure 4 – Public Transport Network



Source: Grimshaw

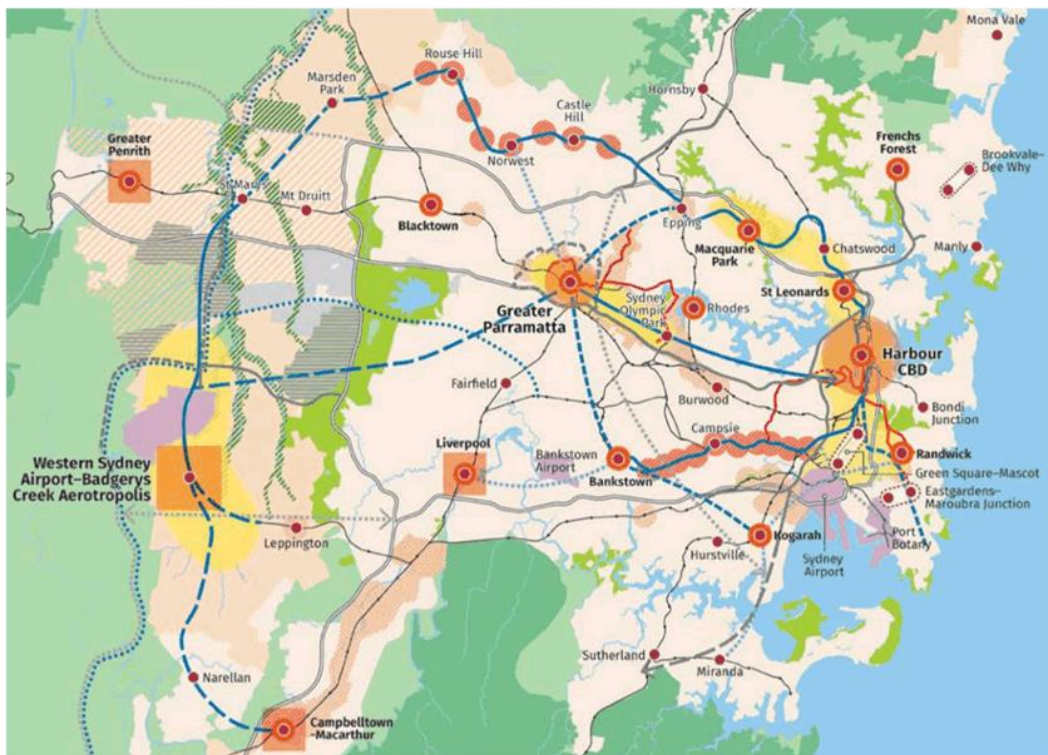
3. STRATEGIC PLANNING CONTEXT

3.1. A METROPOLIS OF THREE CITIES – A GREATER SYDNEY REGION PLAN

A Metropolis of Three Cities – A Greater Sydney Region Plan (the Region Plan), finalised by the Greater Sydney Commission in March 2018, provides a 40-year vision (to 2056) and establishes a 20-year plan to manage growth and change for the Greater Sydney region. The Region Plan is built on a vision of a 30-minute city, 'where most residents live within 30 minutes of their jobs, education and health facilities, services and great places'.

Under the Region Plan, Strathfield Central is located in the Eastern City district and is identified for urban renewal. The area is identified as part of the Greater Parramatta Urban Renewal corridor, which runs along the Main Western rail line from Burwood to Westmead. This area is earmarked for revitalisation in order to provide for a greater range of employment and housing within close proximity to existing and proposed infrastructure.

Figure 5 – A Metropolis of Three Cities – Structure Plan



Source: Greater Sydney Commission

The Region Plan sets 10 directions to achieve 'a metropolis of three cities'. Under each direction, a series of objectives and actions are identified. The urban renewal of Strathfield Town Centre contributes to the 30-minute city vision and specifically addresses the following directions and objectives listed under the Region Plan.

A City supported by infrastructure sets a direction ensuring growth is supported by essential infrastructure. The Greater Parramatta Urban Renewal Corridor benefits from existing infrastructure, such as the M4 Motorway, Main Western and North Rail Lines. In addition, this corridor is earmarked to receive significant investment on new infrastructure, such as the Westconnex, Sydney Metro West line and Parramatta Light Rail.

A city for people, housing the city, and a city of great places directions aim to give people better access to housing, transport and employment as well as social, recreational, cultural and creative opportunities. The Region Plan identifies urban renewal areas as opportunities for creating capacity for new housing. Urban renewal areas identified in the Region Plan are located close to existing or proposed infrastructure investment, which enables residents to access jobs within walking distance. The Greater Sydney Commission acknowledges the opportunity to unlock this capacity to ensure each urban renewal area is utilised to its highest and best use.

A well-connected city and jobs and skills for the city outlines strategies and actions to rebalance opportunities for all residents to have greater access to jobs, shops and services. To achieve these directions, the Region Plan identifies need for integrated land use and transport to create walkable and 30-minute cities. The redevelopment of Strathfield Central creates new job opportunities and greater access to shops and services. By responding to surrounding redevelopment, it responds to the Greater Sydney Commission's vision to create a 30-minute city leveraging off local and regional transport connections offered via the Strathfield Rail Station and surrounding bus interchanges.

The Planning Proposal is consistent with the plan, as it responds to the following:

- *A city supported by infrastructure*
 - Objective 1: Infrastructure supports the three cities
 - Objective 3: Infrastructure adapts to meet future needs
 - Objective 4: Infrastructure is optimised
- *A city for people*
 - Objective 6: Services and infrastructure meet communities' changing needs
- *Housing the city*
 - Objective 10: Greater housing supply
 - Objective 11: Housing is more diverse and affordable
- *A city of great places*
 - Objective 12: Great places that bring people together
- *A well-connected city*
 - Objective 14: *A Metropolis of Three Cities* – integrated land use and transport creates walkable and 30-minute cities
 - Objective 15: The Eastern, GPOP and Western Economic Corridors are better connected and more competitive
 - Objective 17: Regional connectivity is enhanced
- *Jobs and skills for the city*
 - Objective 22: Investment and business activity in centres

3.2. EASTERN CITY DISTRICT PLAN

The *Eastern City District Plan* (the District Plan) builds off the directions and objectives set by the Region Plan tailoring them to the district. The District Plan was finalised in conjunction with the Region Plan in March 2018.

The site is located within the Eastern City District. The Greater Sydney Commission (GSC) envisaged that by 2036 the District will “become more innovative and globally competitive, carving out a greater portion of knowledge-intensive jobs from the Asia Pacific Region, as well as improve the District’s lifestyle and environmental assets”.

The GSC has identified a five-year housing targets that is based on both the Eastern City District’s dwelling need and the opportunity to deliver supply. The local government area of Strathfield is to provide 3,650 dwellings by 2021, which represents just 8% of the 46,550 dwellings required across the entire Eastern City District. The GSC has also identified a minimum 20-year housing target of 157,500 dwellings to be delivered within the Eastern City District by 2036.

The District Plan identifies the need to leverage a number of existing opportunities, investments, and economic assets in order to drive economic activity and diversity, not only for the Eastern City District but for NSW and Australia. Strathfield Town Centre is identified as a local centre (refer to Figure 6). The GSC identified local centres as an important contributor to the 30-minute city, which plays a role in providing day-to-day goods and services close to where people live and provide opportunities for local employment.

Figure 6 – Eastern City District – Structure Plan



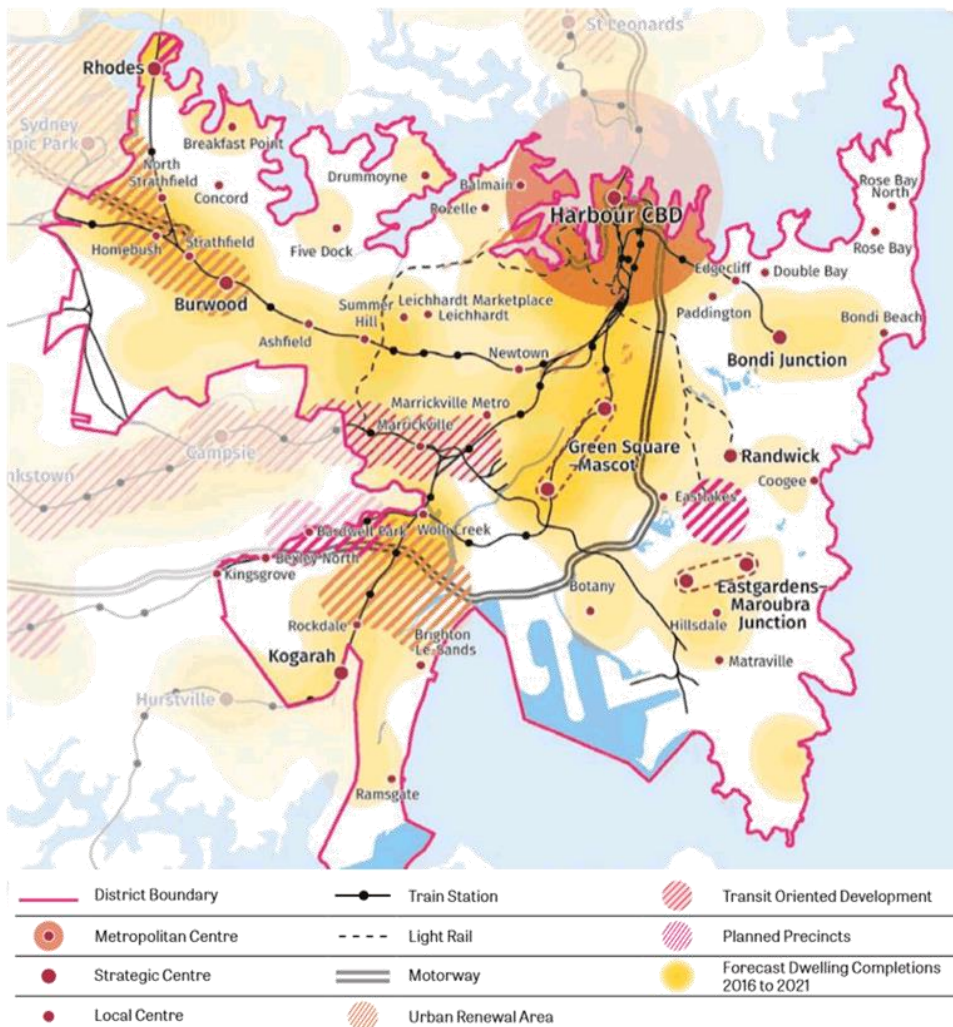
Source: Greater Sydney Commission

The Strathfield Town Centre is identified as a **Planned Precinct** in the Eastern City District Plan (refer to Figure 7). These Planned Precinct areas have been identified as having the greatest potential for mixed use growth given their strategic location close to public transport, shops and services, while retaining a community's character.

The Planning Proposal is consistent with the Eastern City District Plan, as it:

- Provides essential services and social infrastructure to meet people's changing needs (Planning Priority E3);
- Fosters healthy, creative, culturally rich and socially connected communities (Planning Priority E4);
- Provides housing supply, choice and affordability with access to jobs, services and public transport (Planning Priority E5);
- Creates and renews great places and local centres, and respects the District's heritage (Planning Priority E6); and
- Delivers integrated land use and transport planning and a 30-minute city (Planning Priority E10).

Figure 7 – Eastern City District future housing supply



3.3. STRATHFIELD 2030 COMMUNITY STRATEGIC PLAN

The *Strathfield 2030 Community Strategic Plan* is Council's long-term strategic document which sets goals and strategies through identification of the community's main priorities and aspirations for the future. It was adopted by Strathfield Council on June 2018. Strathfield 2030 identifies Strathfield Town Centre as the major commercial centre in the local government area (LGA).

The plan identified transport was rated as the highest priority through community engagement, which highlighted the public's concern with traffic congestion and local traffic movement creating difficulties moving around Strathfield Town Centre. The community also commented that Strathfield Town Centre needs revitalisation and better integrated transport. Through this community feedback, Strathfield 2030 identified the need to review the current planning controls for Strathfield Town Centre and "develop plans for upgrading Strathfield Town Centre and integrating transport services".

The Strathfield Central Planning Proposal responds to Strathfield 2030 by revitalising the shopping centre and offering better integration between the Strathfield Railway Station and the bus interchange. The planning proposal seeks to provide better goods and services to meet the needs of the local residents.

3.4. STRATHFIELD RESIDENTIAL LAND USE STUDY

The *Strathfield Residential Land Use Study*, commissioned by Strathfield Council, assisted Council to accommodate growth over the next 25 years, while at the same time having regard to the character of the existing LGA and the availability of community and public services to meet the needs of the new residents. The study was finalised in November 2011.

The study concluded the majority of the residential capacity is located in the northern portion of the LGA (i.e. around the Parramatta Road corridor and the rail line corridors) and recommended Council increase capacity for a further 1,394 dwellings. Strathfield Town Centre was identified for an additional 595 dwellings. The study made recommendations for Strathfield Central to promote redevelopment of the Strathfield Town Centre, which include the following:

- Envisioned built form: **B4 Mixed Use Residential**
- Floor space ratio: **3.1 + 3.4 bonus = 6.5:1**
- Height: **22 storeys (90m)**

While this study was prepared in 2011, it shows that the need for a review of planning controls for Strathfield Town Centre has been a priority for Council and the community for some time. It recognises the untapped potential to provide additional homes close to a robust public transport network. The planning proposal responds to this housing study by facilitating an increased supply of residential accommodation within the Town Centre. The proposed planning controls in this report build upon this study and reflect the current strategic policies and directions from the Greater Sydney Commission, while also responding to today's market conditions.

3.5. STRATHFIELD AT THE CROSSROADS OF SYDNEY: AN ECONOMIC LAND USE AND EMPLOYMENT STRATEGY

Strathfield at the Crossroad of Sydney is an economic land use and employment strategy prepared for Strathfield Council. The purpose of the study was to improve the knowledge of Strathfield's economic base and investigate the economic issues facing the LGA. The study was finalised in June 2010.

The study recommended to consolidate retail, commercial and service provision and encourage entertainment and recreation facilities. It identified Strathfield Town Centre as the primary centre within the LGA with opportunities to extend the centre beyond its current boundaries, diversifying the mix of uses and variety of retailing available and improving local transport connections with surrounding suburbs and employment lands. It recommended Strathfield Town Centre to focus on civic entertainment and cultural activities and promote a night time economy. Limited office supply was identified in this study for Strathfield Town Centre with the recommendation to investigation office space adjacent to Homebush Station.

The planning proposal presents an opportunity to address constraints identified in this study, such as the limited office supply. In addition, the redevelopment of the Strathfield Central will foster a night time economy and allow flexible hours for residents to access the amenities.

3.6. GREATER PARRAMATTA INTERIM LAND USE AND INFRASTRUCTURE IMPLEMENTATION PLAN

The *Greater Parramatta Interim Land Use and Infrastructure Implementation Plan* (interim LUIIP) identifies how more jobs, homes and essential services will be accommodated in the priority growth area over the next 20 years. It includes a land use framework to guide future redevelopment of the priority growth area, identifies key actions for the short term and allows government agencies to identify and plan for the infrastructure required to unlock its potential. The interim LUIIP was prepared by the Department of Planning, Industry and Environment in July 2017.

The interim LUIIP identifies key actions to achieve the Greater Parramatta vision, including commence planning investigation for Strathfield Planned Precinct, which includes Strathfield Town Centre. Strathfield Planned Precinct was identified by the NSW Government for a review of planning controls on 1 June 2017. The exhibition and finalisation of the Strathfield Planned Precinct review is unknown at this time. However, the planning proposal aligns with the interim LUIIP's vision by contributing to the creation of new jobs and housing.

4. STATUTORY PLANNING CONTEXT

4.1. STRATHFIELD LOCAL ENVIRONMENTAL PLAN 2012

The *Strathfield Local Environmental Plan 2012* (SLEP 2012) is the principal environmental planning instrument applicable to the site. SLEP 2012 was gazetted on 15 March 2013 and commenced on 29 March 2013.

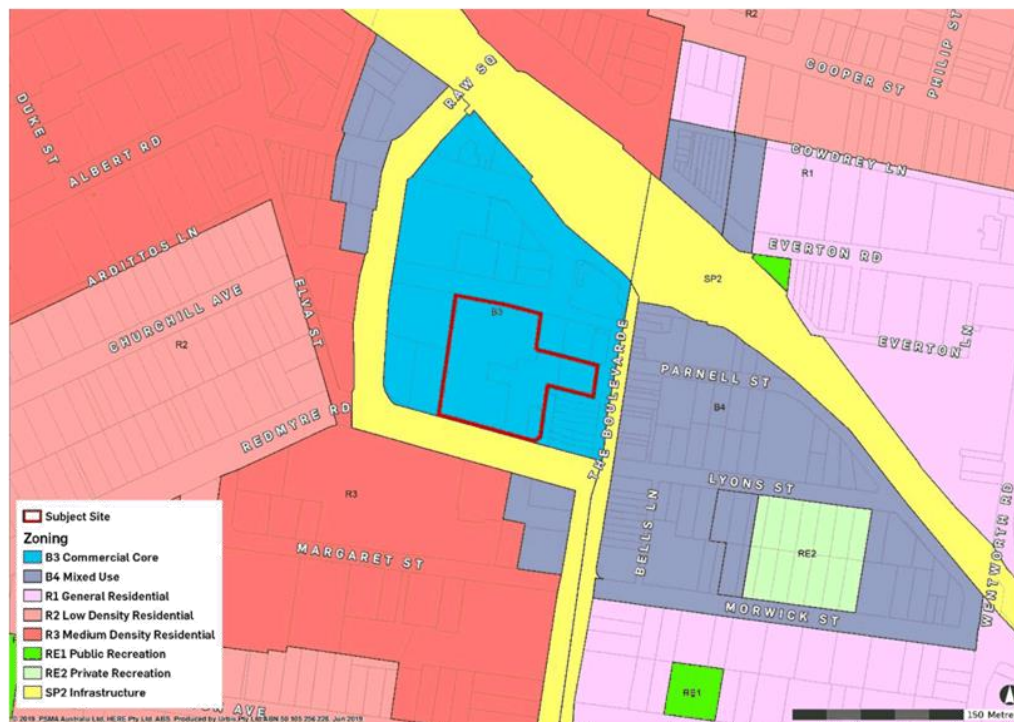
4.1.1. Land Use Zoning

The site is zoned **B3 Commercial Core** under the SLEP 2012 (refer to Figure 8). The objectives of the B3 Commercial Core zone are as follows:

- To provide a wide range of retail, business, office, entertainment, community and other suitable land uses that serve the needs to the local and wider community.
- To encourage appropriate employment opportunities in accessible locations.
- To maximise public transport patronage and encourage walking and cycling.

The following uses are permissible with consent within the B3 Commercial Core zone: *Centre-based child care facilities; Commercial premises; Community facilities; Educational establishments; Entertainment facilities; Function centres; Hostels; Hotel or motel accommodation; Information and education facilities; Medical centres; Oyster aquaculture; Passenger transport facilities; Recreation facilities (indoor); Registered clubs; Residential flat buildings; Respite day care centres; Restricted premises; Roads; Seniors housing; Shop top housing; Tank-based aquaculture.*

Figure 8 – SLEP 2012 – Land Use Zoning Map

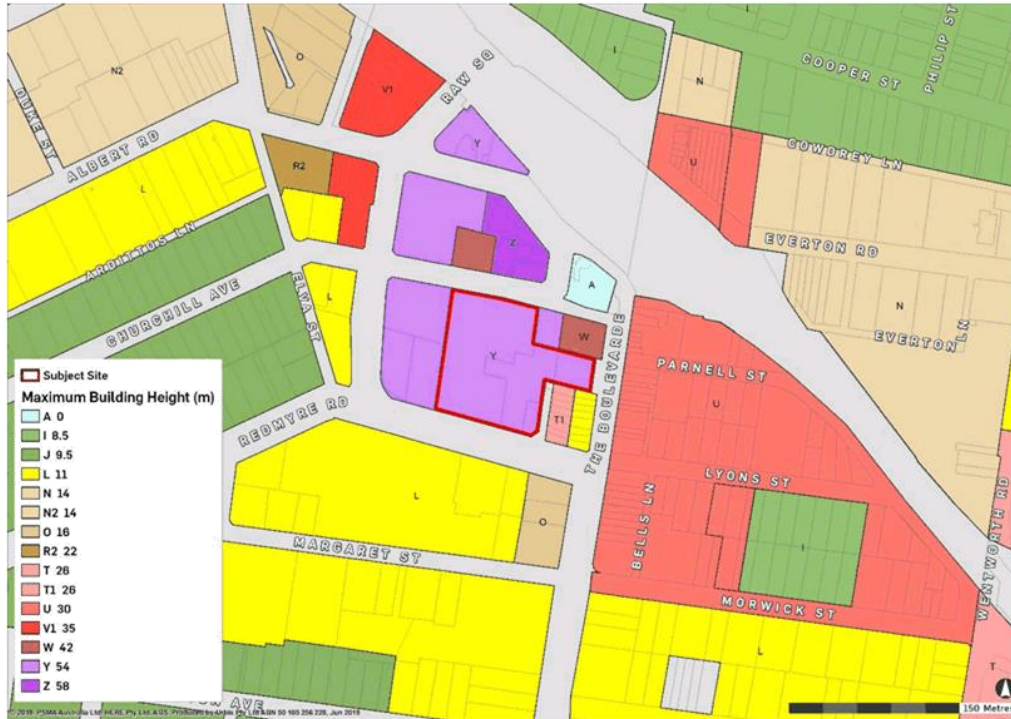


Source: NSW Legislation

4.1.2. Building Height

The site has a maximum building height of **54 metres** (clause 4.3) as shown in Figure 9 below.

Figure 9 – SLEP 2012 – Height of Buildings Map



Source: NSW Legislation

4.1.3. Floor Space Ratio

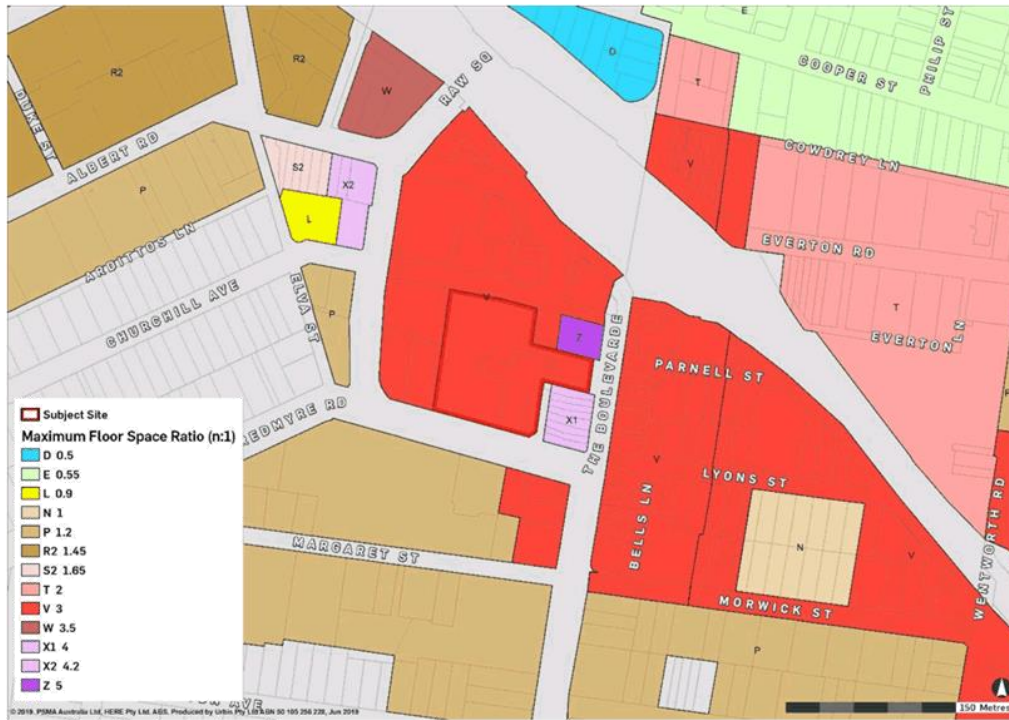
The base floor space ratio identified for the site is **3:1** (clause 4.4), as shown in Figure 10 below. However, the site is able to achieve a maximum floor space ratio of **7.5:1** (clause 4.4B), where the lot is able to meet a series of criteria outlined in SLEP 2012.

Clause 4.4B Exceptions to floor space ratio (Strathfield Town Centre) states:

- (1) *Despite clause 4.4, the maximum floor space ratio for a building on a lot in “Area 2” identified on the Floor Space Ratio Map may exceed 3:1 if the size of the lot exceeds 1,500 square metres but:*
 - a. *must not exceed 5:1, or*
 - b. *if the building will meet the design excellence criteria specified in clause 6.7 – must not exceed 7.5:1.*

The site is included in “Area 2” on the floor space ratio map and meets the minimum lots size criterion. Therefore, the site is eligible for the additional 5:1 FSR achieved through the design excellence criterion provided in clause 6.7. This is discussed further in Section 4.1.5.

Figure 10 – SLEP 2012 – Floor Space Ratio Map



Source: NSW Legislation

4.1.4. Heritage Conservation

The site is not identified as a local heritage item, nor is it located within a heritage conservation area. However, there are several local heritage items and a conservation area located adjacent to and within the vicinity of the site, including:

- Item C15: The Boulevarde Retail Conservation Area
- Item I200: Two Storey Shops, 35-39 The Boulevarde
- Item I201: "Keary's Corner" – Victorian Shop, 39 The Boulevarde

Figure 11 maps heritage items identified in the SLEP 2012 and their relationship to the proposed site. *Clause 5.10 Heritage Conservation and Schedule 5 Environmental heritage* in SLEP 2012 provides guidance on the treatment of heritage items and process to propose new development applications on or in the vicinity of a heritage item.

Figure 11 – SLEP 2012 – Heritage Map



Source: NSW Legislation

4.1.5. Design Excellence

Clause 6.7 Design excellence for Strathfield Town Centre in the SLEP 2012 lists criteria to demonstrate design excellence, including:

- (a) has a high standard of architectural, landscape and urban design, will be constructed with quality materials and has a high standard of detailing that reflects the building type, location and the surrounding buildings, and
- (b) will significantly improve the quality and amenity of the public domain through the form, external appearance and ground level detailing of the development that addresses any heritage or streetscape issues, and
- (c) will not detrimentally impact on the amenity of the surrounding area, nor on any view corridors, vistas or landmark locations, and
- (d) will not detrimentally impact on pedestrian movements and experience, but will reinforce the public transport interchange as a focal point of movement for the area and facilitate the ease of such movement, and
- (e) is designed to encourage an integrated land use mix that reflects the desired future high quality town centre that supports a vibrant economic location, a lively social and community hub, and a diversity of public open spaces at the ground level, as well as the roof and other levels of the building, and
- (f) includes building massing, modulation and bulk that is appropriate in the context of surrounding buildings in terms of its separation, setback, building street height and amenity and its relationship to such buildings, and

(g) *has achieved a high level of ecologically sustainable design, including low-energy or passive design, and minimises environmental impacts such as overshadowing, wind effects and reflectivity, and*

(h) *supports designed sustainable urban mobility through a high quality pedestrian environment, high quality provision for cycling infrastructure, high quality service access, circulation and vehicular design.*

Once the consent authority is satisfied the applicant has demonstrated the criteria listed above, the proposed development is eligible of the maximum FSR of 7.5:1 as detailed in Section 4.1.3.

4.1.6. Additional provisions for development in Strathfield Town Centre

Clause 6.8 of SLEP provides *Additional provisions for development in Strathfield Town Centre*. The clause limits the proportion of residential development in Strathfield Town Centre by placing a cap on residential accommodation permitted on land identified as "Area 2" and "Area 3" on the Floor Space Ratio Map. This includes the subject site.

The provision states: *"Development consent must not be granted for development that is a building on a lot, being land to which this clause applies, that has an area greater than 1,500 square metres unless the consent authority is satisfied that the part of the building that will be used for residential accommodation will not exceed 35% of the floor space of the building"*.

4.2. STRATHFIELD DEVELOPMENT CONTROL PLAN 2005

The *Strathfield Development Control Plan* (DCP 2005) was adopted by Council on 4 April 2006 and came into force on 3 May 2006. The Strathfield DCP 2005 provides guidelines for new development and encourages high quality design based on sound planning principles, sustainability, and to enhance the quality of the landscape, streetscape character and amenity.

Council has also adopted site specific controls for strategic redevelopment areas through the Strathfield Local Government Area (LGA), including Strathfield Town Centre, which is subject to *Development Control Plan No 13* (DCP 13). In instances where there is inconsistency between the general provisions of DCP 2005 and those of DCP 13, the DCP 13 is considered by Council to prevail.

4.3. STRATHFIELD DCP NO 13 – STRATHFIELD TOWN CENTRE

Development Control Plan No 13 has been prepared to control and guide the nature, form and scale of the Strathfield Town Centre. Under DCP No 13 the subject site is envisaged to strengthen the Strathfield Town Centre's role as a municipality's major retail and commercial centre. The site specific DCP identifies a series of objectives to promote redevelopment of the town centre, including:

- *To accommodate or modify new development in order to respect existing Centre elements and strengthen Centre function.*
- *To achieve a high quality of design of the built environment.*
- *To enhance the street environment and general visual appearance of the Centre.*
- *To promote buildings with human scale, detailing and materials.*
- *To encourage sympathetic infill development and restoration of original detail whenever possible.*
- *To encourage improvement of existing building performance or appearance where redevelopment is not achievable.*
- *To ensure development complements Council improvements within public road reservations.*
- *To protect buildings and places of heritage significance and ensure that new development complements rather than detracts from that significance.*
- *To provide sufficient and appropriate facilities for buses and taxis.*
- *To improve pedestrian access, movement and amenity to and within the Town Centre.*

- *To provide access for disabled.*
- *To ensure that new development provides for sufficient car parking within the Centre to accommodate any likely future demand.*
- *To ensure adequate provision of loading, service and emergency vehicle access.*
- *To encourage more street front retail development within the Centre of a type providing a service to the public.*
- *To encourage diversity of use, pedestrian activity and vibrancy within the Centre including its public spaces.*
- *To ensure appropriate levels of sunlight access to public spaces and protection from adverse elements such as wind, rain, noise and fumes.*
- *To ensure any development of railway land complies with the objectives of this Plan.*
- *To ensure that all new development comply with the Sydney Electricity policy for undergrounding or bundles overhead cabling of electricity services.*

While these objectives are relevant to the future redevelopment of the site, the built form controls are outdated and do not align with the development standards contained in SLEP 2012, nor do they align with the strategic objectives and directions produced by the Greater Sydney Commission. The DCP fails to leverage Strathfield Town Centre's strategic location adjacent to a major train station and contribute to the 30-minute city vision. For these reasons it is considered that DCP No 13 is no longer a relevant guide for the future redevelopment of the site.

It is noted that the purpose and status of development control plans is to "provide guidance" to proponents and Councils in achieving land use zone objectives and facilitating permissible development under an environmental planning instrument. In instances where there is an inconsistency between a development control plan and a local environmental plan, the local environmental plan is considered to prevail.

5. INDICATIVE DEVELOPMENT CONCEPT

5.1. OVERVIEW

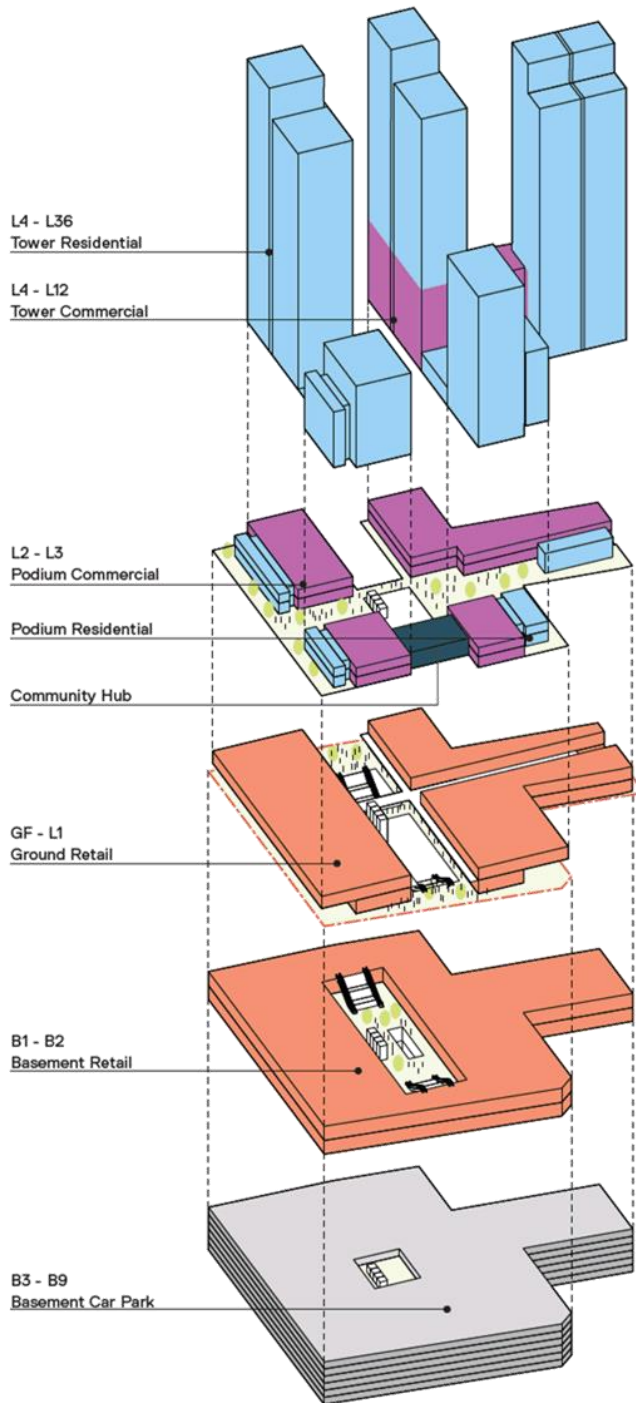
An indicative development concept has been prepared by *Grimshaw Architects* and is detailed in the Urban Design Report at **Appendix A**. The key components of the concept design include:

- A vibrant and active retail plaza at the ground and lower floors with provision for supermarkets, speciality retail, restaurants and cafes.
- A publicly accessible through site link and plaza, providing much needed open space for the Town Centre, activating the ground plane and facilitating direct pedestrian connectivity between Strathfield Station and the wider precinct.
- A commercial office campus, with versatile floorplates to support a broad range of market requirements, interconnected by landscaped terraces and communal meetings spaces.
- Five residential towers ranging in height from 13 to 38-storeys providing approximately 753 apartments of varying sizes, typologies, and layouts including one, two, three and four bedroom units with rooftop communal open spaces.
- Provision for 10% of the uplift in gross floor area to be dedicated as 'key worker' subsidised rental housing for a period of 10 years.
- Dedication of a 700m² community centre.
- A new Transport Hub incorporating a bus interchange, taxi / ride-share drop-off and pick-up, and bicycle parking with direct connections to the existing Strathfield Station, facilitating and encouraging use of sustainable transport options.

Table 1 – Indicative Development Concept – Key Components

Component	Amount	Percentage
<i>Gross Floor Area</i>		
Retail	19,185 sqm	17.9%
Commercial	16,666 sqm	15.6%
Residential	70,456 sqm	65.8%
Community	700 sqm	0.65%
Total	107,007 sqm	100%
<i>Apartments</i>		
Studio	38	5%
One Bedroom	166	22%
Two Bedroom	406	54%
Three Bedroom	131	17%
Four Bedroom	12	2%
Total	753	100%

Figure 12 – Indicative Development Concept – Land Use Structure



Source: Grimshaw

5.2. RETAIL

Continuing the important legacy held by Strathfield Central - a keystone of the local community - the proposed development will substantially increase the provision of retail, and provide a world class shopping precinct for Strathfield.

The 19,185m² of retail floor space is organised over four levels, two below ground and two above ground. These levels are connected by a generous Central Atrium, which allows light to reach all circulation areas providing a vastly improved user experience compared to the current situation. The Central Atrium is covered by a glazed canopy, which provides protection from the elements, filters light into the space and acts as an acoustic barrier between the public spaces at ground level and the commercial and residential land uses above (refer Figure 13).

The new Strathfield Central maintains its current on-grade connection to Strathfield Square, but serves as a secondary public space to support interaction within the community. Large, civic feature stairs are located at the North and South entrances to the plaza, which lead users down to the retail at B1 at the base of the atrium. These stairs also provide respite and form raked seating, encouraging the central plaza to be used as a stage for performances and events.

Restaurants flank the entrances to the Central Plaza from the North (Town Square), East (The Boulevard) and South (Redmyre Road) at Level 01, giving animation to the primary gateways and views across the site. The lowest level of retail has been designed to accommodate an anchor Supermarket, with good access to the basement loading bay and direct access from the car park.

Figure 13 – Photomontage of proposed Strathfield Central retail and commercial podium



Source: Grimshaw

Figure 14 – Photomontage of proposed Strathfield Central Food Court



Source: Grimshaw

5.3. PUBLIC PLAZA

To align with Council's desire for improved public amenity within Strathfield Town Centre a new public open space is proposed. The Central Plaza, which dissects the site north-south from Churchill Avenue to Redmyre Road and east-west from The Boulevard to the new transport hub, will provide a high quality public realm and new destination for local residents.

The Plaza delivers a permeable pedestrian network enabling unrestricted access through the Town Centre, connecting existing public transport modes with the wider neighbourhood. The Plaza provides the opportunity for community engagement, through cultural and social opportunities, whilst also supporting ground level activation through enhanced foot traffic.

Figure 15 – Proposed Strathfield Central Public Plaza



Source: Grimshaw

Figure 16 – Proposed Strathfield Central Plaza and ground plane circulation diagram



Source: Grimshaw

5.4. COMMERCIAL

The development provides a total of 16,666m² commercial office floor space. The commercial campus forms an integral part of the mixed-use concept for Strathfield Central. The campus is located on levels 02 and 03 of the podium and levels 04 to 11 of Tower 2 and 3, situated on the boundary with 1-9 The Boulevard and 14 Strathfield Square.

The campus is accessed by independent lobbies positioned at key ground floor locations. Varied floor plate sizes and layouts are provided ensuring the development is able to attract a range of employment generating uses. The campus acts as a green buffer, defining the top of the podium and providing separation between the retail and residential land uses.

The provision of a commercial core will help secure the future prosperity of Strathfield Town Centre by drawing jobs into the area and by providing existing local businesses with a resilient daytime economy. The existing financial and medical services currently operating in Strathfield Central would be well suited to the new commercial office space.

The proposed commercial campus will benefit from proximity to Strathfield Station with great accessibility to the Sydney CBD, Parramatta and Sydney Olympic Park. The campus will provide workers with flexible modern floorplates, district views, communal facilities and amenities, and direct access to high quality landscaped spaces.

Figure 17 – Commercial Campus – Typical Floor Plan



Source: Grimshaw

Figure 18 – Photomontage of proposed Strathfield Central Commercial Campus

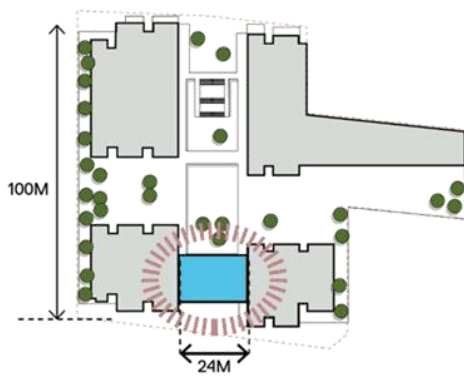


5.5. COMMUNITY HUB

The proposal incorporates a 700m² multi-purpose community hub. This is a shared facility that can be used for the local and broader community. The floorplate is versatile and flexible, while the prominent location on Levels 3 and 4 holds a unique position within the development and visibly engages with the public realm.

The community hub will contribute to the provision of high quality community facilities within the Strathfield Town Centre and wider Local Government Area.

Figure 19 – Proposed Strathfield Central Community Hub



5.6. RESIDENTIAL

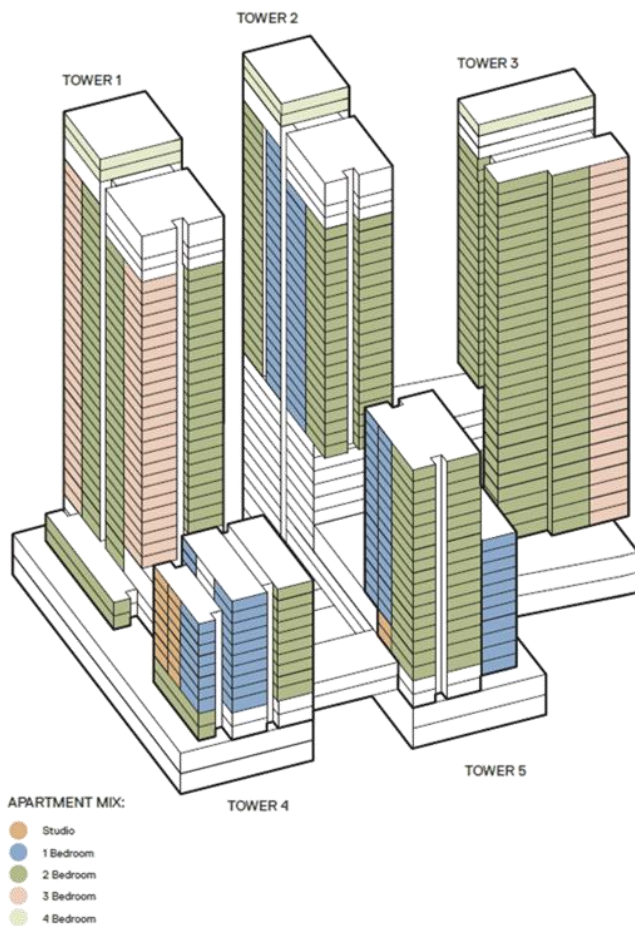
The residential component is located within five towers which extend beyond the retail and commercial podium. The towers range in height from 13-storeys to 38-storeys and are separated by a minimum of 24 metres in order to achieve visual and acoustic privacy.

A total of 753 apartments are provided, including a mix of studio, one, two, three and four bedroom apartments with varying layouts, orientation and sizes.

Towers 4 and 5 are reduced in height in order to protect sunlight access to neighbouring properties, particularly the existing school, open space and residential uses to the south. The towers provide a transition in height across the site, from the high density commercial hub around Strathfield Station to the medium density mixed-use neighbourhood to the south.

Each tower has its own ground level street entrance, away from the primary retail activation areas. The towers contain between 6 and 8 apartments arranged around a central core. The apartments are oriented to achieve solar access and natural cross ventilation, whilst also maintaining visual and acoustic privacy. Each core gives direct access from the tower to the residential basement levels at B7-B9 inclusive.

Figure 20 – Residential Apartment Mix

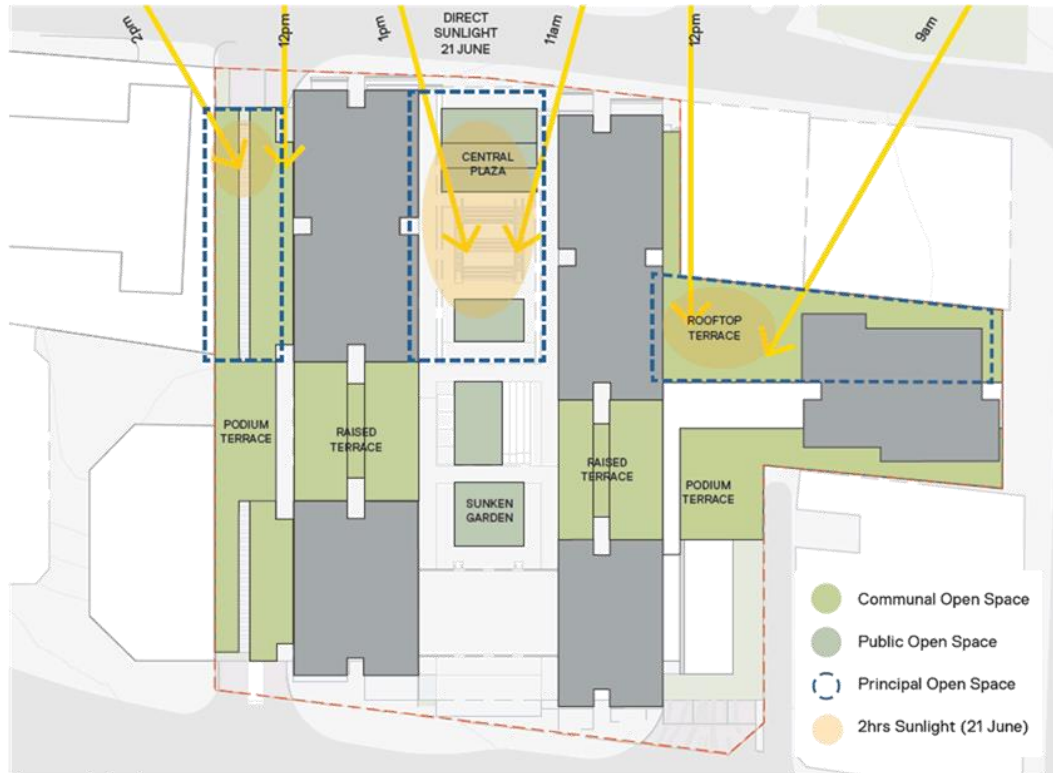


Source: Grimshaw

5.7. COMMUNAL AND PUBLIC OPEN SPACE

In addition to the Central Plaza and Sunken Garden at ground level, the development provides communal open space for the enjoyment of the future residents across multiple podium and rooftop levels (refer Figure 21). The combined area of open space is equivalent to 25% of the site area. Further, 50% of the communal open space receives a minimum of 2 hours of sunlight between 9 am and 3 pm in mid-winter.

Figure 21 – Communal and Public Open Space



Source: Grimshaw

5.8. TRANSPORT HUB

The proposal provides a unique opportunity for Council and the NSW Government to redefine the Strathfield Town Centre transport interchange. The development concept proposes to deliver a new Transport Hub incorporating a bus interchange, taxi / ride-share drop-off and pick-up, and bicycle parking with direct connections to the existing Strathfield Station.

The new Transport Hub would provide a safe and secure environment 24 hours a day, 7 days a week. The relocated bus interchange and taxi / ride-share drop-off and pick-up would significantly improve the existing pedestrian environment around Albert Road, Churchill Avenue and Strathfield Square by removing the existing conflict between vehicles and pedestrians.

The new Transport Hub incorporates a sheltered and enclosed waiting area for pedestrians resulting in improved comfort, away from harsh weather conditions. By integrating the new Transport Hub within the mixed-use Strathfield Central development there is a greater amount of activation resulting in improved passive and active surveillance and increased patronage.

Figure 22 – Photomontage of proposed Strathfield Central Transport Hub



Source: Grimshaw

5.9. PUBLIC BENEFIT OFFER

Under Section 7.4 of the EP&A Act, a proponent may enter into a Voluntary Planning Agreement (VPA) where a change is sought to an environmental planning instrument, under which the developer agrees to dedicate land free of cost, pay a monetary contribution, and/or provide any other material public benefit to be used for or applied towards a public purpose. A draft VPA is normally prepared following 'Gateway' approval of a Planning Proposal and the associated Public Benefit Offer.

The proponent is prepared to enter into discussions with Council to contribute to a range of **direct** and **indirect** Public Benefits in association with this Planning Proposal, which may include:

Direct

- Delivery of a new vibrant Public Plaza dissecting the site north-south and east-west to be accessible 24 hours a day, 7 days a week.
- Delivery of a new integrated public transport hub.
- Delivery of a 700m² multi-purpose community facility.
- Delivery of affordable housing for key workers – equivalent to 10% of the gross floor area uplift;

Indirect

- Embellishment of Churchill Avenue to create an extended public realm and share-way, connecting Strathfield Square with Strathfield Plaza.
- Creation of a subterranean pedestrian link, creating a safe, secure and direct connection between Strathfield Station and the Strathfield Central transport hub.
- Creation of a new footbridge crossing over Redmyre Road, facilitating improved public access to the Strathfield Central transport hub and Strathfield Station.

Following preliminary review of this Planning Proposal and discussions with Council, the proponent will confirm further details of the proposed Public Benefit Offer. Once the Planning Proposal receives 'Gateway' approval, this Public Benefit Offer will be translated into a Draft VPA for exhibition with the Draft SLEP 2012 Amendment.

6. THE CASE FOR CHANGE

Achievement of the vision for the redevelopment of the site and the associated arising significant public benefits, requires amendment to existing planning controls. There are compelling reasons why the Planning Proposal should be supported, summarised as follows:

6.1. CATALYST FOR GROWTH

Strathfield Town Centre is positioned within a strategically important urban growth corridor between Parramatta and the Sydney CBD. The corridor is the focus of urban renewal due to its excellent public transport connectivity, and proximity to existing and planned employment opportunities, health and education services, and community facilities.

Strathfield Central is the largest and most strategically located landholding within Strathfield Town Centre. Accordingly, the proposal presents a unique opportunity to revitalise the Strathfield Town Centre.

The site is capable of providing significant uplift in density without major impacts on public infrastructure or adjacent uses. Further, it is one of the few remaining major development sites in the Town Centre not affected by heritage, access, small lot size, or strata-title constraints.

Redevelopment of Strathfield Central provides the catalyst for urban renewal and growth of the Strathfield Town Centre and wider Local Government Area by encouraging other land owners, Council and the State Government to invest in the area. Further, the increase in population through greater employment and residential uses creates impetus and flow on benefits to other businesses and services in the area.

6.2. PLANNING CONTROLS INHIBIT GROWTH

Current planning controls applicable to the Strathfield Central site under *SLEP 2012* and Strathfield DCP 2005 are outdated and not reflective of the desired strategic future for Strathfield.

The current maximum FSR of 7.5:1 cannot be delivered within the limiting 54 metre maximum building height standard. Further, the current 30% cap on residential accommodation within the Town Centre is at odds with the Council's aspirations for a diverse and vibrant hub, and results in such a significant quantum of commercial floor space (i.e. >50,000m²) that it renders redevelopment option unviable.

There is an imperative for the current planning controls to be changed to better reflect the strategic futures desired for Strathfield Town Centre and the Strathfield Central site.

6.3. SIGNIFICANT PUBLIC BENEFITS

The Strathfield Central Planning Proposal embraces the critical place it occupies in the Strathfield Town Centre through creating significant direct and indirect public benefits.

Key direct public benefits provided by the proposal area:

- Creation of a large Public Plaza dissecting the site north-south and east-west to be accessible 24 hours a day, 7 days a week. This plaza is designed as a gathering space for people and is carefully designed to facilitate safe and efficient movement of people around the Town Centre.
- Provision of a public transport hub within the site, facilitating enhanced movement of vehicles and people around the Town Centre. This public transport hub facilitates resolution of the existing congested and unsafe movement that currently occurs around the Town Centre and railway station, fostering the creation of a world class public space benefitting everyone.
- Provision of 700m² space suitable for multi-purpose community facility. The provision of this space reinforces the public focus of the Strathfield Plaza proposal and enhances the appeal and function of the Town Centre as a major gathering place for local people.
- Provision of a significant amount of affordable housing for key workers. The proposal embraces Council's plans to improve opportunities for people from all parts of the community to live in the Town Centre by incorporating a generous offer of providing affordable housing in future development.

The Planning Proposal provides the opportunity to create significant indirect public benefits which can be pursued in partnership with Council. These include:

- Embellishment of Churchill Avenue to create an extended public realm and share-way, connecting Strathfield Square with Strathfield Plaza.
- Provision of a subterranean pedestrian link, creating a safe, secure and direct connection between Strathfield Station and the Strathfield Central transport hub.
- Provision of a new footbridge crossing over Redmyre Road, facilitating greater public access to the Strathfield Central transport hub and Strathfield Station.

6.4. TRANSIT ORIENTED DEVELOPMENT

The Strathfield Central Planning Proposal achieves a contemporary, exemplar Transit Orientated Development optimising development outcomes in an area benefitting from exception transport infrastructure. The site is located in close proximity to high frequency public transport, including rail and bus networks and has easy access to essential services, employment and educational opportunities.

The proposed Transport Hub provides a unique opportunity to deliver a truly integrated public transport interchange for Strathfield with direct connections between the train station, bus interchange, Strathfield Plaza and the wider town centre. The Transport Hub allows the existing bus interchange to be modified resulting in significant improvements to pedestrian amenity by removing vehicles from Albert Road and prioritising pedestrian and bicycle movements.

The proposed Transport Hub supports the existing public transport system, improve the Strathfield Town Centre and promotes the use of alternate transport modes through better connectivity

The combination of high frequency sustainable transport modes, essential services, employment and education all within walking distance can significantly reduce private car dependency through implementing key transport principles within the development. This is highly consistent with government policy focussing density in high access location.

6.5. AN ACTIVE AND VIBRANT TOWN CENTRE

The mix of uses proposed to be delivered at Strathfield Central contributes strongly to enhancing Strathfield Town Centre as an active and vibrant place 24 hours a day, 7 days a week. The publicly accessible Plaza, flanked by retail uses will activate the locality, providing a safe and secure environment for the community to gather at all times of the day.

The proposed mix of retail, office, community and residential uses ensures the Town Centre is populated not only during the typical office or retail operating hours, but on weekends and well into the evening.

6.6. MORE JOBS AND HOMES FOR STRATHFIELD

The redevelopment of Strathfield Central provides a significant contribution to local employment opportunities and diversity through the provision of approximately 5,285 jobs during construction and 1,421 additional jobs upon completion. Strathfield Central will deliver between 40% to 53% of the estimated forecast 10-year demand for jobs in the LGA.

The redevelopment of Strathfield Central provides a significant contribution to local housing choice, diversity and supply. The site can accommodate approximately 753 dwellings within the short-medium term. A mix of one, two, three and four bedroom apartments are provided with a range of sizes and layouts so as to meet the current and future needs of residents. Apartment configurations support diverse household types and stages of life including single person households, families, multi-generational families and group households.

For these reasons, we submit that Strathfield Council (as the relevant planning authority) resolve to initiate the amendment process under section 56 of the EP&A Act and seek a 'gateway determination' from the NSW Department of Planning, Industry and Environment.

7. PLANNING PROPOSAL

This Planning Proposal has been prepared in accordance with Section 3.33 of the EP&A Act with consideration of the NSW Department of Planning and Environment's 'A guide to preparing planning proposals' and 'A guide to preparing local environmental plans', August 2016. Accordingly, the Planning Proposal is addressed in the following six parts:

- **Part 1** – A statement of the objectives and intended outcomes of the proposed instrument.
- **Part 2** – An explanation of the provisions that are to be included in the proposed instrument.
- **Part 3** – The justification of those objectives, outcomes and the process for their implementation.
- **Part 4** – Maps, where relevant, to identify the intent of the planning proposal and the area to which it applies.
- **Part 5** – Details of the community consultation that is to be undertaken on the planning proposal.
- **Part 6** – A project timeline to detail the anticipated timeframe for the plan making process.

7.1. PART ONE: OBJECTIVES OR INTENDED OUTCOMES

The intended outcome of this Planning Proposal is to amend *SLEP 2012* in order to facilitate the future development of Strathfield Central, 11-23 The Boulevard, Strathfield. The proposed amendments to the height of buildings and floor space ratio development standards, as well as other local provisions, will facilitate a landmark mixed-use development incorporating:

- A vibrant and active retail plaza at the ground and lower floors with provision for supermarkets, speciality retail, restaurants and cafes.
- A publicly accessible through site link and plaza, providing much needed open space for the Town Centre, activating the ground plane and facilitating direct pedestrian connectivity between Strathfield Station and the wider precinct.
- A commercial office campus, with versatile floorplates to support a broad range of market requirements, interconnected by landscaped terraces and communal meetings spaces.
- Five residential towers ranging in height from 13 to 38-storeys providing approximately 753 apartments of varying sizes, typologies, and layouts including one, two, three and four bedroom units with rooftop communal open spaces.
- Provision for 10% of the uplift in gross floor area to be dedicated as 'key worker' subsidised rental housing for a period of 10 years.
- Dedication of a 700m² community centre.
- A new Transport Hub incorporating a bus interchange, taxi / ride-share drop-off and pick-up, and bicycle parking with direct connections to the existing Strathfield Station, facilitating and encouraging use of sustainable transport options.

7.2. PART TWO: EXPLANATION OF PROVISION

The objectives and intended outcomes of the Planning Proposal will be achieved by amending the height of buildings and floor space ratio development standards, as well as other local provisions, of SLEP 2012 as they apply to the site as follows:

- Amend the applicable maximum height of buildings development standard, under *Clause 4.3: Height of buildings*, to permit buildings with a height of up to **156 metres**.
- Amend the applicable maximum floor space ratio development standard, under *Clause 4.4 Floor Space Ratio*, to permit a floor space ratio of **9.5:1** and identify Strathfield Central as "Area 4".
- Remove the application of *Clause 4.4B Exceptions to floor space ratio (Strathfield Town Centre)*.
- Amend *Clause 6.7 Design excellence for Strathfield Town Centre* to include "Area 4" on the Floor Space Ratio Map.
- Amend *Clause 6.8: Additional provisions for development in Strathfield Town Centre* on "Area 4" to increase the cap on residential accommodation permitted on the site to 70%.

In addition, it is anticipated that the Planning Proposal will be accompanied by the preparation of a site specific **Development Control Plan** that would further detail the key parameters of development on the site including matters such as building massing, typologies, active frontages, setbacks, access, transport and movement etc. However, it is expected that this document would be developed in consultation with Council following preliminary review of the Planning Proposal.

7.3. PART THREE: JUSTIFICATION

7.3.1. SECTION A – Need for the planning proposal

Q1. Is the planning proposal a result of any strategic study or report?

Yes, the Planning Proposal is entirely consistent with *A Metropolis of Three Cities: Greater Sydney Region Plan (GSC 2018)* and *Eastern City District Plan (GSC 2018)*, which was adopted by the NSW Government in March 2018.

In addition, the Planning Proposal is consistent with the directions established by Strathfield Council in the *Strathfield 2030 Community Strategic Plan (June 2018)*, which identified the need to "develop plans for upgrading Strathfield Town Centre and integrating transport services".

Q2. Is the planning proposal the best means of achieving the objectives or intended outcomes, or is there a better way?

Yes, the proposed amendments to the height of buildings and floor space ratio development standards, as well as other local provisions, of SLEP 2012 are required to facilitate the redevelopment of the site to achieve the anticipated built form and development outcomes described in Section 5 of this report.

7.3.2. SECTION B – Relationship to strategic planning framework

Q3. Is the planning proposal consistent with the objectives and actions of the applicable regional, sub-regional or district plan or strategy (including any exhibited draft plans or strategies)?

Yes, as described in Section 3 of this Report and summarised in Table 2, the Planning Proposal is entirely consistent with the objectives and actions of *A Metropolis of Three Cities: Greater Sydney Region Plan (2018)*, the *Eastern City District Plan (2018)*, and the *Greater Parramatta Interim Land Use and Infrastructure Implementation Plan (2017)*.

Table 2 – Relationship to Strategic Planning Framework

Strategic Plan	Consistency
<p><i>A Metropolis of Three Cities: Greater Sydney Region Plan (2018)</i></p>	<p>The Planning Proposal is consistent with <i>A Metropolis of Three Cities: Greater Sydney Region Plan</i>, as it would:</p> <ul style="list-style-type: none"> • Provide and embellish infrastructure to support a 30-minute city (Direction: A city supported by infrastructure); • Provide services and infrastructure to meet communities’ changing needs, such as a 700m² dedication to Council for community purposes, transport hub and 10% uplift dedicated to key worker housing for a period of 10 years (Direction: A city for people); • Increase the supply of housing near Strathfield Station and responds to increased housing diversity, and choice through the provision of housing stock to suit the needs of a changing population (Direction: Housing the city); • Create a community hub to bring people together (Direction: A city of great places); • Contribute to a 30-minute city and integrates land use and transport to create a walkable environment (Direction: A well-connected city); and • Increase investment and business activity in Strathfield Town Centre to ensure day-to-day access to goods and services is supplied to users (Direction: Jobs and skills for the city).
<p><i>Eastern City District Plan (2018)</i></p>	<p>The Planning Proposal is consistent with <i>Eastern City District Plan</i>, as it would:</p> <ul style="list-style-type: none"> • Provides services and social infrastructure to meet people’s changing needs (Planning Priority E3); • Fosters healthy, creative, culturally rich and socially connected communities (Planning Priority E4); • Provides housing supply, choice and affordability with access to jobs, services and public transport (Planning Priority E5); • Creates and renews great places and local centres, and respects the District’s heritage (Planning Priority E6); and • Delivers integrated land use and transport planning and a 30-minute city (Planning Priority E10).
<p><i>Greater Parramatta Interim Land Use and Infrastructure Implementation Plan (2017)</i></p>	<p>The Planning Proposal is consistent with the interim LUIIP for Greater Parramatta as it will provide housing in close proximity to existing and planned transport links, and will contribute to the Greater Sydney Commission’s vision for a connected, unified heart at Greater Sydney’s true centre.</p>

Q4. Is the planning proposal consistent with a council's local strategy or other local strategic plan?

Yes, the Planning Proposal is consistent with the following relevant local strategy and planning studies as described in Section 3 of this Report and summarised in Table 3 below.

Table 3 – Relationship to Local Strategic Plans and Planning Studies

Local Council Plans and Studies	Consistency
<i>Strathfield 2030 Community Strategic Plan</i>	<p>The Planning Proposal is consistent with the <i>Strathfield 2030 Community Strategic Plan</i>, as it would:</p> <ul style="list-style-type: none"> • Provide Strathfield with an intuitive and well-planned infrastructure system, such as the redesigned bus interchange, that can support future population growth and maximise its potential as a major transport interchange. • Provide Strathfield's residents with a safe and easily accessible infrastructure that will ease traffic congestion and promote pedestrian activation. • Foster Strathfield's strong sense of community by providing accessible and high-quality public amenities at the heart of town. • Provide Strathfield with accessible leisure and community facilities that will help celebrate diversity and support tolerance within the population. • Revitalise the town centre with a public space that can provide Strathfield with a nucleus for the community and reinforce sense of place. • Provide a diverse range of modern apartments with communal gardens and shared amenities • Create a strong and resilient commercial core supporting a diversity of business. • Provide Strathfield with its own distinct identity as a retail destination.
<i>Strathfield Residential Land Use Study</i>	<p>The Planning Proposal is consistent with the key conclusions and recommendations of the <i>Strathfield Land Use Study</i> (November 2011), as follows:</p> <ul style="list-style-type: none"> • The Strathfield Residential Land Use Study indicated that the total existing capacity and recently constructed/ approved dwellings within the Strathfield LGA equated to approximately 4,900 (as at the time of the report). This represents just under 60% of the overall target for new residential development in the LGA as stipulated in the Inner West Subregional Strategy (8,300 dwellings by 2031) and a shortfall of approximately 900 dwellings to reach the Stage 1 (2021) housing target. • The Study acknowledged the review of planning controls and made recommendations to promote the redevelopment of the Strathfield Town Centre, which includes the following: <ul style="list-style-type: none"> ○ Envisioned built form: B4 Mixed Use Residential ○ Floor Space Ratio: 3.1 + 3.4 bonus ○ Height: 22 storeys (90m) <p>These planning control recommendations reflect the opportunities to leverage off Strathfield's strategic location and increase residential capacity.</p>

Local Council Plans and Studies	<p>Consistency</p> <ul style="list-style-type: none"> The proposed amendment to the SLEP 2012 will facilitate the delivery of approximately 753 dwellings in the short-medium term. The site is a rare opportunity for redevelopment to allow revitalisation of the town centre, provide 21st century services to meet the needs of residents, and leverage off the proximity to regionally significant transport interchange. As such, the site is capable of providing a significant uplift in density without major impact on the amenity of existing residents.
<i>Strathfield at the Crossroads of Sydney: A Economic Land Use and Employment Strategy</i>	<p>The Planning Proposal is consistent with <i>Strathfield at the Crossroads of Sydney: A Economic Land Use and Employment Strategy</i>, as it would:</p> <ul style="list-style-type: none"> Cements Strathfield Town Centre as the primary centre in the LGA. Provides a diversity of mixed uses and variety of retail. Improves local transport connections with the bus interchange and intermodal connections. Provides opportunities for additional commercial uses, such as office.

Q5. Is the planning proposal consistent with applicable State Environmental Planning Policies?

Yes, the Planning Proposal is consistent with the following applicable State Environmental Planning Policies (SEPP):

- SEPP No 65 – Design Quality of Residential Apartment Development:** The concept design has been prepared with regard to SEPP 65 and the accompanying Apartment Design Guide (ADG). In particular, the concept design is able to achieve the required solar access, natural cross-ventilation, building separation, apartment size and layout requirements. Detailed compliance with SEPP 65 and the ADG will be demonstrated as part of the future development application.
- SEPP No 70 – Affordable Housing (Revised Schemes):** SEPP No. 70 is not specifically relevant to the Planning Proposal but may be a consideration for the future development application.
- SEPP (Building Sustainability Index: BASIX) 2004:** The future development application will be accompanied by a BASIX assessment and certificate.
- SEPP (Infrastructure) 2007:** A referral to NSW Roads and Maritime Services for traffic generating development will be required at the development application stage.

Q6. Is the planning proposal consistent with applicable Ministerial Directions (s.9.1 directions)?

Yes, the Planning Proposal is consistent with the applicable Ministerial Directions under section 9.1 of the EP&A Act, as discussed in Table 4 below.

Table 4 – Section 9.1 Directions

Direction and Objective	Comment
Direction 1.1 Business and Industrial Zones	
<p>(1) <i>The objective of this direction are to:</i></p> <ul style="list-style-type: none"> a. <i>encourage employment growth in suitable locations,</i> b. <i>protect employment land in business and industrial zones, and</i> c. <i>support the viability of identified centres.</i> 	<p>The Planning Proposal retains the B3 Commercial Core zone. The proposal seeks to provide a variety of employment uses, including commercial, retail and community facilities.</p> <p>An economic impact assessment has been prepared by HillPDA (refer Appendix B). The assessment confirms the Planning Proposal will result in:</p> <ul style="list-style-type: none"> • A significant contribution to employment opportunities and diversity through the provision of around 5,285 jobs/year arising from construction and 1,421 additional jobs when completed. • The value added to the local economy is \$135,7 million every year. <p>The planning proposal complies with Direction 1.1 as it presents an opportunity to revitalise the Strathfield Town Centre.</p>
Direction 2.3 Heritage Conservation	
<p>(2) <i>The objective of this direction is to conserve items, areas, objects and places of environmental heritage significance and indigenous heritage significance.</i></p>	<p>The Planning Proposal does not have any heritage significance but is adjacent to locally listed heritage items and heritage conservation area.</p> <p>Careful consideration and mitigation measures have been applied to minimise any negative impacts on the heritage items. A heritage impact assessment has been prepared by Urbis (refer Appendix E). The report acknowledges the conservation of the adjacent heritage items and conservation area is dependent on a suitable demand for retail premises, which can only be achieved by a vibrant town centre, which the planning proposal aims to rectify.</p>
Direction 3.1 Residential Zones	
<p>(1) <i>The objectives of this direction are:</i></p> <ul style="list-style-type: none"> a. <i>to encourage a variety and choice of housing types to provide for existing and future housing needs,</i> b. <i>to make efficient use of existing infrastructure and services and ensure that new housing has</i> 	<p>The Planning Proposal proposes to introduce an apartment mix, which allows future residents to be close to a major transport interchange meeting the 30-minute city vision. The proposal seeks to provide 10% of the uplift to key workers housing.</p>

<p>Direction and Objective</p> <p><i>appropriate access to infrastructure and services, and</i></p> <p><i>c. to minimise the impact of residential development on the environment and resource lands.</i></p>	<p>Comment</p>
<p>Direction 3.4 Integrating Land Use and Transport</p>	
<p><i>(1) The objective of this direction is to ensure that urban structures, building forms, land use locations, development designs, subdivision and street layouts achieve the following planning objectives:</i></p> <p><i>a. improving access to housing, jobs and services by walking, cycling and public transport, and</i></p> <p><i>b. increasing the choice of available transport and reducing dependence on cars, and</i></p> <p><i>c. reducing travel demand including the number of trips generated by development and the distances travelled, especially by car, and</i></p> <p><i>d. supporting the efficient and viable operation of public transport services, and</i></p> <p><i>e. providing for the efficient movement of freight.</i></p>	<p>The site is well serviced by a range of public transport networks. It is located within walking distance of Strathfield Railway Station. Services on the main line through these stations generally operate with headways of 5 to 10 minutes in each direction during peak periods and 10 to 15 minutes in each direction outside peak hours.</p> <p>Local and regional bus services through the area are provided by Sydney Buses. There are bus stops on both side of Strathfield Station in the vicinity of the site.</p> <p>The proximity of these transport services will encourage public transport use, cycling or walking and discourage use of private transport.</p>
<p>Direction 3.5 Development Near Regulated Airports and Defence Airfields</p>	
<p><i>(1) The objectives of this direction are:</i></p> <p><i>a. to ensure the effective and safe operation of regulated airports and defence airfields;</i></p> <p><i>b. to ensure that their operation is not compromised by the development that constitutes an obstruction, hazard or potential hazard to aircraft flying in the vicinity; and</i></p> <p><i>c. to ensure development, if situated on noise sensitive land, incorporates appropriate mitigation measures so</i></p>	<p>The obstacle limitation surface for the Sydney Airport applies to the proposed development. The proposed heights are within the outer horizontal surface maximum of 156m AHD.</p>

Direction and Objective	Comment
<p><i>that the development is not adversely affected by aircraft noise.</i></p>	
Direction 4.3 Flood Prone Land	
<p>(1) <i>The objectives of this direction are:</i></p> <ul style="list-style-type: none"> a. <i>to ensure that development of flood prone land is consistent with the NSW Government's Flood Prone Land Policy and the principles of the Floodplain Development Manual 2005; and</i> b. <i>to ensure that the provisions of an LEP on flood prone land is commensurate with flood hazard and includes consideration of the potential flood impacts both on and off the subject land.</i> 	<p>The Infrastructure Services Assessment prepared by Cardno and including at Appendix D provides a description of the potential flood impacts both on and off the subject site and outlines the flood planning levels to be further developed through consultation with Council and other service authorities.</p>
Direction 7.1 Implementation of A Plan for Growing Sydney	
<p>(1) <i>The objective of this direction is to give legal effect to the planning principles; directions; and priorities for subregions, strategic centres and transport gateways contained in A Plan for Growing Sydney.</i></p>	<p>The Planning Proposal is entirely consistent with the objectives and actions of <i>A Plan for Growing Sydney (2014)</i>. Specifically, the Planning Proposal aims to increase housing supply in an area with excellent access to public transport, employment and education opportunities, retail, and community facilities and services.</p>

7.3.3. SECTION C – Environmental, social and economic impact

Q7. Is there any likelihood that critical habitat or threatened species, populations or ecological communities, or their habitats, will be adversely affected as a result of the proposal?

No. the site is situated within an urban context and was historically used for commercial purposes. The site is highly modified and therefore it is expected that the Planning Proposal will not affect any critical habitat or threatened species, populations or ecological communities.

Q8. Are there any other likely environmental effects as a result of the planning proposal and how they are proposed to be managed?

The Planning Proposal is not expected to give rise to any unreasonable environmental impacts. Where potential environmental impacts have been identified, mitigation and management measures have been provided. The following section provides a summary of the potential environmental impacts and management measures.

RESIDENTIAL AMENITY

As detailed in the Urban Design Report prepared by *Grimshaw Architects* (refer **Appendix A**) the concept design accompanying this Planning Proposal has been designed with regard to SEPP 65 and the accompanying ADG. The concept design is able to achieve compliance with the key numerical standards detailed in the ADG including solar access, natural ventilation, apartment size and layout, private and communal open space, and building separation requirements. Specifically:

- 613 of the 753 apartments (or 81%) receive a minimum of 2 hours solar access to the living room and private open space during mid-winter.
- 90 of the 162 apartments (or 90%) within the first 9-storeys of the development are naturally cross-ventilated.
- 25% of the site area is provided as communal open space and 50% of the principal open space receives a minimum of 2 hours solar access during mid-winter.
- Building separation is provided in accordance with the ADG requirements, including a minimum of 24m between habitable rooms eight-storeys and above.

Detailed compliance with the relevant State and local residential amenity controls will be provided with any future development application.

OVERSHADOWING

An overshadowing assessment is provided in the Urban Design Report prepared by Grimshaw and included at **Appendix A**. The assessment was prepared in order to identify and mitigate against potential shadow impacts on the following surrounding sensitive receivers:

- Meriden School open space
- Santa Sabina Early Education and Infant School open space
- Apartment units immediately south on Redmyre Road
- High rise apartment block on Morwick and Lyons Streets
- Residential houses and units in the wider vicinity

The assessment informed the ultimate height and location of the proposed podium and residential tower heights across the site.

The shadow diagrams provided demonstrate the tower massing and height has been carefully considered to ensure there is no overshadowing to the open space of the open space within Meriden School during play lunch hours (12pm-2pm) as a result of the development.

With regard to the adjacent 'The Boulevard Apartments' a majority of apartments currently receive direct sunlight in the morning (min. 9am-10am), with a second hour in the afternoon (2pm-3pm). Due to its location east of Strathfield Central, approximately 70-80% of apartments are unaffected by the proposed development. In summary, the proposal for Strathfield Central has little or no negative impact upon the surrounding sensitive receivers.

ENVIRONMENTAL SUSTAINABILITY

The proposal identifies key initiatives and performance targets to be further considered during design development, including:

- Achieving a Green Star 5-6 Star rating.
- Achieving a NABERS rating of 5* (commercial campus) – through the use of high performance façades, LEDs throughout, co-generation for pool, local ventilation, common area natural ventilation, and economy cycle mechanical systems.
- Achieving a BASIX water 45+ target (residential) – through rainwater collection for irrigation, cooling towers, and grey water, efficient fixtures and fittings, and condensate capture.

- Achieving a BASIX energy 30+ target (residential) – though the use of high performance façades, double-sided natural ventilation and wintergardens, appliance package (washing machine, dryer, dishwasher, fridge), four pipe fan coil units in apartments, day/night air conditioning operation, common area natural ventilation, LEDs throughout + control, and co-generation for pool.

TRAFFIC AND TRANSPORT

A Traffic and Transport Assessment has been prepared by *Cardno* and is included at **Appendix C**. The findings of the assessment are summarised as follows.

The site is conveniently located to take advantage of the connectivity of existing public transport services and support the greater use of sustainable modes of transport, likely reducing reliance on private vehicles.

The subject site is currently well served by public transport services being 100 metres from Strathfield Station. The station is served by Sydney Trains T1 North Shore, Northern & Western Line and T2 Inner West & Leppington Line suburban services as well as NSW TrainLink Intercity and regional services. Strathfield Station is a junction point on the Sydney Trains network.

The Strathfield Town Centre is also serviced by a major bus interchange with services to Ryde, Parramatta, Rhodes, Burwood, Liverpool, Fairfield, Hornsby, and the Sydney CBD. The transport interchange also provides access taxi services.

Journey to work data from the 2016 Census demonstrates that the private car dependency of people within Strathfield is significantly lower than Greater Sydney. This is the result of the high frequency public transport options provided, which allow easy connections to Parramatta and the Sydney CBD.

The Strathfield Central Planning Proposal would achieve the principles and benefits of a **Transit Orientated Development** (TOD), leveraging existing and future / planned investment in transport infrastructure. As described above, the site is located in close proximity to high frequency public transport, including rail and bus networks and has easy access to essential services, employment and educational opportunities.

The combination of high frequency sustainable transport modes, essential services, employment and education all within walking distance can significantly reduce private car dependency through implementing key transport principles within the development.

The Planning Proposal has the ability to transform Strathfield into an exemplar TOD characterised by:

- A rapid and frequent transit service;
- High accessibility to the transit station;
- A mix of residential, retail, commercial and community uses; and
- High quality public spaces and streets, which are pedestrian and cyclist friendly.

The proposed Transport Hub provides a unique opportunity to re-think the existing interchange and achieve improved safety and operational efficiencies. The benefits of such a proposal are:

- Provides a sheltered and enclosed waiting area for pedestrians resulting in improved comfort, away from harsh weather conditions.
- Allows the existing bus interchange to be modified to improve pedestrian amenity between Strathfield Station, Strathfield Plaza and the wider Town Centre. Notably, the existing Taxi rank and Kiss 'n' Ride areas can be relocated to pedestrianize Albert Road immediately in front of the station entrance.
- Improve congestion during peak times by reducing the number of conflict points and level of interaction between buses stopping and cars circulating within the town centre.

The proposed Transport Hub will support the existing public transport system, improve the local area of Strathfield Town Centre and promote the use of alternate transport modes through better connectivity.

The proposed car parking strategy and provision for the site adopts outcomes in line with TOD, including restricting on-site car parking in order to influence travel mode choice, along with the delivery of green travel plans, bicycle parking, and improved access to car share facilities to reduce reliance on private car dependency.

INFRASTRUCTURE SERVICES

An Infrastructure Services Assessment has been prepared by *Cardno* and is included at **Appendix D**. The key findings of the assessment are summarised as follows.

- Two existing trunk Sydney Water stormwater culvert assets traverse the site, one is live and the other appears to be unused and dormant.
- Two existing Sydney Water DN225 sewer services traverse the site.
- Other services also exist onsite including electrical mains/kiosks and telecommunications services.
- Other stormwater, sewer, power, gas and telecoms services exist on the site and on adjoining properties and road reserves that surround the site.
- The subject site and its surrounding neighbours and roads are currently affected by 1% AEP flood events.

This Planning Proposal provides an opportunity to re-engineer, revitalise, renew and replace existing aged stormwater and sewer assets and to improve flooding conditions both on the site and in the broader region of the Strathfield Central site. The construction of a brand new stormwater culvert system as proposed would bring and provide considerable new benefits over the existing stormwater culvert situation including:

- Replacement and amplification of the existing aged 2.54 x 1.83m stormwater culvert asset (both within the site and at substantial length immediately upstream of the site under Redmyre Rd.)
- The removal and unburdening of this trunk stormwater system under numerous adjoining neighbour properties fronting the "The Boulevarde".
- A new culvert which would be designed to not adversely impact on existing overland flow and flooding.
- An opportunity for existing overland flooding to be diverted under the site within a specifically designed new culvert.
- Stormwater collected off the site would be suitably collected, detained, quality treated and recycled onsite (where viable).
- All habitable floors would be designed at suitable levels relative to large flood events.

The report provides initial concept design solutions for civil, stormwater, flood, sewer and other key services to be further developed as part of any future development application, including:

- A logical new diversion solution to the Sydney Water owned trunk stormwater system that traverses the site.
- A logical new route for the conveyance of overland stormwater events via new culverts and new stormwater overland conveyance flow paths.
- A logical diversion solution for the existing DN225 sewer that traverses the site.
- Recommended flood planning levels.

HERITAGE

A Heritage Impact Study (his) has been prepared by *Urbis* and is included at **Appendix E**. The subject property is not an individual listed heritage item on any statutory heritage list, nor is it located within a heritage conservation area. However, it is adjacent to two heritage listed retail buildings at the corner of Redmyre Road and The Boulevarde, being Items 200 and 201 under the Strathfield LEP 2012.

These two heritage items are separated from the subject property by Orrs Lane, and together with the five retail properties to the north along The Boulevarde, are identified as the C15 The Boulevarde Retail Conservation Area under the Strathfield LEP 2012.

The existing buildings and structures on the site are not considered to be of heritage significance and are not required to be retained on heritage grounds. Demolition of the existing buildings will not result in any detrimental heritage impacts to the place.

Conservation of the adjacent heritage items and conservation area buildings is dependent on a suitable demand for retail premises, which can only be achieved within a growing and vibrant town centre. The retention of a low-scale town centre would be to the detriment of the wider locality and may impact the longevity of the heritage retail buildings by depriving this area of a suitable retail demand.

While the proposed podium is of a higher scale than the existing retail buildings, it interprets the two-storey typology and the streetscape pattern and building rhythm in its form. Taller tower elements have been setback from the immediate streetscape and have been articulated to prevent dominant building forms. In this particular instance, the proposed higher scale future development is considered to have an acceptable heritage impact and will be complementary to the development of an energised and diverse town centre.

The heritage listed buildings are located outside of the subject site boundaries and are not included in the proposal. No buildings or fabric of heritage significance will be removed or impacted by the proposal. The heritage conservation area and its historic retail character will be retained.

The Planning Proposal is restricted to land to the rear of the adjacent heritage items and future built form will therefore not obscure any existing or important views towards the heritage buildings. While views towards these heritage buildings will change as they will be read in a broader context of high-density development, this is considered to be an acceptable given the town centre location and proximity to other elements of this important urban centre. There are no visual impacts to any of the heritage items in the wider vicinity of the subject property.

Overall the Planning Proposal will have negligible heritage impact.

WIND

A Wind Assessment has been prepared by Windtech and is included at **Appendix F**. The Assessment provides a description of the wind conditions on and around the proposed Strathfield Central redevelopment. The site is generally exposed to the prevailing southerly and north-easterly winds given the surrounding context, which is generally low-rise built form in all directions. The existing adjacent built form currently directs some of the north-easterly winds south towards the site.

The proposed development has considered the prevailing wind directions that currently impact the site, as well as the built adjacent. The tower has incorporated a number of key design features to respond to the potential impacts. These features include reducing the width of the tower forms and podium heights, increasing the setback of the towers from site boundaries, providing recessed areas and awnings along key street frontages and the Public Plaza space to reduce downwash. A landscape design solution will be developed as part of any future development application to mitigate impacts from wind exposure on the elevated outdoor terrace spaces.

Q9. Has the planning proposal adequately addressed any social and economic effects?

SOCIAL AND ECONOMIC

A Social and Economic Impact Assessment has been prepared by HillPDA and is included at **Appendix B**. The key findings of the assessment are summarised as follows:

Employment Generating Uses

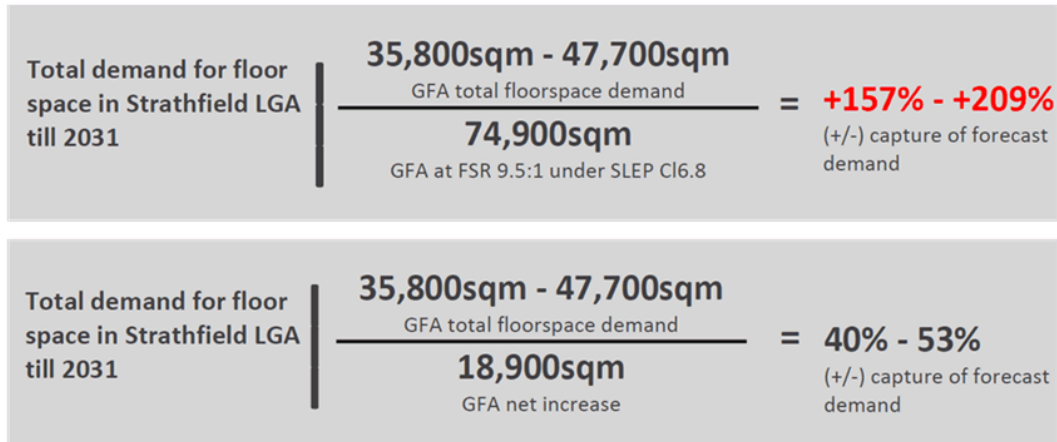
The Strathfield LEP 2012 Cl6.8 applies additional provisions for development in the Strathfield Town Centre. Specifically Cl6.8 requires 70% of the current maximum FSR at 7.5:1 for employment generating uses. The Planning Proposal provides 32,700sqm GFA of employment generating uses which equates to 31% of the proposed total GFA. The application of Cl6.8 to an FSR of 9.5:1 would equate to 74,900sqm GFA.

Using the Transport Performance and Analytics (TPA) employment projections data we calculate the total office floorspace demand at in Strathfield Local Government Area (LGA) to generate an additional demand of 35,800sqm - 47,700sqm GFA. If the Planning Proposal provided 74,900sqm GFA, this would oversupply the local market by 157%-209% of the estimated 10-year demand for the LGA (refer Figure 23).

Based on the forecast, it is economically unjustifiable for the Planning Proposal to provide 70% of the total GFA (74,900sqm GFA). The outcome of this quantum of employment floorspace would likely undermine the centres' economic demand for the next 10 years; as well as affect rents, and potentially create an undesirable outcome for the town centre.

The Planning Proposal provides an 18,900sqm GFA net increase of employment floorspace which would capture between 40%-53% of the estimated forecast 10-year demand for the LGA.

Figure 23 – Comparison of employment generating floor space 70% (above) vs. 31% (below)



Source: HillPDA

This Planning Proposal meets the overarching objective and intent of the SLEP Cl6.8 by providing additional employment floorspace in the town centre and is a considerable contribution to the Strathfield LGA. Providing 74,000sqm GFA of employment floorspace in one development is unrealistic.

Further, discussions with local agents found there is a demand for higher quality commercial office space than the existing B and C Grade stock currently available, but there would be limited/ constrained demand for a >50,000sqm GFA commercial tower. Agents also mentioned they regularly receive enquires for quality coworking space that can provide jobs close to homes. HillPDA would add, given the infrastructure investment into Parramatta and Burwood, these two higher order strategic centres would be a first preference to construct >50,000sqm GFA speculative office space. Parramatta and Burwood also possess significant competitive advantages over Strathfield Town Centre.








Economic Benefits

An economic benefit snapshot of the Planning Proposal, when compared to the Base Case is depicted in as follows:

Additional economic benefits include:

- Providing a catalyst for further investment in the locality;
- Contributing to increasing housing diversity and affordability in the LGA and District;
- Providing jobs closer to home and contributing to the LGAs employment targets;
- Contribute to Transit Orientated Development (TOD) objectives by concentrating more people near the train station and commercial services, thereby reducing the reliance on private motor vehicle travel and increasing the use of public transport; and
- Contributing to Sydney achieving the 30-minute city concept.

Figure 24 – Economic Benefit snapshot

		Base Case (i.e. do nothing)	Planning Proposal*	Above Base Case
During Construction (1)				
Economic Activity		Nil	\$2bil	+\$2bil
Jobs during construction		Nil	1,333	+1,333
Total jobs years from Construction		Nil	5,285	+5,285
Post construction				
Employment floorspace		13,800sqm	32,700sqm	+18,700sqm
Jobs on Site		608	1,421	+813
Staff Remuneration		\$24.8mil	\$280.3mil	+\$220.5mil
Gross Value Added to local economy		\$10.9mil	\$135.7mil	+\$125mil

Social Benefits

Figure 25 provides a summary of the social infrastructure audit findings and corresponding Planning Proposal benefits. Other social benefits from the Planning Proposal include:

- The delivery of jobs and homes in a highly accessible location close to services;
- A significant contribution to employment opportunities and diversity through the provision of around 5,285 jobs years during construction, and 1,421 additional local jobs upon completion;
- Increased housing diversity including an increase in the supply of dwellings that are more affordable for couples and young families;
- The increase in employment floorspace coupled with high quality retail space that will draw in workers and visitors from a broader catchment, facilitating opportunity for social interaction and ideas sharing;
- Improving the identity and character of Strathfield by locating an iconic mixed use precinct at the gateway of centre of the Strathfield Town Centre; and
- The potential to reposition Strathfield as a contemporary workplace and destination with a stimulating day and night time economy.

Figure 25 – Social Audit findings

Audit findings	Corresponding benefit
The audit identified a lack of open space, retail and community/cultural facilities in the area.	This Planning Proposal includes approximately 2,500sqm GFA of formal and informal public spaces activated through ground floor hospitality, retail uses in a highly accessible location. This proposal includes 700sqm GFA community facility located in a prominent area within the development.
The LGA has a shortage of smaller more affordable dwellings suited to young couples and young families	The proposed residential component of the Planning Proposal will go some way to addressing this need. The Planning Proposal will also provide 10% of the GFA uplift dedicated to key worker subsidised rental housing for 10 years.
Existing connections around the Strathfield Train Station precinct are relatively poor	This proposal creates fluid transport hub that connects to the train station and other key locations in the Strathfield Town centre, enhancing the accessibility for workers, residents and visitors in the broader precinct. This would assist in the potential decrease of anxiety and stress that is associated with traffic congestion.

Source: HillPDA

The Planning Proposal assists in meeting the Eastern City District housing targets. In addition, the Strathfield Economic Land Use and Employment Strategy 2010 calls for an increase residential development that will bring demand for additional retail and local services. The proposed redevelopment directly responds to the actions outlined in the Strathfield Economic Land Use and Employment Strategy 2010 by fostering high quality commercial floor space in the town centre, bolstered with public realm upgrades and a range of civic, retail and dining options.

Potential benefits arising from the development include 5,285 jobs years arising from construction and 1,421 additional local jobs when completed. The value added to the local economy is \$135.7mil every year. There will also be significant flow on effects both during and after construction, further increasing local economic activity and making significant differences in a region with higher levels of economic disadvantage.

Overall, the Planning Proposal would provide a significant benefit to the social and economic environment of Strathfield.

7.3.4. SECTION D – State and Commonwealth interests

Q10. Is there adequate public infrastructure for the planning proposal?

The Planning Proposal is adjacent to Strathfield Railway Station: a major junction for regional and suburban rail services with connections to Sydney CBD, Parramatta CBD, Liverpool, Penrith, Epping, Blue Mountains and Newcastle. The proposal seeks to embellish the existing public transport network by reconfiguring the bus interchange to provide better transition between mode shares and improve pedestrian connectivity to and from the train station. This investment will provide an impetus for a mode shift to public transportation versus private vehicles.

The Infrastructure Services Assessment (refer **Appendix D**) identifies that two trunk stormwater and two sewer services traverse the site. Other services also exist onsite including electrical kiosk/services and telecommunications services. Numerous other stormwater, sewer, power, gas and telecommunications services exist in the adjoining properties and road reserves that surround the site.

The report demonstrates via initial concept design:

- a diversion solution to the Sydney Water owned stormwater system that traverses the site by providing new routes for the conveyance of major stormwater events via new culverts and new stormwater overland conveyance flow paths.
- a diversion solution for the existing DN225 sewer that traverses the site.
- the on-site stormwater detention requirements from the analysis of Council's design standards and a recent DA approval near the site.

Future designs will be subject to the gaining of more detailed survey and further detailed design development, flood modelling and consultation with the applicable authorities such as Council and Sydney Water. The Proposal provides the opportunity to upgrade and embellish existing infrastructure needed to support the development and adjacent properties.

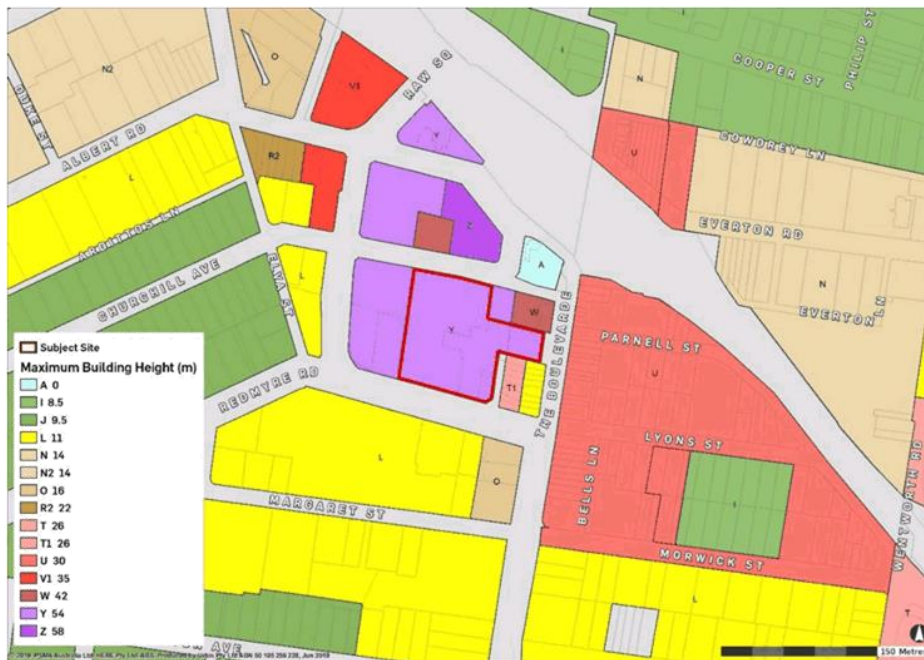
Q11. What are the view of State and Commonwealth public authorities consulted in accordance with the Gateway determination?

Gateway determination has not occurred at this stage. It is anticipated a pre-Gateway review process will be undertaken by the Department of Planning, Industry and Environment and the Sydney Eastern City Planning Panel.

7.4. PART FOUR: MAPPING

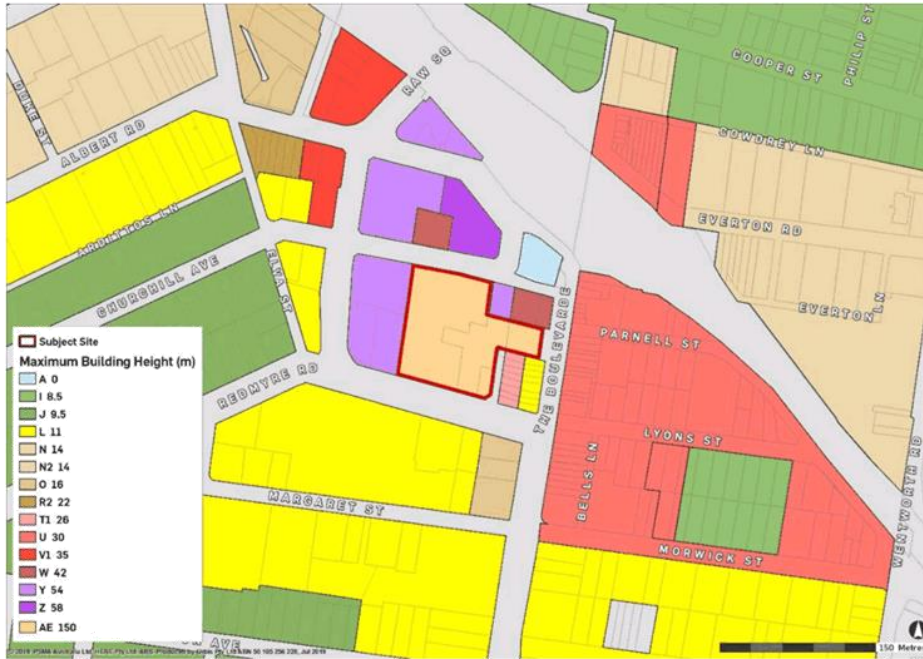
Detailed mapping has been prepared to show the current and proposed Height of Building and Floor Space Ratio development standards for the site (refer Figure 26 - Figure 29). The intended outcomes of the Planning Proposal are consistent with these maps.

Figure 26 – Existing Height of Building Map



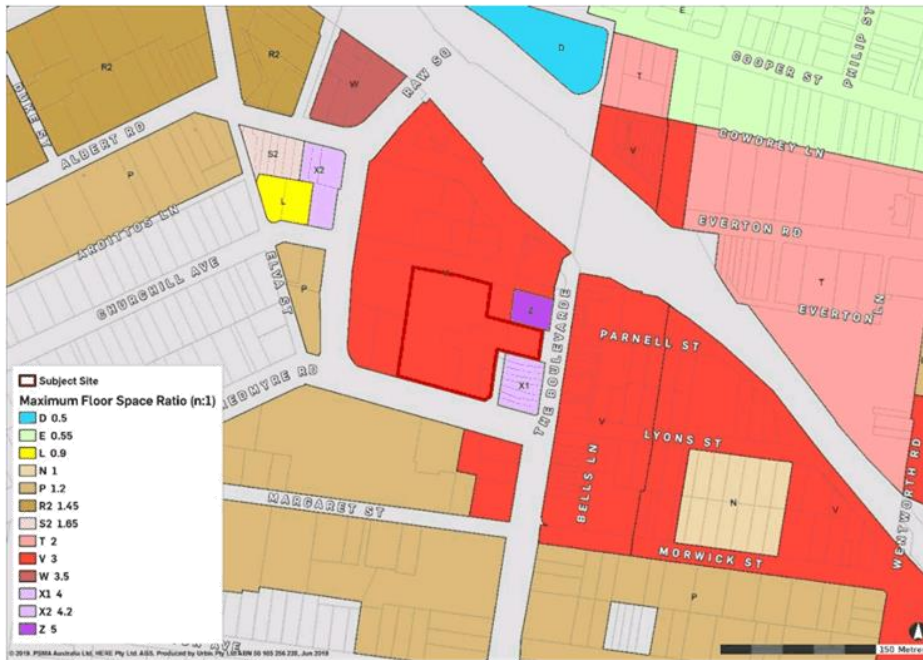
Source: Urbis

Figure 27 – Proposed Height of Building Map



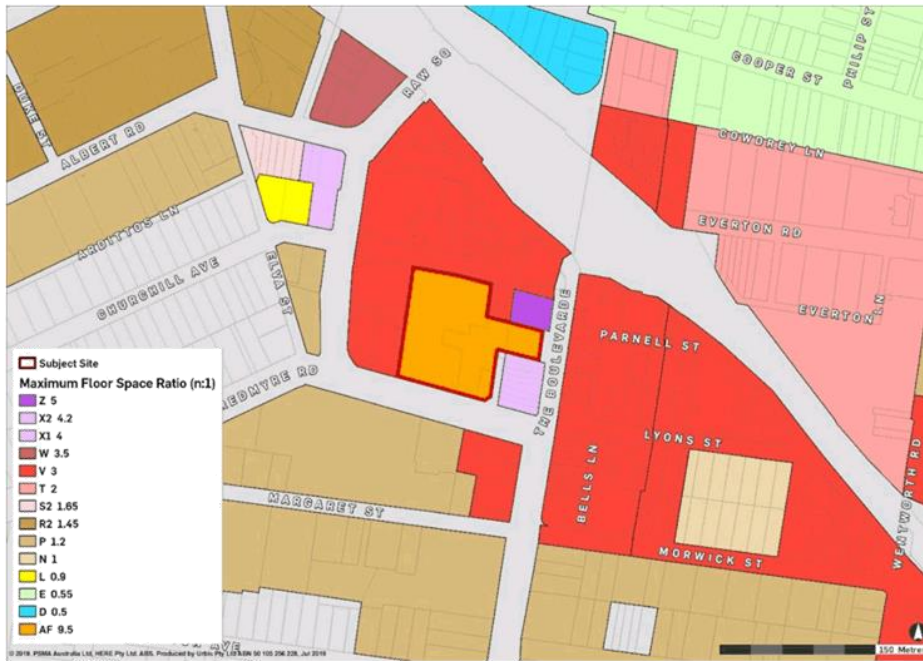
Source: Urbis

Figure 28 – Existing Floor Space Ratio Map



Source: Urbis

Figure 29 – Proposed Floor Space Ratio Map



Source: Urbis

7.5. PART FIVE: COMMUNITY CONSULTATION

No public community consultation has been undertaken to date in regard to this Planning Proposal. It is anticipated Gateway Determination will require the Planning Proposal to be made available for a minimum of 28 days for public comment.

7.6. PART SIX: PROJECT TIMELINE

In accordance with the requirements set out in 'A guide to preparing planning proposals', Table 5 sets out the anticipated project timeline, in order to provide a mechanism to monitor the progress of the planning proposal through the plan making process.

Table 5 – Anticipated Project Timeline

Process	Indicative Timeframe
Planning Proposal submitted to Strathfield Council	September 2019
Council endorsement of the Planning Proposal	November 2019
Strathfield Council forward Planning Proposal to the NSW Department of Planning, Industry and Environment	December 2019
Gateway Determination	February 2020
Update Planning Proposal (technical studies)	March 2020
Public Exhibition and Consultation	April 2020
Public Hearing	May 2020
Post Exhibition Review (consideration of submissions)	June – July 2020
Submission to NSW Department of Planning, Industry and environment for finalisation	August 2020
Legal Drafting of the LEP	September – November 2020
Notification of the LEP	December 2020
Total	18 Months

Note: Timeframe is indicative and based off an assumption of a 18-month rezoning process.

DISCLAIMER

This report is dated 24 September 2019 and incorporates information and events up to that date only and excludes any information arising, or event occurring, after that date which may affect the validity of Urbis Pty Ltd's (**Urbis**) opinion in this report. Urbis prepared this report on the instructions, and for the benefit only, of Memocorp Australia (**Instructing Party**) for the purpose of Planning Proposal (**Purpose**) and not for any other purpose or use. To the extent permitted by applicable law, Urbis expressly disclaims all liability, whether direct or indirect, to the Instructing Party which relies or purports to rely on this report for any purpose other than the Purpose, and to any other person which relies or purports to rely on this report for any purpose whatsoever (including the Purpose).

In preparing this report, Urbis was required to make judgements which may be affected by unforeseen future events, the likelihood and effects of which are not capable of precise assessment.

All surveys, forecasts, projections and recommendations contained in or associated with this report are made in good faith and on the basis of information supplied to Urbis at the date of this report, and upon which Urbis relied. Achievement of the projections and budgets set out in this report will depend, among other things, on the actions of others over which Urbis has no control.

In preparing this report, Urbis may rely on or refer to documents in a language other than English, which Urbis may arrange to be translated. Urbis is not responsible for the accuracy or completeness of such translations and disclaims any liability for any statement or opinion made in this report being inaccurate or incomplete arising from such translations.

Whilst Urbis has made all reasonable inquiries it believes necessary in preparing this report, it is not responsible for determining the completeness or accuracy of information provided to it. Urbis (including its officers and personnel) is not liable for any errors or omissions, including in information provided by the Instructing Party or another person or upon which Urbis relies, provided that such errors or omissions are not made by Urbis recklessly or in bad faith.

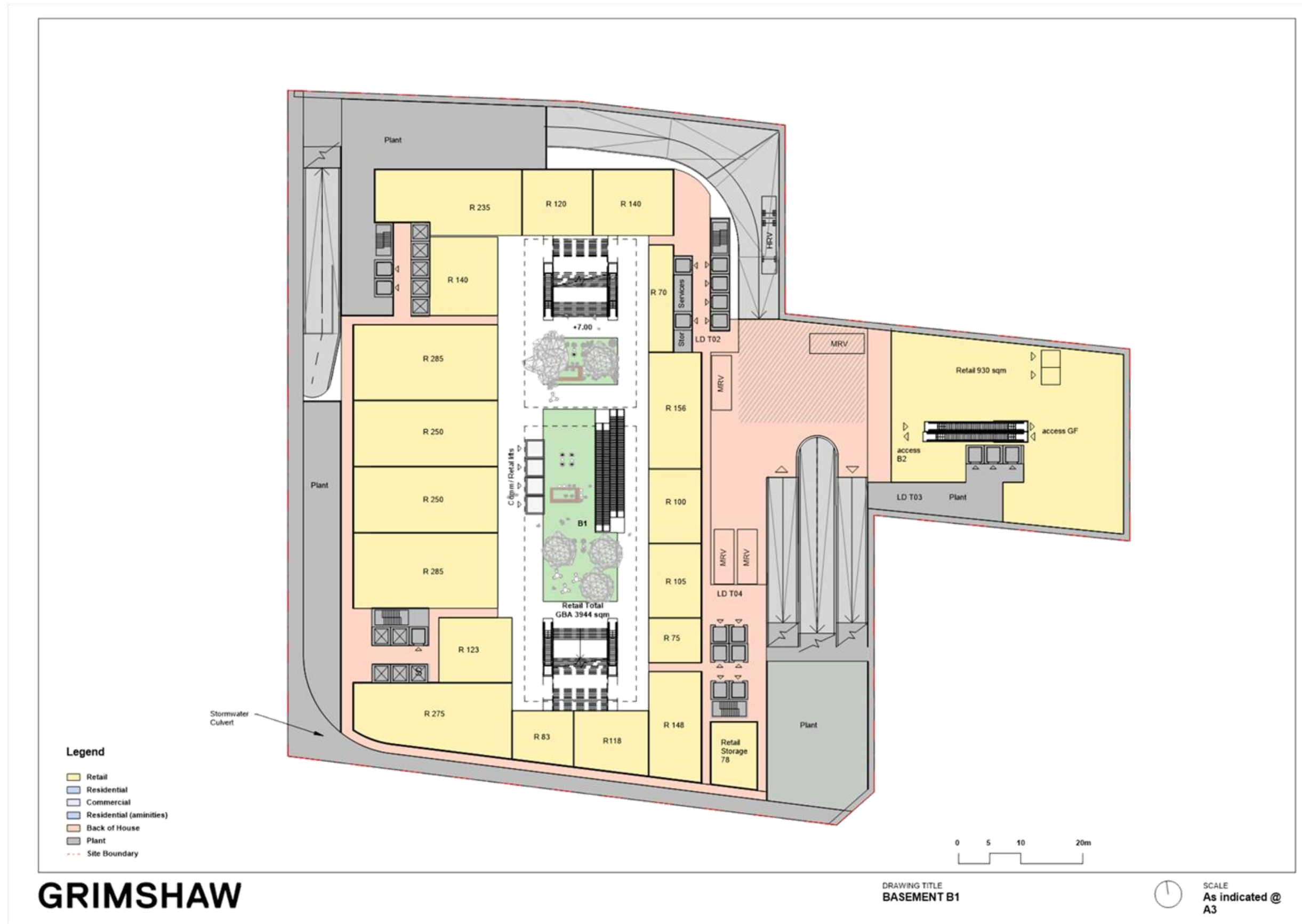
This report has been prepared with due care and diligence by Urbis and the statements and opinions given by Urbis in this report are given in good faith and in the reasonable belief that they are correct and not misleading, subject to the limitations above.

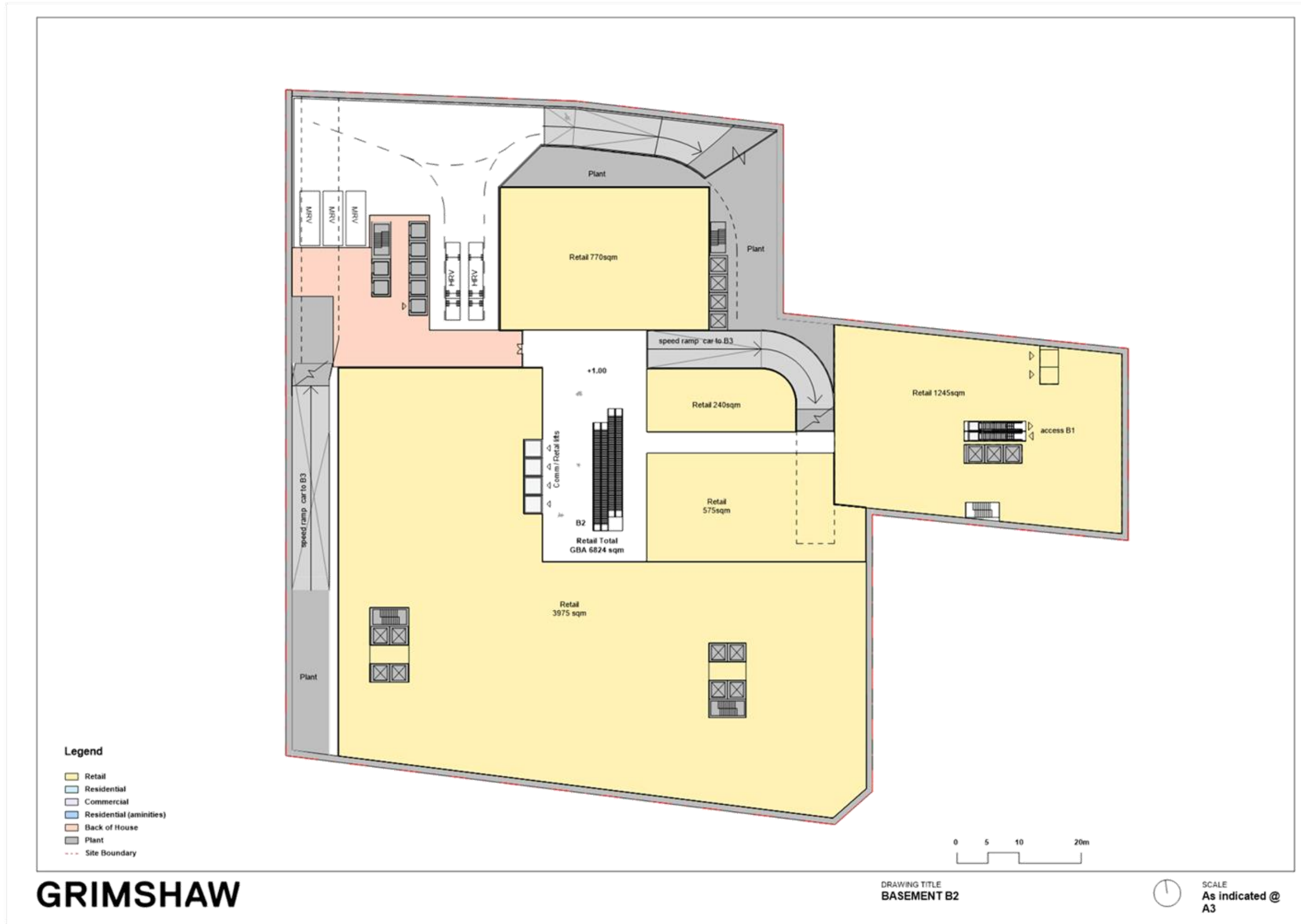
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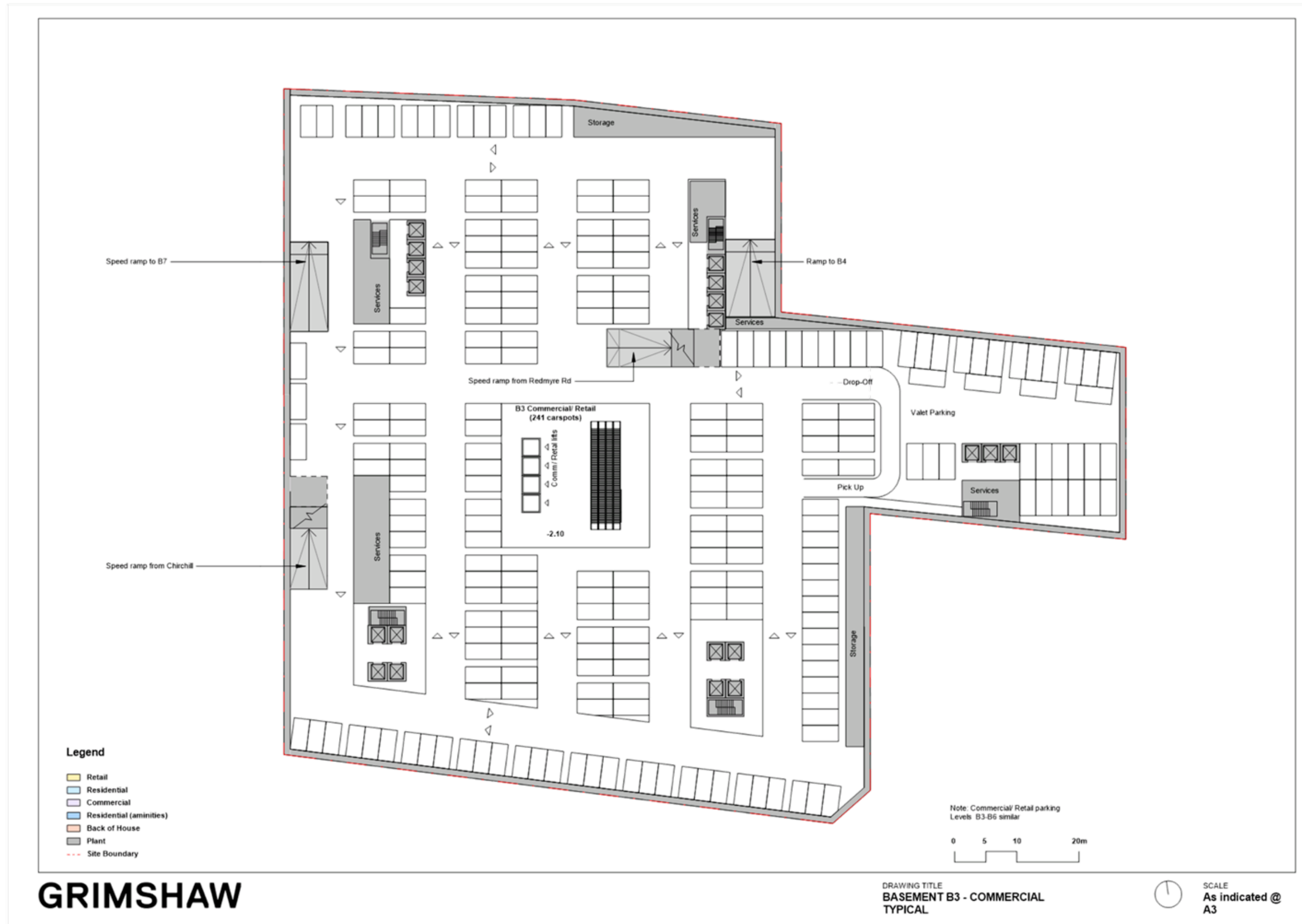
URBIS
STRATHFIELD CENTRAL_PLANNING PROPOSAL_FINAL

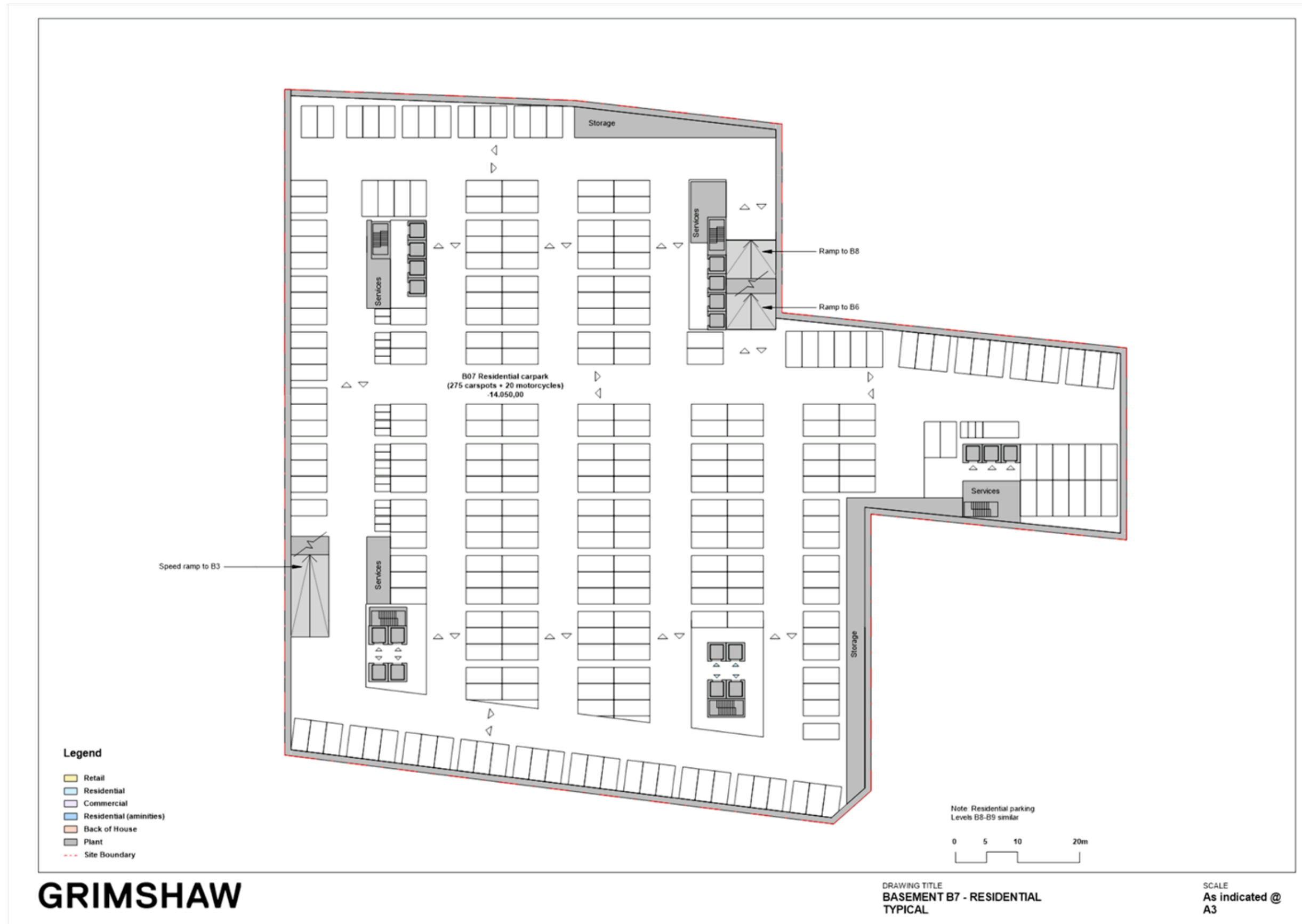


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GRIMSHAW

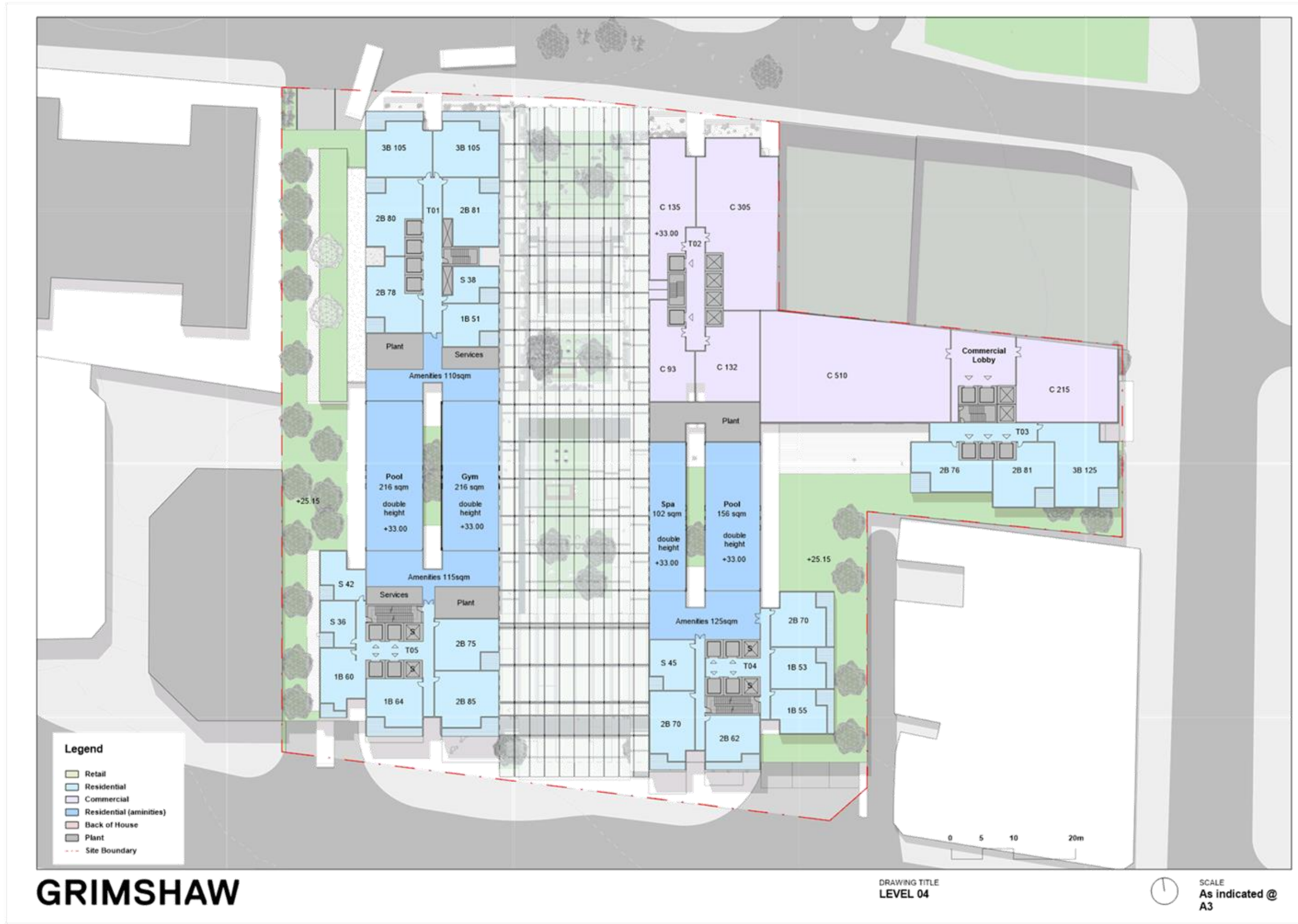
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SCALE
As indicated @
A3





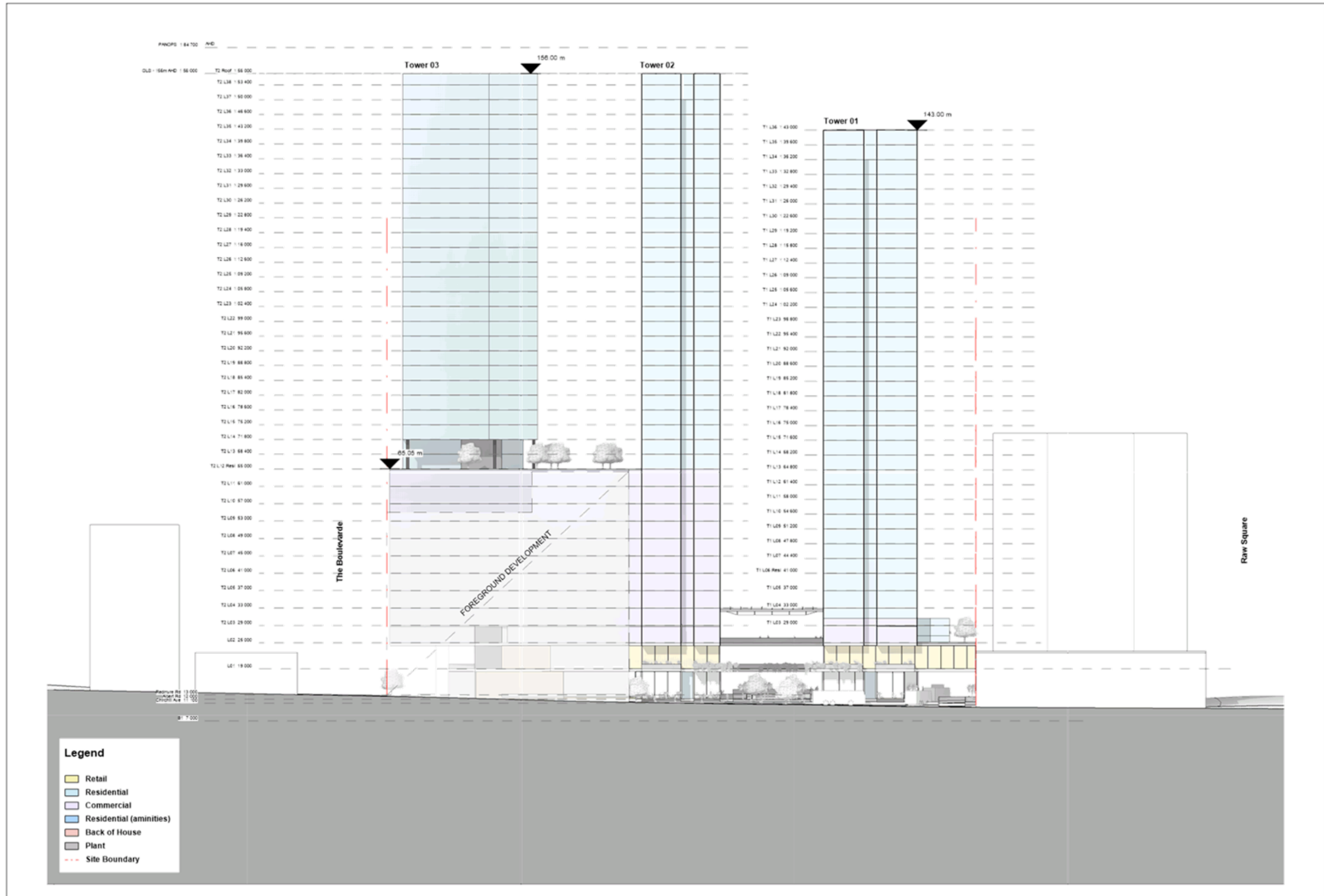








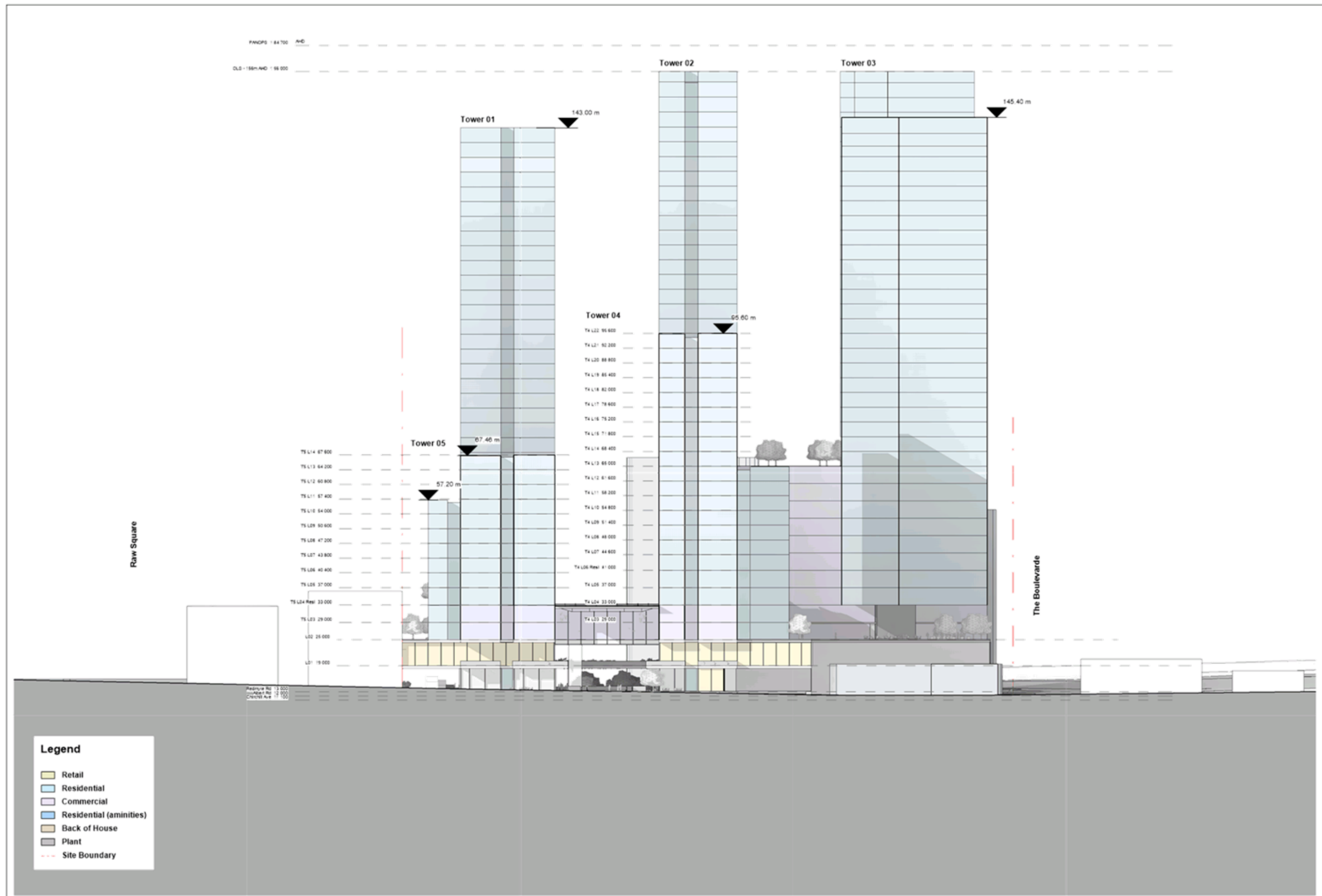




GRIMSHAW

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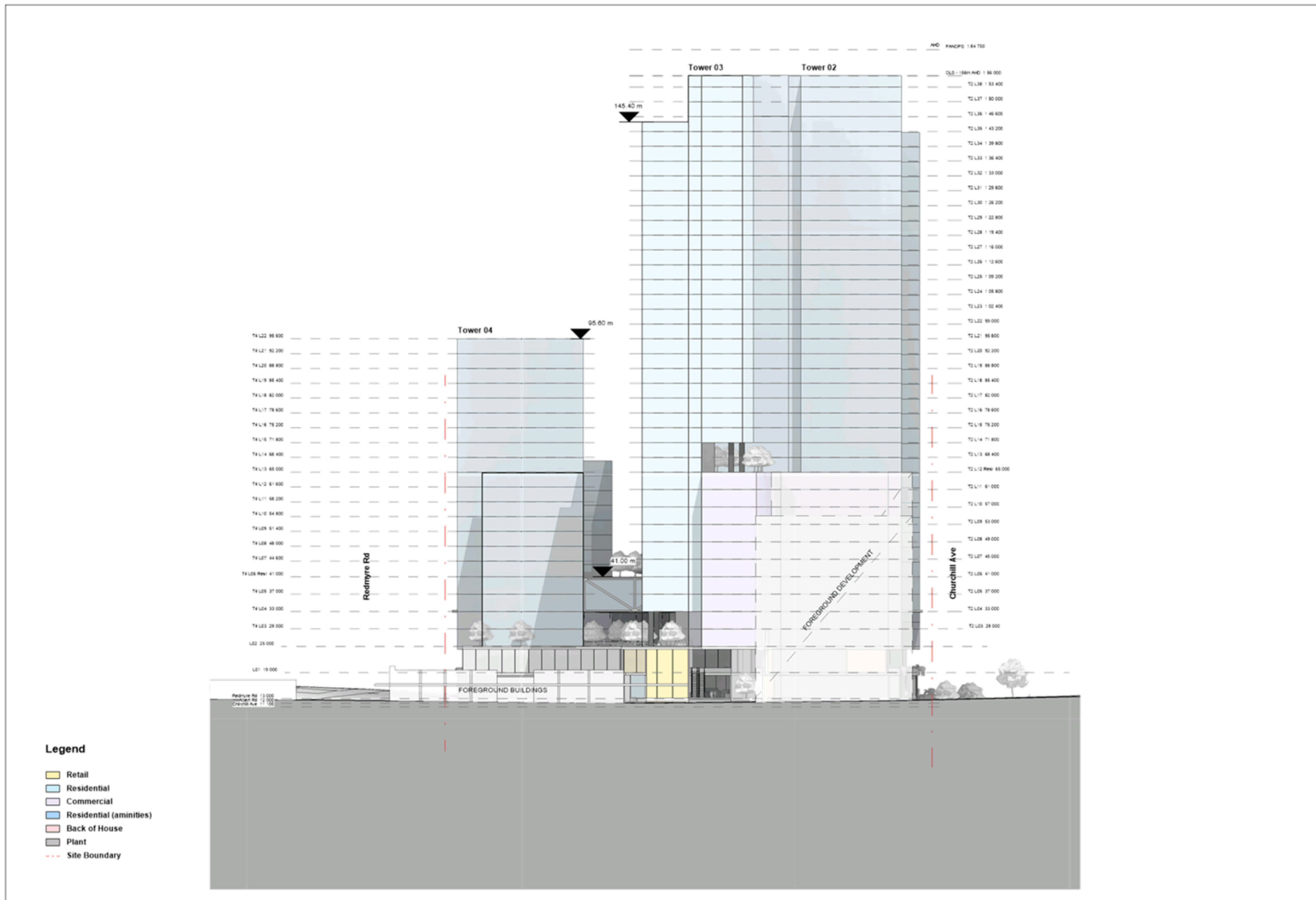
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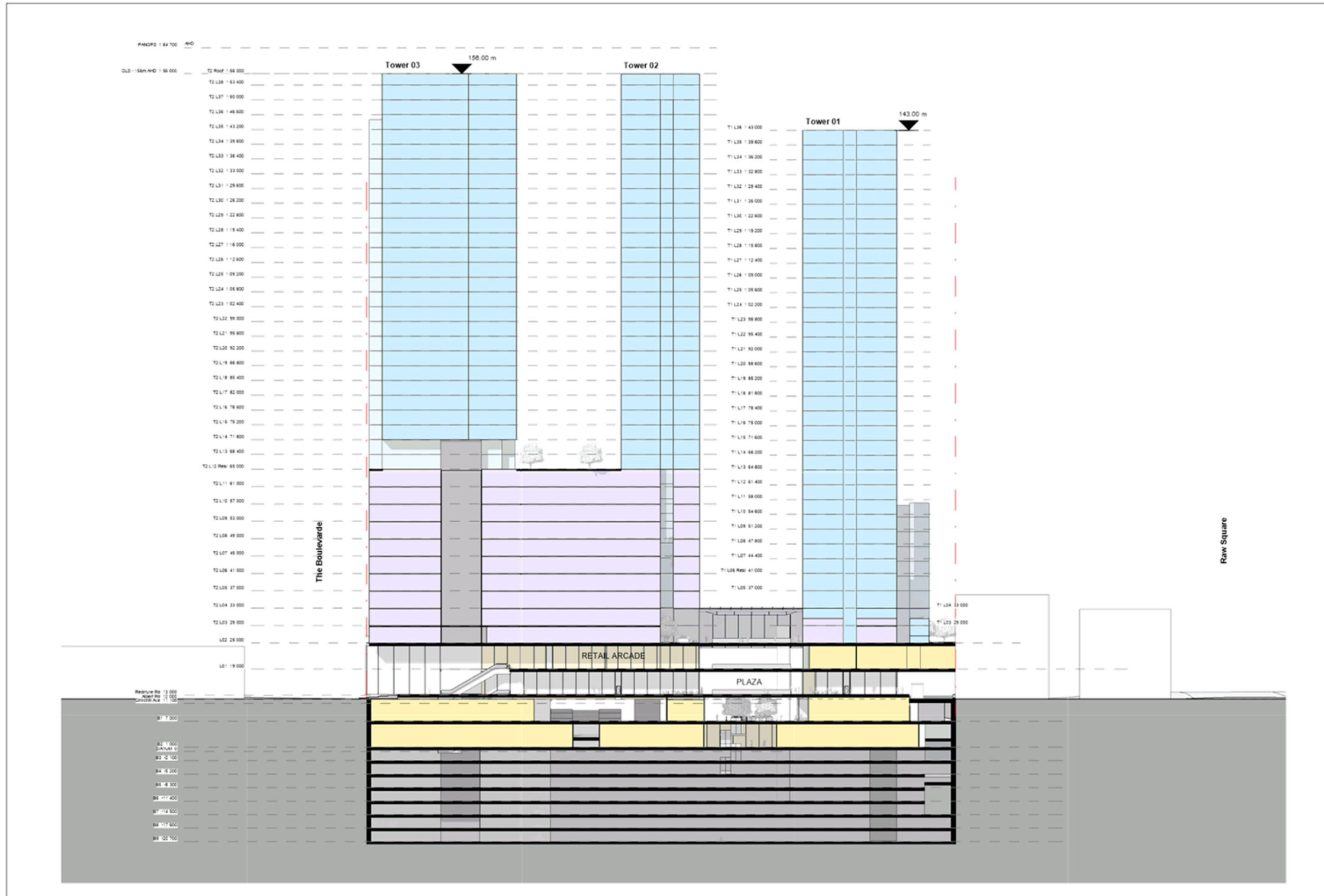
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GRIMSHAW

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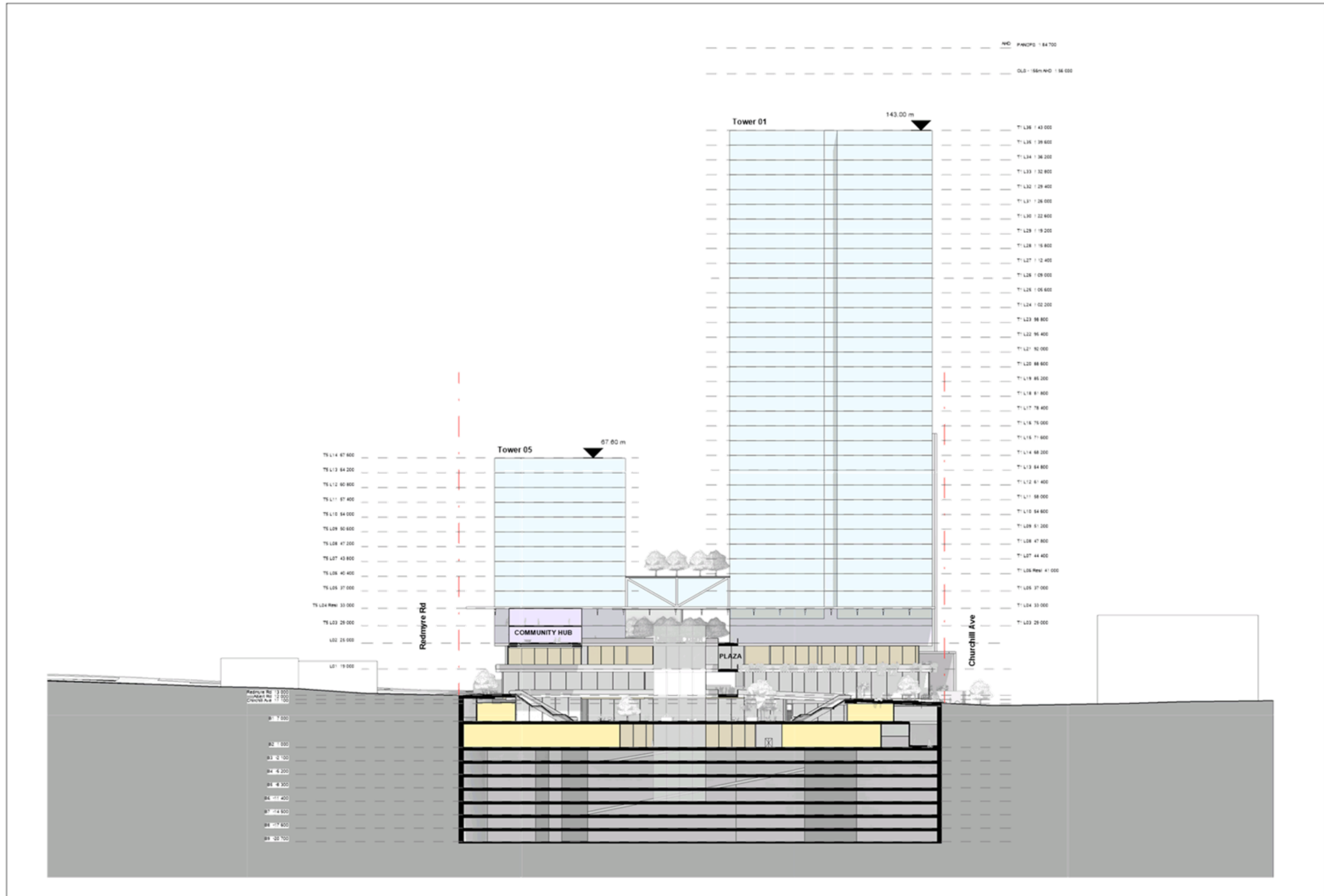
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GRIMSHAW

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SCALE
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GRIMSHAW

DRAWING TITLE
TRANSVERSAL

SCALE
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